



AGENDA

MILWAUKIE PLANNING COMMISSION Tuesday, June 10, 2014, 6:30 PM

MILWAUKIE CITY HALL
10722 SE MAIN STREET

- 1.0 **Call to Order - Procedural Matters**
- 2.0 **Planning Commission Minutes** – Motion Needed
 - 2.1 April 8, 2014
- 3.0 **Information Items**
- 4.0 **Audience Participation** – This is an opportunity for the public to comment on any item not on the agenda
- 5.0 **Public Hearings** – Public hearings will follow the procedure listed on reverse
 - 5.1 Summary: Moda Parking Expansion
 - Applicant/Owner: Moda Health
 - Address: 10505 SE 17th Ave
 - File: P-14-01, DR-14-03, WG-14-01
 - Staff: Denny Egner for Brett Kelper
- 6.0 **Worksession Items**
- 7.0 **Planning Department Other Business/Updates**
- 8.0 **Planning Commission Discussion Items** – This is an opportunity for comment or discussion for items not on the agenda.
- 9.0 **Forecast for Future Meetings:**
 - June 24, 2014
 - 1. Worksession: Moving Forward Milwaukie Downtown and Central Milwaukie Action & Implementation Plan
 - July 8, 2014
 - 1. TBD

Milwaukie Planning Commission Statement

The Planning Commission serves as an advisory body to, and a resource for, the City Council in land use matters. In this capacity, the mission of the Planning Commission is to articulate the Community's values and commitment to socially and environmentally responsible uses of its resources as reflected in the Comprehensive Plan

1. **PROCEDURAL MATTERS.** If you wish to speak at this meeting, please fill out a yellow card and give to planning staff. Please turn off all personal communication devices during meeting. For background information on agenda items, call the Planning Department at 503-786-7600 or email planning@ci.milwaukie.or.us. Thank You.
2. **PLANNING COMMISSION MINUTES.** Approved PC Minutes can be found on the City website at www.cityofmilwaukie.org
3. **CITY COUNCIL MINUTES** City Council Minutes can be found on the City website at www.cityofmilwaukie.org
4. **FORECAST FOR FUTURE MEETING.** These items are tentatively scheduled, but may be rescheduled prior to the meeting date. Please contact staff with any questions you may have.
5. **TIME LIMIT POLICY.** The Commission intends to end each meeting by 10:00pm. The Planning Commission will pause discussion of agenda items at 9:45pm to discuss whether to continue the agenda item to a future date or finish the agenda item.

Public Hearing Procedure

Those who wish to testify should come to the front podium, state his or her name and address for the record, and remain at the podium until the Chairperson has asked if there are any questions from the Commissioners.

1. **STAFF REPORT.** Each hearing starts with a brief review of the staff report by staff. The report lists the criteria for the land use action being considered, as well as a recommended decision with reasons for that recommendation.
2. **CORRESPONDENCE.** Staff will report any verbal or written correspondence that has been received since the Commission was presented with its meeting packet.
3. **APPLICANT'S PRESENTATION.**
4. **PUBLIC TESTIMONY IN SUPPORT.** Testimony from those in favor of the application.
5. **NEUTRAL PUBLIC TESTIMONY.** Comments or questions from interested persons who are neither in favor of nor opposed to the application.
6. **PUBLIC TESTIMONY IN OPPOSITION.** Testimony from those in opposition to the application.
7. **QUESTIONS FROM COMMISSIONERS.** The commission will have the opportunity to ask for clarification from staff, the applicant, or those who have already testified.
8. **REBUTTAL TESTIMONY FROM APPLICANT.** After all public testimony, the commission will take rebuttal testimony from the applicant.
9. **CLOSING OF PUBLIC HEARING.** The Chairperson will close the public portion of the hearing. The Commission will then enter into deliberation. From this point in the hearing the Commission will not receive any additional testimony from the audience, but may ask questions of anyone who has testified.
10. **COMMISSION DISCUSSION AND ACTION.** It is the Commission's intention to make a decision this evening on each issue on the agenda. Planning Commission decisions may be appealed to the City Council. If you wish to appeal a decision, please contact the Planning Department for information on the procedures and fees involved.
11. **MEETING CONTINUANCE.** Prior to the close of the first public hearing, *any person* may request an opportunity to present additional information at another time. If there is such a request, the Planning Commission will either continue the public hearing to a date certain, or leave the record open for at least seven days for additional written evidence, argument, or testimony. The Planning Commission may ask the applicant to consider granting an extension of the 120-day time period for making a decision if a delay in making a decision could impact the ability of the City to take final action on the application, including resolution of all local appeals.

The City of Milwaukie will make reasonable accommodation for people with disabilities. Please notify us no less than five (5) business days prior to the meeting.

Milwaukie Planning Commission:

Sine Bone, Chair
Wilda Parks, Vice Chair
Shannah Anderson
Scott Barbur
Shaun Lowcock
Gabe Storm

Planning Department Staff:

Denny Egner, Planning Director
Li Alligood, Associate Planner
Brett Kelver, Associate Planner
Vera Kolias, Associate Planner
Alicia Martin, Administrative Specialist II

CITY OF MILWAUKIE
PLANNING COMMISSION
MINUTES
Milwaukie City Hall
10722 SE Main Street
TUESDAY, APRIL 8, 2014
6:30 PM

COMMISSIONERS PRESENT

Sine Bone, Chair
Wilda Parks, Vice Chair
Shannah Anderson
Scott Barbur
Shaun Lowcock
Gabe Storm

STAFF PRESENT

Denny Egner, Planning Director
Li Alligood, Associate Planner
Brett Kelper, Associate Planner
Vera Koliass, Associate Planner
Peter Watts, City Attorney

COMMISSIONERS ABSENT

1.0 Call to Order – Procedural Matters*

Chair Bone called the meeting to order at 6:30 p.m. and read the conduct of meeting format into the record.

Note: The information presented constitutes summarized minutes only. The meeting video is available by clicking the Video link at <http://www.ci.milwaukie.or.us/meetings>.

2.0 Planning Commission Minutes

2.1 June 25, 2013

It was moved by Commissioner Barbur and seconded by Vice Chair Parks to approve the June 25, 2013, Planning Commission minutes as corrected. The motion passed unanimously.

2.2 February 25, 2014

It was moved by Commissioner Barbur and seconded by Commissioner Storm to approve the February 25, 2014, Planning Commission minutes as presented. The motion passed unanimously.

3.0 Information Items

Denny Egner, Planning Director, welcomed the new Planning Commissioner Shannah Anderson.

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Commissioner Anderson introduced herself and noted her background as natural resource planner for the City of Portland’s Bureau of Environmental Services. She said she was a new Milwaukie citizen and she looked forward to helping Milwaukie become a good place to live and play.

4.0 Audience Participation –This is an opportunity for the public to comment on any item not on the agenda. There was none.

5.0 Public Hearings

- 5.1 Summary: Mural Code Amendments
- Applicant: City of Milwaukie, Beth Ragel
- File: ZA-14-01
- Staff: Vera Kolas

Chair Bone called the hearing to order and read the conduct of legislative hearing format into the record.

Vera Kolas, Associate Planner, and Beth Ragel, Program Specialist, presented the staff report via PowerPoint.

Ms. Ragel noted she worked with the Milwaukie Art Board, known as artMOB, which had been working on this project for a few years. She described why public murals would benefit Milwaukie, including community enrichment and engagement. Local businesses and organizations were very interested in murals and added that, with changes happening in the city, this would be an opportunity to ‘brand’ the city.

She gave the background of the project, noting that murals previously fell under the sign code which limited the size of possible murals and there was no model in place that addressed legal issues. However, other local jurisdictions had implemented processes for murals which helped guide other communities. To address possible legal issues with regard to the First Amendment, objective review without review of content or discretionary review coupled with grant funding was advised. Ms. Ragel noted the public outreach conducted which yielded positive support.

77 **Ms. Koliias** explained that there was a new title being proposed, Title 20 Public Art Murals.
78 There would be two review options under the new title; Public Art Mural with discretionary
79 review; and Original Art Mural with objective review. Amendments to the Sign Ordinance, Title
80 14, would include clarification that murals were exempt from the sign code and would be
81 reviewed under Title 20. She noted the zones where art murals would be allowed. She
82 referenced the Commission's role in the review process for the amendments and reviewed the
83 decision options.

84

85 **Staff** answered questions by the Commission about legal issues, discretion, funding, lighting,
86 maintenance, and coverage percentages.

87

88 **Chair Bone** called for public testimony.

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90 **Kara Busick 4306 SE Harrison St**, thanked the Commission for their work on the project. She
91 was on the arts committee (artMOB) and was excited about the mural project. She felt that, in a
92 busy and technical modern world, murals were something to engage the community in
93 conversation or spark the imagination.

94

95 **Lisa Fossen, 11836 SE 34th Ave**, was the newest member of artMOB as was excited about the
96 mural project.

97

98 **Alicia Hamilton, 11921 SE 19th Ave**, was a co-organizer of First Friday and so was invested in
99 the arts and seeing it grow. She felt it would help build and strengthen the community. She
100 introduced Jay Freeman, a mural and street artist who had done demonstrations at First Friday
101 and donated projects for fundraising for the schools. She gave an example of him adding a
102 mural at a local studio with his own money, supplies, and time. She encouraged the
103 Commission to consider reasonable permit fees for murals to make it more accessible.

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105 **Chair Bone** closed the public hearing.

106

107 The Commission deliberated.

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109 **Mr. Egner** clarified that the Commission was required to make a recommendation on the Title
110 14 amendments, but staff were only asking for their comments on the Title 20 amendments.

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112 **Peter Watts, City Attorney**, advised for the two mural types to be as similar as possible as the
113 discrepancy between the Public Art Mural's up-to 40% coverage and the Original Art Mural's up-
114 to 100% coverage was a concern.

115

116 **Mr. Egner** explained that the reasoning being the 40% for the original art mural was due to the
117 possibility that that review option could be used for a sign with a purely commercial purpose with
118 no content review, whereas with the public art mural, there was content review.

119

120 **Vice Chair Parks** asked that the codes to be brought back to the Commission once the
121 program was up and running for a re-review.

122

123 **Commissioner Storm** asked why the Original Art mural option was necessary.

124

125 **Mr. Watts** clarified that under the Public Art Mural option, the content and aesthetic was
126 reviewed by a board and approved upon the merit of the piece. If there was only a content-
127 reviewed option, it would be violating freedom of speech. The Original Art Mural review was a
128 content-neutral path that was not regulating free speech. The areas of exposure laid in the
129 differences between the review options.

130

131 **Ms. Ragel** noted that staff recommended a 5-year minimum expectation for the Public Art
132 Murals.

133

134 **It was moved by Commissioner Storm and seconded by Commissioner Anderson to**
135 **make a recommendation to City Council for Title 20 Art Murals for definition of the**
136 **coverage percentages with relation to the "canvas"; clarification of how to assess fees;**
137 **and add a 5-year minimum rule.**

138

139 **It was moved by Vice Chair Parks and seconded by Commissioner Barbur to approve the**
140 **recommendation to City Council for ZA-14-01 Title 14 amendments and findings as**
141 **presented.**

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143 **6.0 Worksession Items**

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145 **7.0 Planning Department Other Business/Updates**

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147 **Mr. Egner** noted the Moving Forward Milwaukie Project Advisory Committee meeting that was
 148 held the previous week. He felt it was a good meeting with good discussion and turnout.

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150 **Chair Bone**, who was on the PAC, noted that the project was a good learning experience so far
 151 for the group regarding what the current code prescribed and how that matched up with market
 152 demand; in many cases the two were not in line. Also, public response seemed to not be in-line
 153 with market demands either (i.e. building height, etc.). The focus at the last meeting was on the
 154 Murphy and McFarland sites – should the city be more concerned about what was on the sites
 155 or how the sites were constructed (how it looked).

156

157 **Mr. Egner** added that there was support from the group regarding financial tools to explore the
 158 different types of public assistance to get the development the community would like to see.

159

160 **Chair Bone** encouraged the Commissioners to take the project's online survey.

161

162 **Mr. Egner** noted there was a worksession on the project tentatively scheduled with the
 163 Commission for May 13, 2014.

164

165 **8.0 Planning Commission Discussion Items**

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167 **9.0 Forecast for Future Meetings:**

168 April 22, 2014 1. TBD

169 *The Commission agreed to cancel the April 22, 2014 meeting.*

170 May 13, 2014 1. TBD

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173 Meeting adjourned at approximately 8:16 p.m.

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Respectfully submitted,

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179 Alicia Martin, Administrative Specialist II

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184 Sine Bone, Chair



MILWAUKIE

Dogwood City of the West

To: Planning Commission

Through: Dennis Egner, Planning Director

From: Brett Kolver, Associate Planner

Date: June 3, 2014, for June 10, 2014, Public Hearing

Subject: File(s): P-14-01, DR-14-03, WG-14-01
Applicant: Moda Health (represented by Jerry Offer of OTAK, Inc.)
Address: 10505 SE 17th Ave
Legal Description (Map & Taxlot): 1S1E35AA 03500
NDA: Historic Milwaukie

ACTION REQUESTED

Approve applications P-14-01, DR-14-03, and WG-14-01 and the recommended Findings and Conditions of Approval found in Attachments 1 and 2. This action would approve a modification of the maximum allowed parking for the existing office use, allowing for expansion of an existing parking area within the Willamette Greenway zone, including installation of parking lot lighting that matches existing lighting but is not consistent with the style noted in the Downtown Design Guidelines.

BACKGROUND INFORMATION

The applicant received approval in 2000 to improve the office building and off-street parking areas on the site through land use master file MU-00-01. The applicant proposes to expand the western parking area by reconfiguring it and adding approximately 30 spaces.

A. Site and Vicinity

The site is located at 10505 SE 17th Avenue. The site is approximately 7.2 acres and contains an office building approximately 58,500 square feet in area, as well as two off-street parking areas that total approximately 85,000 square feet. Access to the site is provided from SE Lava Drive, and Johnson Creek crosses through the southeastern portion of the property.

The surrounding area consists of residential uses. Multifamily apartments (Waverly Greens Apartments) are across Lava Dr to the north of the site, and single-family detached homes are across SE Riverway Lane to the west and south. The site's distance to the Willamette River to the west ranges from approximately 250 to 900 feet, with existing residential development in between (see Figure 1).

B. Zoning Designation

The site is primarily zoned Downtown Office (DO), with a small area of Residential R-2 in the southern portion and Downtown Open Space (DOS) on the east side of Johnson Creek. The site is also partly covered by the Willamette Greenway (WG) overlay (see Figure 2).

C. Comprehensive Plan Designation

Town Center (TC), with small area of High Density (HD) designation on southern portion of site

D. Land Use History

- April 2000:** Land use master file #MU-00-01, approved with conditions. Approved improvements to the office building and off-street parking areas. Project included applications for variance (from requirement to provide housing and ground-floor retail), Willamette Greenway conditional use review, natural resource review, and transportation plan review.

E. Proposal

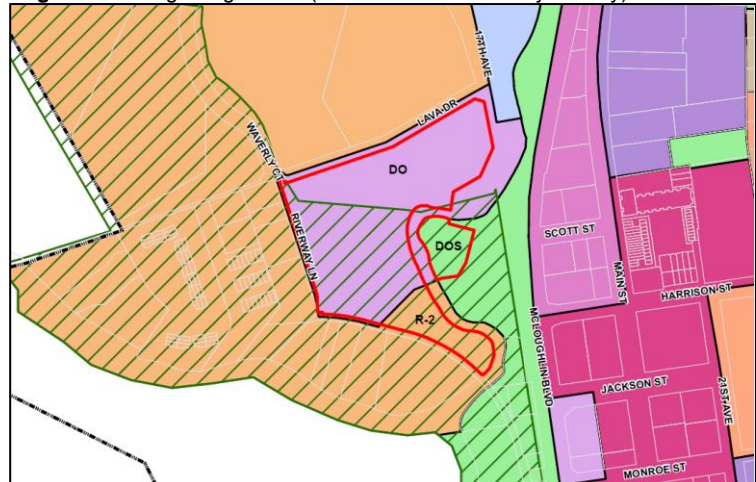
The applicant is seeking land use approvals for a parking quantity modification, downtown design review, and Willamette Greenway conditional use review.

The proposed development will expand the off-street parking area that currently exists on the west side of the office building, extending it to the south and reconfiguring some of the existing layout. Approximately 25 existing spaces will be removed and 55 new spaces added, resulting in a net increase of approximately 30 spaces. Twenty four (24) spaces will be designated for carpool use and 3 spaces would be reserved for fleet vehicles. Interior and perimeter landscaping will be installed in the expanded and reconfigured areas and additional lighting will be provided to match existing lighting. See Attachment 3 for applicant's materials.

Figure 1. Site and vicinity



Figure 2. Zoning designations (w/ Willamette Greenway overlay)



The project requires approval of the following applications:

1. Parking Quantity Modification (P-14-01)
2. Downtown Design Review (DR-14-03)
3. Willamette Greenway conditional use review (WG-14-01)

KEY ISSUES

Summary

Staff has identified the following key issues for the Planning Commission's deliberation. Aspects of the proposal not listed below are addressed in the Findings (see Attachment 1) and generally require less analysis and discretion by the Commission.

- A. How does the applicant's proposed maximum parking ratio compare to the ratios established by other jurisdictions for a similar office use?
- B. How do the applicant's Transportation Demand Management (TDM) efforts relate to the request to exceed the maximum allowable number of spaces?
- C. What did the Design and Landmarks Committee (DLC) have to say about the request to copy the existing style of lighting (instead of the ornamental style recommended by the Downtown Design Guidelines) and the Willamette Greenway review?

Analysis

A. How does the applicant's proposed maximum parking ratio compare to the ratios established by other jurisdictions for a similar office use?

The maximum parking allowance for general office use, established in MMC Table 19.605.1, is 3.4 spaces per 1,000 sq ft of floor area. After utilizing the exemptions for employee carpool parking and fleet parking allowed in MMC Subsection 19.605.3.A, the applicant has proposed a maximum ratio of 3.6 spaces per 1,000 sq ft. The applicant's narrative notes a general comparison of the maximum parking allowances for office uses from several local jurisdictions. Staff look at a few additional jurisdictions to confirm the applicant's notes, with the results presented in Table 1, below.

Table 1. Comparison of Off-Street Parking Standards for General Office Uses

Jurisdiction	Maximum Allowed Ratio* # of spaces per 1,000 sq ft of floor area
Milwaukie	3.4 spaces
Gladstone	4.1 spaces
West Linn	3.1 spaces
Lake Oswego	4.2 spaces
Tualatin	4.1 spaces

Oregon City	3.33 spaces
Clackamas County	4.1 spaces
Portland	2.5 to 3.4 spaces
Beaverton	4.1 spaces
Washington County	4.3 spaces

* For areas not served by high frequency peak period transit within a quarter mile

There is no uniformity among the jurisdictions considered, with respect to the maximum parking allowance for office uses. However, it does appear that Milwaukie is among those jurisdictions with a more restrictive standard. The increase proposed by the applicant (to 3.6 spaces per 1,000 sq ft) is a modest one and would still be far below the maximum standards maintained by most of the jurisdictions listed.

B. How do the applicant's Transportation Demand Management (TDM) efforts relate to the request to exceed the maximum allowable number of spaces?

According to the applicant's narrative, Moda Health does a number of things to reduce the demand for on-site parking. The company provides transit information and subsidies for transit passes for employees, and there is a stop for TriMet's route 70 bus line on 17th Ave in front of the office. There is secure bicycle storage available both inside the office building and outside the south entrance, with lockers and employee showers available for those who commute by bike. The existing site provides 6 designated carpool parking spaces, and the proposed improvements include 18 new carpool spaces near the south entrance. The company does a monthly drawing for prizes to encourage alternative means of transportation and allows flex-time scheduling and telecommuting.

The company's TDM efforts help reduce parking demand, and there is the potential for even greater demand reduction in the future with anticipated new transportation facilities and connectivity. The City is in the process of constructing a multiuse path on 17th Ave between SE Ochoco Street and McLoughlin Blvd at Harrison St, which will improve connections with the Springwater Trail, Tacoma Street light rail station, Trolley Trail, and downtown light rail station. The new light rail (due to open in September 2015) will replace several existing bus lines and may provide a new commuting option for some employees—the company could explore options for employee shuttles to the nearby light rail stations at peak times. Additional bicycle parking outside the building could make biking an even more accessible option for employees and visitors alike, without giving up any more interior floor space.

At the same time, despite the company's efforts to reduce parking demand, the existing lots are filled to capacity while employment at the site continues to grow. There are no nearby opportunities for shared parking, and one can wonder whether the 30 parking permits that Moda has obtained for the public lots downtown are really the best use of the limited parking inventory there, especially since those permits represent vehicles displaced from the Moda site and not an actual reduction of vehicle trips. Since the company has room on the site to expand parking impacting neighboring properties or the designated natural resource areas along Johnson Creek, it seems reasonable to allow the proposed modest increase in maximum allowed parking to accommodate the current need.

C. What did the Design and Landmarks Committee (DLC) have to say about the request to copy the existing style of lighting (instead of the ornamental style recommended by the Downtown Design Guidelines) and the Willamette Greenway review?

The DLC reviewed the Downtown Design Review and Willamette Greenway conditional use review aspects of the proposed development. At a design review meeting on May 19, 2014, the DLC agreed to recommend approval (see Attachment 4).

With respect to the issue of lighting, it is worth noting that the existing parking area was established in 2000, prior to the adoption of the Downtown Design Guidelines in 2003. The existing lighting fixtures (see Figure 3) do not match the ornamental style prescribed in the Downtown Design Guidelines (see Figure 4). However, the site is not physically connected to the central downtown area, where other ornamental lights have been installed. The DLC agreed that requiring the new lights to be a different style than the existing lights would promote incongruous design on the site. Furthermore, the ornamental style of light does not avail itself to shielding for limiting light trespass into the natural resource areas on the site adjacent to the parking area.

The DLC recommends that the new lighting provided in the expanded portions of the parking area be allowed to be similar in style to the existing lighting, instead of requiring the ornamental style of light.

With respect to the Willamette Greenway, the DLC agreed that the proposed development is consistent with the nature of existing development on the site, which is largely not visible from the river. Views to and from the river will not be affected by the

proposed development and the site does not provide public access to the river. Impacts to the designated Habitat Conservation Area (HCA) on the site are minor in scale and can be addressed through the nondiscretionary process outlined in MMC Subsection 19.402.11.D.

In short, the proposed development is consistent with the objectives and policies for the Willamette Greenway, as established in both the Milwaukie Comprehensive Plan and Zoning Ordinance (specifically, the approval criteria of MMC Subsection 19.401.6—see Attachment 1, Recommended Findings in Support of Approval).

Figure 3. Existing light (proposed)

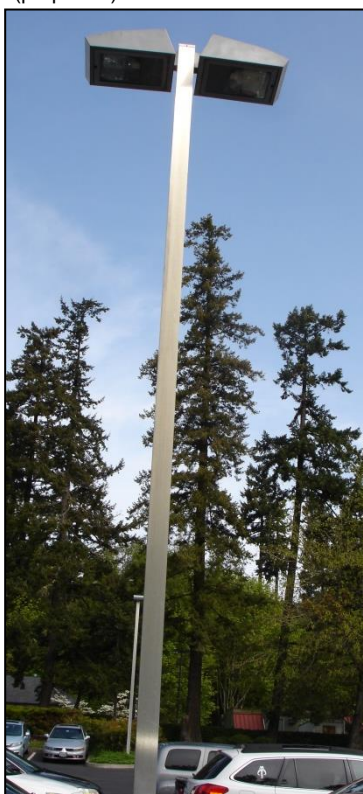


Figure 4. Ornamental light (recommended)



CONCLUSIONS

A. Staff recommendation to the Planning Commission is as follows:

1. Approve the requested Parking Modification, to allow the existing western parking area to be expanded to provide a higher ratio of spaces per square footage of office use than is provided in MMC Table 19.605.1.
2. Approve the Downtown Design Review application for the proposed parking expansion. This will allow the new parking lot lighting to match the existing lighting style instead of the recommended ornamental style.
3. Approve the Willamette Greenway conditional use application for the proposed parking expansion. This will allow the parking expansion as a major modification to the existing Willamette Greenway conditional use.
4. Adopt the attached Findings and Conditions of Approval.

B. Staff recommends the following key conditions of approval (see Attachment 2):

- At the time of development review, provide a final parking lot site plan that demonstrates compliance with all applicable standards of MMC Section 19.606, including parking space and drive aisle dimensions, pavement markings, perimeter and interior landscaping, pedestrian access, and lighting.

Other conditions of approval may be generated by the Planning Commission in association with the parking quantity modification request, and/or to ensure that standards established in the zoning code are met at specific points in the development process.

CODE AUTHORITY AND DECISION-MAKING PROCESS

The proposal is subject to the following provisions of the Milwaukie Municipal Code (MMC).

- MMC 19.1006 Type III Review
- MMC Chapter 19.600 Off-Street Parking and Loading
- MMC Section 19.907 Downtown Design Review
- MMC Section 19.1011 Design Review Meetings
- MMC Section 19.401 Willamette Greenway Zone
- MMC Section 19.905 Conditional Uses
- MMC Chapter 19.700 Public Facility Improvements

This application is subject to Type III review, which requires the Planning Commission to consider whether the applicant has demonstrated compliance with the code sections shown above. In Type III reviews, the Commission assesses the application against review criteria and development standards and evaluates testimony and evidence received at the public hearing.

The Commission has four decision-making options as follows:

- A. Approve the applications subject to the recommended Findings and Conditions of Approval.

- B. Approve the applications with modified Findings and Conditions of Approval. Any modifications must be read into the record.
- C. Deny the applications upon finding that they do not meet approval criteria.
- D. Continue the hearing.

The final decision on these applications, which includes any appeals to the City Council, must be made by August 23, 2014, in accordance with the Oregon Revised Statutes and the Milwaukie Zoning Ordinance. The applicant can waive the time period in which the application must be decided.

COMMENTS

Notice of the proposed changes was given to the following agencies and persons: City of Milwaukie Community Development, Building, and Engineering Departments; Clackamas Fire District; Historic Milwaukie Neighborhood District Association (NDA); Design and Landmarks Committee; Oregon Department of Transportation (ODOT); Oregon Department of Parks and Recreation; Department of State Lands; Oregon Department of Fish and Wildlife; and State Marine Board.

The comments received are summarized as follows:

- a. **John Stelzenmueller, Milwaukie Building Department:** No comments on the proposal at this time.
- b. **Philip Kolb, Milwaukie Engineering Department:** Comments related to MMC Chapter 19.700 Public Facility Improvements, which is not applicable to the proposed development. Those comments have been incorporated into these findings. Other requirements related to stormwater management have been noted with the conditions of approval.
- c. **Jean Baker, Historic Milwaukie NDA Land Use Committee Chair:** The Historic Milwaukie NDA has no objection to the proposal.
- d. **Shawn Olson, Clackamas Fire District:** Comments related to the need for “No Parking Fire Lane” signs at appropriate locations and for turning radius dimensions on plans submitted with development permits. Those comments have been noted with the conditions of approval.
- e. **Seth Brumley, ODOT:** No comments on this proposal.

ATTACHMENTS

Attachments are provided as indicated by the checked boxes. All material is available for viewing upon request.

	Early PC Mailing	PC Packet	Public Copies	E-Packet
1. Recommended Findings in Support of Approval	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
2. Recommended Conditions of Approval	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>

	Early PC Mailing	PC Packet	Public Copies	E- Packet
3. Applicant's Narrative and Supporting Documentation dated April 25, 2014				
a. Narrative	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
b. Appendices <i>(including Preapplication notes, Notice of Decision from MU-00-01, Application forms, Parking area lights cut sheet, Photos of existing bicycle parking, and Storm drainage report)</i>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
c. Plan Sheets <i>(including Cover sheet/vicinity map, Existing conditions, Overall site plan, Western parking lot site plan, Site grading and tree removal plan, Landscape plan, and Planting notes and details)</i>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
4. Minutes from DLC Design Review Meeting DLC (May 19, 2014)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>

Key:

Early PC Mailing = paper materials provided to Planning Commission at the time of public notice 20 days prior to the hearing.

PC Packet = paper materials provided to PC 7 days prior to the hearing.

Public Copies = paper copies of the packet available for review at City facilities and at the Planning Commission meeting.

E-Packet = packet materials available online at <http://www.milwaukieoregon.gov/planning/planning-commission-105>.

**Recommended Findings in Support of Approval
File #s P-14-01, DR-14-03, and WG-14-01
Moda Health Parking Expansion**

Sections of the Milwaukie Municipal Code not addressed in these findings are found to be inapplicable to the decision on this application.

1. The applicant, Moda Health, has applied for approval to construct additional off-street parking at the company's office building at 10505 SE 17th Avenue. The proposal requires modification of the maximum allowed parking quantity for the office use. This site is in the Downtown Office Zone, and a portion of the site is covered by the Willamette Greenway overlay. The land use application file numbers are P-14-01, DR-14-03, and WG-14-01.
2. The proposed development will expand the off-street parking area that currently exists on the west side of the office building, extending it to the south and reconfiguring some of the existing layout. Approximately 25 existing spaces will be removed and 55 new spaces added, resulting in a net increase of approximately 30 spaces. Twenty four (24) spaces will be designated for carpool use and 3 spaces will be reserved for fleet vehicles. Interior and perimeter landscaping will be installed in the expanded and reconfigured areas and additional lighting will be provided to match existing lighting.
3. The proposal is subject to the following provisions of the Milwaukie Municipal Code (MMC):
 - MMC Section 19.1006 Type III Review
 - MMC Chapter 19.600 Off-Street Parking and Loading
 - MMC Section 19.907 Downtown Design Review
 - MMC Section 19.1011 Design Review Meetings
 - MMC Section 19.401 Willamette Greenway Zone
 - MMC Section 19.905 Conditional Uses
 - MMC Chapter 19.700 Public Facility Improvements
4. The application has been processed and public notice provided in accordance with MMC Section 19.1006 Type III Review. As required by MMC Subsection 19.907.8, and in accordance with the procedures outlined in MMC Section 19.1011, a public design review meeting was held by the Design and Landmarks Committee on May 19, 2014. A public hearing was held by the Planning Commission on June 10, 2014, as required by law.
5. MMC Chapter 19.600 Off-Street Parking and Loading

MMC 19.600 regulates off-street parking and loading areas on private property outside the public right-of-way. The purpose of these requirements includes providing adequate space for off-street parking, minimizing parking impacts to adjacent properties, and minimizing environmental impacts of parking areas.

 - a. MMC Section 19.602 Applicability

MMC 19.602 establishes the applicability of the provisions of MMC 19.600.

 - (1) MMC Subsection 19.602.1 General Applicability

MMC 19.602.1 provides that the regulations of MMC 19.600 apply to all off-street parking areas, whether required by the City as part of development or voluntarily installed for the convenience of users. Activity that is not described by

MMC Subsections 19.602.3 or 19.602.4 is exempt from compliance with the provisions of MMC 19.600.

The subject property includes existing off-street parking areas. The proposed development is an activity that meets the applicability standards of MMC 19.602.4 (see Finding 5-a-(3)).

(2) MMC Subsection 19.602.2 Maintenance Applicability

MMC 19.602.2 provides that property owners shall ensure conformance with the standards of MMC 19.600 with regard to ongoing maintenance, operations, and use of off-street parking areas. Any change to an existing off-street parking area shall not bring the area out of conformance, or further out of conformance if already nonconforming.

The proposed development will expand an existing off-street parking area. The proposed improvements are in conformance with the applicable standards of MMC 19.606, including for stall dimensions, landscaping, and lighting (see Finding 5-d).

(3) MMC Subsection 19.602.4 Applicability not Associated With Development or Change in Use

MMC 19.602.4.A addresses applicability for parking projects developed to serve an existing use but not associated with other development activity or a change in use. Such activity shall conform to the requirements of MMC Sections 19.604 and 19.606-19.611. In addition, the total number of new spaces in the existing and new parking areas shall not exceed the maximum allowed quantity of parking as established in MMC Section 19.605.

The proposed development will serve an existing use on the subject property and is not associated with other development activity or a change in use. As addressed in Findings 5-c and 5-e through 5-h, the proposed development will meet all applicable standards of MMC Sections 19.604 and 19.606-611. As discussed in Finding 5-d, the applicant has requested a modification to the maximum allowed parking quantity.

The Planning Commission finds that the standards and requirements of MMC 19.600 are applicable to the proposed development.

b. MMC Section 19.603 Review Process and Submittal Requirements

(1) MMC Subsection 19.603.1 Review Process

MMC 19.603.1 establishes the Planning Director as the entity with authority to apply the provisions of Chapter 19.600 unless an application is subject to a quasi-judicial review or appeal, in which case the body reviewing the application has the authority.

The application package (Downtown Design Review, Willamette Greenway conditional use review, and Parking Modification request) is subject to Type III review by the Planning Commission, which is therefore the body with authority to apply the provisions of Chapter 19.600.

(2) MMC Subsection 19.603.2 Submittal Requirements

MMC 19.603.2 establishes the requirements for submittal of a parking plan, including the various details that must be presented.

The applicant has submitted a parking plan and supporting information with sufficient detail to demonstrate compliance with the applicable standards of Chapter 19.600.

The Planning Commission finds that this standard is met.

c. MMC Section 19.604 General Parking Standards

MMC 19.604 establishes general standards for off-street parking areas, including requirements related to the provision of parking in conjunction with development activity, the location of accessory parking, and use and availability of parking areas.

The proposed development is an expansion of an existing off-street parking area related to an ongoing office use on the subject property. The new parking will be on the same site as the primary office use and will be available for the office use.

The Planning Commission finds that this standard is met.

d. MMC Section 19.605 Vehicle Parking Requirements

MMC 19.605 establishes standards to ensure that development provides adequate vehicle parking based on estimated parking demand.

(1) MMC Subsection 19.605.1 Minimum and Maximum Requirements

MMC Table 19.605.1 provides minimum and maximum requirements for a range of different uses. For general office uses, a minimum of 2 spaces per 1,000 sq ft of floor area are required; a maximum of 3.4 spaces per 1,000 sq ft are allowed.

The subject property is developed with an office building approximately 58,500 sq ft in area. A minimum of 117 parking spaces are required for that size of office use; a maximum of 199 spaces are allowed. The site includes two existing off-street parking areas that provide a total of 206 spaces. Six of those spaces are for carpool parking and 3 spaces are for fleet vehicles, which are exempt from counting toward the maximum number as per MMC Subsection 19.605.3. Therefore, the site currently provides a net total of 197 spaces, which falls between the minimum requirement and maximum allowance for the current use.

The proposed development would result in a total of 236 spaces, with 24 carpool spaces and 3 fleet vehicle spaces, for a net total of 209 spaces. The applicant has requested a quantity modification as per the standards provided in MMC Subsection 19.605.2. The quantity modification request is addressed in Finding 5-d-(2).

The Planning Commission finds that, with the approved modification discussed in Finding 5-d-(2), this standard is met.

(2) MMC Subsection 19.605.2 Quantity Modifications and Required Parking Determinations

MMC 19.605.2 establishes a process for modifying parking requirements and determining the requirements for uses not similar to those listed in the table.

(a) MMC 19.605.2.B Application

An application for determination of parking ratios shall be reviewed as a Type II land use decision, and shall include a description of the site and site users, factors such as proximity of transit and parking demand

management programs, data and analysis to support the determination request, and proposal of a minimum and/or maximum ratio.

The applicant has submitted a narrative that includes the information required by MMC 19.605.2.B, including a proposed maximum ratio of 3.6 spaces per 1,000 sq ft of office space.

(b) MMC 19.605.2.C Approval Criteria

MMC 19.605.2.C.1 establishes the approval criteria for parking quantity modification requests, with an allowance for placing conditions of approval to ensure compliance with the parking determination. In addition, MMC 19.605.2.C.3 provides other specific criteria for requests to increase the maximum amount of parking allowed for a particular use.

- (i) *Demonstration that the proposed parking quantities are reasonable, based on existing parking demand for similar use in other locations, parking quantity requirements for the use in other jurisdictions, and professional literature about the parking demands of the proposed use.*

The applicant's narrative provides a comparison of the maximum allowed parking ratio for general office uses established in MMC Table 19.605.1 (3.4 spaces per 1,000 sq ft) with the ratios from several other nearby jurisdictions. In general, the Milwaukie maximum allowance is more restrictive than that from other nearby jurisdictions, which allow up to 4.1 spaces per 1,000 sq ft.

The applicant has noted that the professional literature on this specific issue is inconclusive and indicates a wide variety of standards across the country. There is some description of a growing trend in office uses using smaller and more collaborative work spaces. As more employees occupy a fixed square footage, the potential demand for parking increases and the existing ratio fails to adequately address this reality.

Moda Health is not increasing the building square footage, though the company is experiencing growth in employment. Despite the company's efforts to manage parking demand—by offering transit pass subsidies, providing designated carpool parking near the building entrance, and providing bicycle parking and amenities—the applicant reports a continued increase in parking demand and that the existing parking areas are filled to capacity. The proposed increase in parking ratio (to 3.6 spaces per 1,000 sq ft) is a reasonable request.

- (ii) *The proposed development has unique or unusual characteristics that create a higher-than-typical parking demand.*

The office use at the subject property includes a call-center aspect, which does not require as large a workstation area for each employee as might be necessary with other office types. According to the applicant's materials, Moda Health has a very high ratio of employees to square footage, at approximately 4.19 employees per 1,000 sq ft. The applicant anticipates adding 36 more employees in the office, which would raise the ratio to approximately 4.8 employees per 1,000

sq ft. This is much higher than the 3.4 ratio established in MMC Table 19.605.1.F as the maximum number of spaces allowed for general office uses.

There is only one transit service near the subject property, a TriMet bus line (#70) with a stop on 17th Ave in front of the office building. There are approximately 10 other bus lines that utilize the Milwaukie transit center located on SE Jackson Street (between SE Main Street and SE 21st Avenue), but none of those lines have stops any closer to the subject property than approximately 1,500 ft (more than one-quarter mile). The route 70 bus provides 20-minute peak hour service, but it is the only line within one-quarter mile that does so. The nearest station for the new light rail line opening in 2015 is in the south downtown area, approximately one-half mile from the subject property. Despite the company's provision of subsidies for transit passes, mass transit is not conveniently located to significantly reduce parking demand.

- (iii) *The parking demand cannot be accommodated by shared or joint arrangements or by increasing the supply of spaces that are parking exempt from the maximum amount of parking allowed under Subsection 19.605.3.A.*

Moda Health currently purchases 30 monthly parking permits for employees to use any of the designated public parking lots downtown. The nearest of those public lots, at SE McLoughlin Boulevard and SE Harrison Street, is approximately 1,000 ft from the subject property. There are no private lots within 1,000 ft of the subject property that are available for shared parking, as established in MMC Subsection 19.605.4.

As proposed, the site will provide a total of 24 carpool spaces, which represents 20% of the minimum required number of spaces as discussed in Finding 5-d-(1). That number is approximately 10% of the total number of spaces that will be provided on the entire site. The applicant would have to designate 12 more carpool spaces to stay below the maximum of 197 allowed by the parking code. The total of 36 carpool spaces would equate to 30% of the minimum number required and 15% of the total spaces on site. It would be highly unusual to require more than 10% carpool parking on the site in order to meet parking demand.

- (iv) *The requested increase is the smallest increase needed based on the specific circumstances of the use and/or site.*

According to MMC Table 19.605.1, the maximum number of spaces allowed for general office uses is 3.4 spaces per 1,000 sq ft of floor area. Considering the exemptions to the maximum parking allowance provided in MMC Subsection 19.605.3.A, the applicant is proposing a net total of 209 spaces, or 3.6 spaces per 1,000 sq ft of office use. Given the large size of the overall site (approximately 314,000 sq ft, or 7.2 acres), the high ratio of employees per square foot within the office building, and the applicant's efforts to reduce parking demand including by providing transit passes, bicycle parking, and promotion

of carpooling, the requested increase represents the smallest increase needed.

The Planning Commission finds that the approval criteria of MMC 19.605.2.C.1 are met.

The Planning Commission finds that the applicant's request to increase the maximum allowed number of off-street parking spaces for the existing office use, from 3.4 spaces per 1,000 sq ft of area to 3.6 spaces, is approved.

(3) MMC Subsection 19.605.3 Exemptions and By-Right Reductions to Quantity Requirements

MMC 19.605.3.A provides exemptions to the maximum quantity of parking allowed, including for employee carpool parking and fleet parking.

The proposed development will result in a gross total of 236 parking spaces. Twenty-four spaces will be reserved for carpool vehicles and 3 spaces will be reserved for fleet vehicles.

As proposed, the Planning Commission finds that the proposed development will result in net total of 209 spaces that count toward the maximum allowed parking quantity.

e. MMC Section 19.606 Parking Area Design and Landscaping

(1) MMC Subsection 19.606.1 Parking Space and Aisle Dimension

MMC 19.606.1 establishes dimensional standards for required off-street parking spaces and drive aisles. For perpendicular (90-degree-angle) spaces, the minimum width is 9 ft and minimum depth is 18 ft, with 22-ft-wide drive aisles.

The applicant has submitted a parking plan that utilizes perpendicular spaces. As proposed, the dimensions for new or reconfigured spaces and drive aisles appear to meet the minimum standards. A condition has been established to ensure that the final construction plans provided at the time of development permit submittal demonstrate that the applicable dimensional requirements are met.

As conditioned, this standard will be met.

(2) MMC Subsection 19.606.2 Landscaping

MMC 19.606.2 establishes standards for parking lot landscaping, including for perimeter and interior areas. The purpose of these landscaping standards is to provide buffering between parking areas and adjacent properties, break up large expanses of paved area, help delineate between parking spaces and drive aisles, and provide environmental benefits such as stormwater management, carbon dioxide absorption, and a reduction of the urban heat island effect.

(a) MMC 19.606.2.C Perimeter Landscaping

In downtown zones, perimeter landscaping areas are not required along lot lines abutting other properties but must be at least 4 ft wide when abutting a right-of-way. At least 1 tree must be planted for every 40 lineal feet of landscaped buffer area, with the remainder of the buffer planted with grass, shrubs, ground cover, mulch, or other landscaped treatment.

As proposed, perimeter landscaping areas near the public right-of-way along SE Riverway Lane are more than 20 ft wide and will have at least 1 tree planted every 40 lineal feet.

This standard is met.

(b) MMC 19.606.2.D Interior Landscaping

At least 25 sq ft of interior landscaped area must be provided for each parking space. Planting areas must be at least 120 sq ft in area, at least 6 ft in width, and dispersed throughout the parking area. For landscape islands, at least 1 tree shall be planted per island, with the remainder of the buffer planted with grass, shrubs, ground cover, mulch, or other landscaped treatment.

As a result of the proposed development, the western parking area will provide 180 spaces, which require a total of 4,500 sq ft of interior landscaping. As proposed, interior landscaping areas appear to provide at least 4,500 sq ft of area. New interior landscaping areas appear to be at least 6 ft wide and at least 120 sq ft in area, with at least 1 tree planted per island. A condition has been established to ensure that the final construction plans provided at the time of development permit submittal demonstrate that the applicable interior landscaping requirements are met.

As conditioned, this standard will be met.

(c) MMC 19.606.2.E Other Parking and Landscaping Provisions

Preservation of existing trees in the off-street parking area is encouraged and may be credited toward the total number of trees required. Parking area landscaping must be installed prior to final inspection, unless a performance bond is posted with the City. Required landscaping areas may serve as stormwater management facilities, and pedestrian walkways are allowed within landscape buffers if the buffer is at least 2 ft wider than required by MMC 19.606.2.C and 19.606.2.D.

The applicant has submitted a parking plan that preserves some existing trees within the western parking area. As required, parking area landscaping will be installed prior to final inspection, unless a bond is posted with the City.

This standard is met.

(3) MMC Subsection 19.606.3 Additional Design Standards

MMC 19.606.3 establishes various design standards, including requirements related to paving and striping, wheel stops, pedestrian access, internal circulation, and lighting.

(a) MMC 19.606.3.A Paving and Striping

Paving and striping are required for all required maneuvering and standing areas, with a durable and dust-free hard surface and striping to delineate spaces and directional markings for driveways and accessways.

As proposed, the western parking area will be paved and striped, including designations for carpool spaces and fleet vehicle spaces.

This standard is met.

(b) MMC 19.606.3.B Wheel Stops

Parking bumpers or wheel stops are required to prevent vehicles from encroaching onto public right-of-way, adjacent landscaped areas, or pedestrian walkways. Curbing may substitute for wheel stops if vehicles will not encroach into the minimum required width for landscape or pedestrian areas.

As proposed, curbing along the newly expanded perimeter of the western parking area will be provided in place of wheel stops. The width of all perimeter landscaping areas is at least 6 ft, which provides the required 4-ft width plus an additional 2 ft, sufficient to prevent vehicles from encroaching into the minimum required perimeter landscaping width in downtown zones.

This standard is met.

(c) MMC 19.606.3.C Site Access and Drive Aisles

Accessways to parking areas shall be the minimum number necessary to provide access without inhibiting safe circulation on the street. Drive aisles shall meet the dimensional requirements of MMC 19.606.1.

The existing western parking area has only one access onto a public street, onto SE Lava Drive at the north end of the lot. No new accessways are proposed. As proposed, all new drive aisles, as well as drive aisles modified by the proposed development, appear to meet the minimum dimensional requirements. A condition has been established to ensure that the final construction plans provided at the time of development permit submittal demonstrate that all new and modified drive aisles meet the applicable requirements of Subsection 19.606.1.

As conditioned, this standard will be met.

(d) MMC 19.606.3.D Pedestrian Access and Circulation

Pedestrian access shall be provided so that no off-street parking space is further than 100 ft away, measured along vehicle drive aisles, from a building entrance or a walkway that is continuous, leads to a building entrance, and meets the design standards of Subsection 19.504.9.E.

As proposed, none of the new or modified parking spaces will be further than 100 ft from a pedestrian walkway. All new and modified pedestrian walkways will be reviewed through the development permitting process to ensure compliance with the various applicable standards established in Subsections 19.606.3.D and 19.504.9.E.

This standard is met.

(e) MMC 19.606.3.E Internal Circulation

The Planning Director has the authority to review the pedestrian, bicycle, and vehicular circulation of the site and impose conditions to ensure safe and efficient on-site circulation. Such conditions may include, but are not limited to, on-site signage, pavement markings, addition or modification of curbs, and modification of drive aisle dimensions.

The Planning Director has reviewed the proposed parking plan and determined that no additional requirements are necessary to ensure safe and efficient on-site circulation.

This standard is met.

(f) MMC 19.606.3.F Lighting

Lighting is required for parking areas with more than 10 spaces and must have a cutoff angle of 90 degrees or greater to ensure that lighting is directed toward the parking surface. Lighting shall not cause a light trespass of more than 0.5 footcandles measured vertically at the boundaries of the site, and shall provide a minimum illumination of 0.5 footcandles for pedestrian walkways in off-street parking areas. Where practicable, lights shall be placed so they do not shine directly into any Water Quality Resource (WQR) and/or Habitat Conservation Area (HCA) location.

The proposed development will result in a total of 180 parking spaces in the western off-street parking area, which has existing lighting fixtures. As discussed in Finding 6-d, lighting will be provided in new and modified parts of the western parking area, consistent with the existing lighting style. All new lighting will be reviewed through the development permitting process to ensure compliance with the various applicable illumination standards established in Subsection 19.606.3.F.

This standard is met.

As conditioned, the applicable additional design standards of MMC 19.606.3 will be met.

As conditioned, the Planning Commission finds that the applicable design and landscaping standards of MMC 19.606 will be met.

f. MMC Section 19.608 Loading

MMC 19.608 establishes standards for off-street loading areas. For nonresidential buildings with more than 50,000 sq ft of total floor area, a minimum of 2 loading spaces are required, with minimum dimensions of 35 ft long and 10 ft wide.

The existing office building is approximately 58,500 sq ft. There are two existing loading spaces in the western parking area, each approximately 30 ft long and 12 ft wide. When the parking area was developed in 2000, the City's dimensional standards for off-street loading areas were 30-ft length and 12-ft width. The proposed development will not change these existing, nonconforming loading spaces and does not include any new loading spaces.

The Planning Commission finds that this standard is met.

g. MMC Section 19.609 Bicycle Parking

MMC 19.609 establishes standards for bicycle parking, including a requirement to provide a number of bicycle spaces equal to at least 10% of the minimum number of required vehicle spaces. If more than 10 bicycle spaces are required, then a minimum of 50% of the bicycle spaces must be covered and/or enclosed in lockers or a secure room.

As discussed in Finding 5-d-(1), the existing office use requires a minimum of 117 vehicle spaces; a minimum of 12 bicycle spaces are required. According to the applicant's materials, the existing development provides 29 bicycle spaces, including an exterior 5-space rack near the southern entrance to the building and an interior 24-space storage room within the building. No additional bicycle parking is proposed.

The Planning Commission finds that this standard is met.

h. MMC Section 19.610 Carpool and Vanpool Parking

MMC 19.610 establishes parking standards for vehicles used to carpool. For new development that requires at least 20 parking spaces, a minimum of 10% of the minimum amount of required parking shall be dedicated to carpool or vanpool vehicles. Carpool spaces shall be located closer to the main entrances of the building than other employee parking and shall be clearly designated with signage or pavement markings.

As discussed in Finding 5-d-(1), the existing office use requires a minimum of 117 vehicle spaces; a minimum of 12 carpool spaces are required. The existing development provides 6 carpool spaces; the proposed improvements will add 18 more carpool spaces near the building entrance, for a total of 24 designated carpool spaces on the site.

The Planning Commission finds that this standard is met.

6. MMC Section 19.907 Downtown Design Review

MMC 19.907 establishes the procedures and standards for design review in the downtown zones. The purpose of downtown design review is to preserve and enhance the character of downtown Milwaukie; to ensure a degree of order, harmony, and quality in the downtown zones; and to ensure that new development and enlargement of existing development are consistent with the Downtown Design Guidelines.

a. MMC Subsection 19.907.2 Applicability

MMC 19.907.2 establishes that all new construction and changes to buildings and/or properties in the downtown zones is subject to design review.

The proposed development involves expansion of an existing parking area on the subject property, which is a change to the subject property.

The Planning Commission finds that the proposed development is subject to design review in accordance with the procedures provided in MMC Subsection 19.907.5.

b. MMC Subsection 19.907.5 Application Procedure

MMC 19.907.5 establishes the procedures by which applications for design review shall be processed. As per MMC 19.907.5.C, major exterior alterations are subject to Type III review. The Planning Director has determined that new construction is similar to a major exterior alteration with respect to the necessary level of review.

The Planning Commission finds that, as new construction, the proposed development is subject to Type III downtown design review.

c. MMC Subsection 19.907.6 Application

MMC 19.907.6 establishes the requirements for downtown design review applications, including a completed design review checklist, written statement

describing how the proposal meets applicable design guidelines, and site plan showing the proposed development.

The applicant's submittal includes a completed design review checklist, narrative describing the proposed development and addressing applicable criteria, and site plans showing the proposed improvements.

The Planning Commission finds that this standard is met.

d. MMC Subsection 19.907.7 Approval Criteria for Design Review

MMC 19.907.7 establishes the approval criteria for design review applications and the process for modifications to the downtown design standards. The approval authority may approve, approve with conditions, or deny a design review application based on the following criteria:

(1) *Compliance with Title 19 Zoning Ordinance*

The proposed development does not involve construction of a building. No standards from MMC 19.304 Downtown Zones are applicable to the project.

The Planning Commission finds that this approval criterion has been met.

(2) *Substantial consistency with the Downtown Design Guidelines*

Refer to Table 1 below for detailed findings.

The Planning Commission finds that the proposal is substantially consistent with the Downtown Design Guidelines and that this approval criterion has been met.

(3) *Submittal of a complete application and applicable fee as adopted by the City Council*

The applicant submitted an application on April 11, 2014, and it was deemed complete on April 25, 2014. The applicable design review application fee was paid April 11, 2014.

The Planning Commission finds that this approval criterion has been met.

The Planning Commission finds that the approval criteria for downtown design review are met.

Table 1. Design Review Compliance

MILWAUKIE CHARACTER GUIDELINES	
Guideline	Recommended Findings
<p><i>Promote Architectural Compatibility</i></p> <p><i>Buildings should be “good neighbors.” They should be compatible with surrounding buildings by avoiding disruptive excesses. New buildings should not attempt to be the center of attention.</i></p>	<p>The proposed development is not a building, but it involves expansion of a parking area that is a significant accessory feature of the existing building. The expansion is limited in scale, with new disturbance concentrated in a portion of the site that does not affect many adjacent properties. Landscaping will be installed to provide screening from adjacent properties and to maintain consistency with the character of existing Moda Health development and adjacent areas.</p>

	The proposed development meets this guideline.
<i>(9 other guidelines related to Milwaukie Character)</i>	None of the other Milwaukie Character guidelines are applicable to this project.

PEDESTRIAN EMPHASIS GUIDELINES

Guideline	Recommended Findings
<i><u>Reinforce and Enhance the Pedestrian System</u></i> <i>Barriers to pedestrian movement and visual and other nuisances should be avoided or eliminated, so that the pedestrian is the priority in all development projects.</i>	The proposed development will extend the existing on-site pedestrian walkway to connect the new parking spaces with the office building. The proposed development meets this guideline.
<i><u>Integrate Barrier-Free Design</u></i> <i>Accommodate handicap access in a manner that is integral to the building and public right-of-way.</i>	The proposed development includes the provision of one new ADA parking space, with accompanying signage, side aisle, and access ramp. The site currently provides 4 ADA spaces, 2 each near the east and west entrances to the office building. The proposed development meets this guideline.
<i>(4 other guidelines related to Pedestrian Emphasis)</i>	None of the other Pedestrian Emphasis guidelines are applicable to this project.

ARCHITECTURE GUIDELINES

Guideline	Recommended Findings
<i>(12 guidelines related to Architecture)</i>	The proposed development is an expansion of an existing off-street parking area and does not involve a building. The Architecture guidelines refer to buildings and are not applicable to this project.

LIGHTING GUIDELINES

Guideline	Recommended Findings
<i><u>Parking Lot Lighting</u></i> <i>Ornamental street lights should be used to be compatible with downtown streetlight standards identified in the Public Area Requirements.</i>	The existing parking area was established in 2000, prior to the adoption of the Downtown Design Guidelines in 2003. The existing lighting fixtures do not match the ornamental style prescribed in the Downtown Design Guidelines. However, the site is not physically connected to the central downtown area, where other ornamental lights have been installed. To require the new lights to be a different style than the existing lights would promote incongruous design on the site. Furthermore, the ornamental style of

	light does not avail itself to shielding for limiting light trespass into the natural resource areas on the site adjacent to the parking area. The proposed development meets this guideline with the style of lights matching the existing lights.
(3 other guidelines related to Lighting)	None of the other Lighting guidelines are applicable to this project.

SIGN GUIDELINES	
Guideline	Recommended Findings
(7 guidelines related to Signs)	No signs are proposed as part of this application. The Sign guidelines are not applicable to this project.

7. MMC Section 19.401 Willamette Greenway Zone

MMC 19.401 establishes standards for the Willamette Greenway overlay designation. The subject property is within the Willamette Greenway zone as shown on the City's zoning map.

a. MMC Subsection 19.401.5 Procedures

MMC 19.401.5 establishes procedures related to proposed uses and activities in the Willamette Greenway zone. Development in the Willamette Greenway zone requires conditional use review, subject to the standards of MMC Section 19.905 and in accordance with the approval criteria established in MMC Subsection 19.401.6.

The project involves the substantial alteration of natural site characteristics and constitutes "development" as defined in MMC Subsection 19.401.4. The proposed development is subject to conditional use review standards of MMC 19.905 and the approval criteria of MMC 19.401.6.

b. MMC Subsection 19.401.6 Criteria

MMC 19.401.6 establishes the criteria for approving conditional uses in the Willamette Greenway zone.

(1) *Whether the land to be developed has been committed to an urban use, as defined under the State Willamette River Greenway Plan*

The State Willamette River Greenway Plan defines "lands committed to urban use" as "those lands upon which the economic, developmental and locational factors have, when considered together, made the use of the property for other than urban purposes inappropriate. Economic, developmental and locational factors include such matters as ports, industrial, commercial, residential or recreational uses of property; the effect these existing uses have on properties in their vicinity, previous public decisions regarding the land in question, as contained in ordinances and such plans as the Lower Willamette River Management Plan, the city or county comprehensive plans, and similar public actions."

The subject property is zoned for Downtown Office use and is already developed with an office building and two adjacent off-street parking areas. The land is committed to an urban use.

- (2) *Compatibility with the scenic, natural, historic, economic, and recreational character of the river*

The project area is at least 250 ft from the nearest point of the river, with at least one tier of residences between. The site is lower in elevation than the properties between it and the river, and the river is not visible from the project area. Two large trees that will be removed may be partially visible from the river; however, most of the structure of those trees is obscured from the river by adjacent development, and many other large trees on the site will remain. The proposed development presents no significant impacts to the character of the river and is compatible.

- (3) *Protection of views both toward and away from the river*

The ground level of the project area is not visible from the river, due to topography and the existence of residential development between the site and the river. The removal of two large trees for the project will not significantly affect visual corridors to the river, as other large trees will remain on the site.

- (4) *Landscaping, aesthetic enhancement, open space, and vegetation between the activity and the river, to the maximum extent practicable*

The project area is separated from the river by residential development to the west. Landscaping will be provided between the newly expanded portion of the parking area and the adjacent residential properties, as required by MMC Section 19.606 Parking Area Design and Landscaping.

- (5) *Public access to and along the river, to the greatest possible degree, by appropriate legal means*

The subject property is not adjacent to the river and does not limit public access to the river.

- (6) *Emphasis on water-oriented and recreational uses*

The site is at least 250 ft from the nearest point of the river. The existing office use is not water-oriented and is not directed toward the river.

- (7) *Maintain or increase views between the Willamette River and downtown*

The proposed development will remove two large trees from the site, which will have little or no effect on views between the river and downtown.

- (8) *Protection of the natural environment according to regulations in Section 19.402*

The proposed development will disturb 10% or less of the designated Habitat Conservation Area (HCA) on the site. According to the provisions of MMC Subsection 19.402.11.D.1.b, that amount of HCA disturbance is allowed by right, subject to the mitigation requirements of MMC 19.402.11.D.2. The evaluation of impacts and required mitigation will be handled during the development review stage of the process.

- (9) *Advice and recommendations of the Design and Landmark Committee, as appropriate*

The Design and Landmarks Committee (DLC) has reviewed the proposal and recommends approval of the project as proposed.

(10) Conformance to applicable Comprehensive Plan policies

The Willamette Greenway Element in the Milwaukie Comprehensive Plan includes policies related to land use, public access and view protection, and maintenance of private property. These policies include the requirement of a conditional use permit for new development and intensification of existing uses; encouragement for uses that are not water-dependent or water-related to be directed away from the river; evaluation of development impacts to visual corridors; and limitations on authorizing the unrestricted public use of private land.

The proposed development is being reviewed through the conditional use process. The existing approved use is not water-related and is not directed toward the river; the proposed development will not affect that status. No public access through the subject property is proposed, and no public access will be required as a condition of approval of the proposed development.

(11) The request is consistent with applicable plans and programs of the Division of State Lands

The proposed development is not inconsistent with any known plans or programs of the Department of State Lands.

(12) A vegetation buffer plan meeting the conditions of Subsections 19.401.8.A through C

The subject property is more than 250 ft from the river, more than 225 ft beyond the 25-ft buffer prescribed by MMC 19.401.8.

The Planning Commission finds that the proposed development meets all relevant approval criteria provided in MMC 19.401.6.

The Planning Commission finds that the proposed development meets all applicable standards of the Willamette Greenway zone.

8. MMC Section 19.905 Conditional Uses

MMC 19.905 establishes regulations for conditional uses, including standards for reviewing modifications to existing conditional uses.

a. MMC Subsection 19.905.3 Review Process

MMC 19.905.3 establishes the process by which a new conditional use, or a major or minor modification of an existing conditional use, must be reviewed.

As noted in Finding 6-a, the proposed development is an activity within the Willamette Greenway zone that requires review as a conditional use. The existing use on the subject property is a professional office with associated parking areas and was approved through the Willamette Greenway conditional use process in 2000 (land use master file #MU-00-01). The proposed development, which involves increasing the amount of off-street parking on the site beyond the maximum allowed by MMC Section 19.605 for the square footage of use, represents a major modification to the existing use.

MMC 19.905.3.A requires that a major modification of an existing conditional use be evaluated through the Type III review process per MMC Section 19.1006.

b. MMC Subsection 19.905.4 Approval Criteria

MMC 19.905.4.A establishes the general criteria for approval of a new conditional use or a major modification to an existing conditional use.

- (1) *The characteristics of the lot are suitable for the proposed use considering size, shape, location, topography, existing improvements, and natural features.*

The subject property is approximately 314,125 sq ft (7.2 acres) in size. The property is developed with an office building approximately 58,500 sq ft in area and two off-street parking areas that comprise approximately 85,000 sq ft. The proposed expansion of the western parking area will consume approximately 10,000 sq ft of additional site area.

Johnson Creek crosses through the southeastern portion of the property, with the developed areas located in the northern and northwestern portions of the property. The proposed development will tie in to existing improvements on the site and will maintain at least 50 ft between the edge of the expanded parking area and the adjacent stream.

The Planning Commission finds that this standard is met.

- (2) *The operating and physical characteristics of the proposed use will be reasonably compatible with, and have minimal impact on, nearby uses.*

The existing western parking area is adjacent to single-family residences along its western edge (SE Riverway Lane), with screening provided by a combination of evergreen trees and shrubs. The proposed development will not affect the existing landscaping but will extend slightly toward single-family residences along the southern boundary of the site. Additional landscaping will be provided, sufficient to meet the requirements of MMC Section 19.606 Parking Area Design and Landscaping, including the provision of a continuous visual screen in the perimeter landscaping areas that abut a residential use.

The Planning Commission finds that this standard is met.

- (3) *All identified impacts will be mitigated to the extent practicable.*

The primary impact of the proposed development will be the effect of extending the parking use closer to one or two adjacent residential properties across Riverway Ln. No new access points to the existing parking area will be opened. Stormwater runoff from newly paved areas will be treated on site according to the City's stormwater standards. As noted in Finding 7-b-2, landscape screening will be provided. As required by MMC 19.606.3.F, new parking lot lighting will be directed toward the parking area and shielded to limit light trespass.

The Planning Commission finds that this standard is met.

- (4) *The proposed use will not have unmitigated nuisance impacts, such as from noise, odor, and/or vibrations, greater than usually generated by uses allowed outright at the proposed location.*

The proposed development will add to the number of parking spaces on the site but will not generate any significant nuisance impacts that will not be mitigated or are otherwise understood to be associated with off-street parking, which is an allowed activity that is accessory to the allowed office use.

The Planning Commission finds that this standard is met.

- (5) *The proposed use will comply with all applicable development standards and requirements of the base zone, any overlay zones or special areas, and the standards in Section 19.905.*

As addressed in various other findings, the proposed development will comply with all applicable development standards, requirements of the underlying Downtown Office zone and other applicable overlay zones, and the standards of MMC 19.905.

The Planning Commission finds that this standard is met.

- (6) *The proposed use is consistent with applicable Comprehensive Plan policies related to the proposed use.*

As addressed in Finding 6-b-10, the proposed development is consistent with all relevant polices in the Comprehensive Plan.

The Planning Commission finds that this standard is met.

- (7) *Adequate public transportation facilities and public utilities will be available to serve the proposed use prior to occupancy pursuant to Chapter 19.700.*

The Engineering Department has reviewed the proposal and confirmed that existing public transportation facilities and public utilities are adequate to serve the proposed development.

The Planning Commission finds that this standard is met.

The Planning Commission finds that the proposed development meets all of the approval criteria outlined in MMC 19.905.4.A for a major modification to an existing conditional use.

c. MMC Subsection 19.905.5 Conditions of Approval

MMC 19.905.5 establishes the types of conditions that may be imposed on a conditional use to ensure compatibility with nearby uses. Conditions may be related to a number of issues, including access, landscaping, lighting, and preservation of existing trees.

The Planning Commission finds that, as proposed, the new development sufficiently mitigates any negative impacts as proposed and that no additional conditions are necessary to ensure compatibility with nearby uses.

d. MMC Subsection 19.905.6 Conditional Use Permit

MMC 19.905.6 establishes standards for issuance of a conditional use permit, including upon approval of a major modification of an existing conditional use. The provisions include a requirement to record the conditional use permit with the Clackamas County Recorder's Office and provide a copy to the City prior to commencing operations allowed by the conditional use permit.

The Planning Commission finds that the proposed development is consistent with the relevant standards established in MMC 19.905 for conditional uses.

9. MMC Chapter 19.700 Public Facility Improvements

MMC 19.700 establishes provisions to ensure that development provides public facilities that are safe, convenient, and adequate in rough proportion to their public facility impacts. MMC 19.702 establishes the applicability of the provisions of MMC 19.700, including land

divisions, new construction, expansions of existing structures, and changes or intensifications in use.

The proposed development consists of expansion of an existing parking area, which in and of itself does not increase the vehicle trip generation to and from the site. The Engineering Department has reviewed the application and finds that MMC 19.700 does not apply to this application.

The Planning Commission finds that the standards and requirements of MMC 19.700 are not applicable to the proposed development.

10. The application was referred to the following departments and agencies on April 29, 2014:

- Milwaukie Building Department
- Milwaukie Engineering Department
- Clackamas Fire District #1
- Historic Milwaukie Neighborhood District Association Chairperson and Land Use Committee
- Milwaukie Design and Landmarks Committee
- Oregon Department of Transportation (ODOT)

In addition, notice of the application was provided to the Oregon Department of Parks and Recreation, Department of State Lands, Oregon Department of Fish and Wildlife, and State Marine Board.

The comments received are summarized as follows:

- a. **John Stelzenmueller, Milwaukie Building Department:** No comments on the proposal at this time.
- b. **Philip Kolb, Milwaukie Engineering Department:** Comments related to MMC Chapter 19.700 Public Facility Improvements, which is not applicable to the proposed development. Those comments have been incorporated into these findings. Other requirements related to stormwater management have been noted with the conditions of approval.
- c. **Jean Baker, Historic Milwaukie NDA Land Use Committee Chair:** The Historic Milwaukie NDA has no objection to the proposal.
- d. **Shawn Olson, Clackamas Fire District:** Comments related to the need for “No Parking Fire Lane” signs at appropriate locations and for turning radius dimensions on plans submitted with development permits. Those comments have been noted with the conditions of approval.
- e. **Seth Brumley, ODOT:** No comments on this proposal.

**Recommended Conditions of Approval
File #s P-14-01, DR-14-03, and WG-14-01
Moda Health Parking Expansion**

Conditions

1. At the time of submission of the associated development permit application, the following shall be resolved:
 - a. Final plans submitted for building permit review shall be in substantial conformance with plans approved by this action, which are the plans stamped received by the City on April 25, 2014, and except as otherwise modified by these conditions. The required changes are as follows:
 - (1) As per Finding 5-e, provide final construction plans for the western parking lot, with any revisions as necessary to demonstrate that the applicable design and landscaping standards of MMC Section 19.606 are met, including but not limited to parking space and drive aisle dimensions and interior landscaping. The final construction plans shall be accompanied by a narrative explaining how the applicable standards are met.
 - b. Provide a narrative describing all actions taken to comply with these conditions of approval.
 - c. Provide a narrative describing any changes made after the issuance of this land use decision that are not related to these conditions of approval.

Additional Requirements

1. An application for Type I development review is required in conjunction with the submittal of the associated development permit application.
2. Stormwater Management

Submit a stormwater management plan prepared by a qualified professional engineer with required development/building permits as part of the proposed development. The plan shall conform to Section 2 – Stormwater Design Standards of the City of Milwaukie Public Works Standards.

 - a. The stormwater management plan shall demonstrate that the post-development runoff does not exceed the pre-development, including any existing stormwater management facilities serving the development site.
 - b. The stormwater management plan shall demonstrate compliance with water quality standards in accordance with the City of Portland Stormwater Management Manual.

Development/building permits will not be issued for construction until the stormwater management plan has been approved by the City of Milwaukie.
3. Fire Apparatus Access
 - a. “No Parking Fire Lane” signs shall be placed on one or both sides of a fire apparatus access road when road widths are less than 26 ft wide and 26 ft to 32 ft in width. Signs shall be placed on both sides of a fire apparatus access road when roads are less than 26 ft in width and on one side when roads are 26 ft to 32 ft in width. Red painted curbs can be used in lieu of signs.

- b. Provide turning radius dimensions on submitted plans. The inside turning radius and outside turning radius for a 20-ft-wide road shall be not less than 28 ft and 48 ft, respectively, measured from the center point.

4. Limitations on Development Activity

Development activity on the site shall be limited to 7 a.m. to 10 p.m. Monday through Friday and 8 a.m. to 5 p.m. Saturday and Sunday, as per MMC Subsection 8.08.070(I).

5. Prior to final inspection for the associated development permit, the following shall be resolved:

- a. The applicant shall demonstrate that parking area landscaping has been installed sufficient to meet the standards of MMC Subsection 19.606.2, including the provision of a continuous visual screen in the perimeter landscaping areas that abut a residential use. As per MMC Subsection 19.606.2.E.2, a performance bond may be posted with the City in lieu of planting installation, as a temporary measure for no more than 6 months.
- b. The applicant shall demonstrate that the new parking lot lighting meets the standards of MMC Subsection 19.606.3.F. This includes ensuring that the new lighting has a cutoff angle of 90 degrees or greater, does not cause a light trespass of more than 0.5 footcandles measured vertically at the boundaries of the site, provides a minimum illumination level of 0.5 footcandles for pedestrian walkways in the new and modified parking area, and does not shine directly into any WQR and/or HCA location.
- c. The applicant shall record the conditional use permit issued by the City upon approval of the proposed development. The conditional use permit shall be recorded with the Clackamas County Recorder's Office, and a copy of the recorded permit shall be provided to the Planning Director.

6. Expiration of Approval

As per MMC 19.1001.7.E.1.a, proposals requiring any kind of development permit must complete both of the following steps:

- a. Obtain and pay for all necessary development permits and start construction within 2 years of land use approval.
- b. Pass final inspection and/or obtain a certificate of occupancy within 4 years of land use approval.

As per MMC 19.1001.7.E.2.b, land use approvals shall expire unless both steps noted above have been completed or unless the review authority specifies a different expiration date in the land use decision to accommodate large, complex, or phased development projects.

**MODA HEALTH – PARKING REVISIONS
TYPE III DOWNTOWN DESIGN REVIEW AND
WILLAMETTE RIVER GREENWAY REVIEW**

City of Milwaukie, Oregon

Prepared for

MODA Health

Prepared by

Otak, Inc.



HanmiGlobal Partner

April 11, 2014

(April 25, 2014 Resubmittal)

Project No. 17134

PROPOSAL SUMMARY

REQUEST: Type III Downtown Design Review, Parking Determination, and Willamette River Greenway Conditional Use review approval are requested for the plans for Moda Health to modify and expand the existing western parking lot at their office building by 30 parking spaces.

The western parking lot currently has 150 parking spaces. Another 56 spaces are located in the facility's eastern parking lot which is located between the building and SE 17th Avenue. The proposed revisions and expansion to the western parking lot would result in a total of 180 parking spaces in the western lot, and a total of 236 parking spaces overall.

A Parking Quantity Modification to the maximum allowed parking standard of Milwaukie Zoning Ordinance (MZO) 19.600 Table 19.605.1 is requested to allow the existing Moda Health operations to be served with parking at a maximum 3.6 parking spaces per 1,000 square feet of office building ratio instead of the normal Code allowed maximum ratio of 3.4 spaces per 1,000 square feet.

LOCATION: 10505 SE 17th Avenue. SE Lava Drive is immediately north of the site. Tax lot 3500 of Clackamas County assessors map, T1S R1E WM 35AA

ZONING: Downtown Office (DO) zone
Willamette River Greenway overlay zone (a portion of the site, only)
Natural Resource overlay zone (a portion of the site, only)

APPLICANT: Moda Health
601 SW 2nd Avenue
Portland, OR 97204-3156

Contact: David Shaffer
(503) 948-5564
david.shaffer@Modahealth.com

OWNER: Same

**ARCHITECT/
PLANNER:** Otak, Inc.
808 SW Third Avenue, Suite 300
Portland, OR 97204

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TABLE OF CONTENTS

I.	PROJECT DESCRIPTION	5
II.	EXISTING CONDITIONS	6
III.	COMPLIANCE with APPLICABLE APPROVAL CRITERIA.....	6
	A. Compliance with Development Standards of Downtown Zones - Chapter 19.304	6
	B. Compliance with Section 19.600 - Parking.....	7
	C. Compliance with Chapter 19.500 Supplementary Development Regulations	13
	D. Compliance with Chapter 19.401 Willamette Greenway Zone	13
IV.	CONSISTENCY with APPLICABLE DOWNTOWN DESIGN GUIDELINES.....	18
V.	CONCLUSION	19
VI.	APPENDICES	
	A. Pre-application	
	B. Conference Notes	
	C. City of Milwaukie Notice of Decision – Casefile MU-00-01	
	D. Application forms	
	E. Parking area lights cut sheet	
	F. Existing bicycle parking - photos	
	G. Storm drainage report	
VII.	REDUCED SIZE PLANS	
	Sheet P0.0 – Cover Sheet/Vicinity Map	
	Sheet P1.0 – Existing Conditions (with Tree Survey and Aerial Photo)	
	Sheet P1.1 – Overall Moda Health Site Plan/Aerial photo	
	Sheet P1.2 – Western Parking Lot Site Plan (shows Greenway and Natural Resource overlay zone boundaries)	
	Sheet P2.0 – Site Grading and Tree Removal Plan	
	Sheet L1.0 – Landscape Plan – Western Parking Area	
	Sheet L1.1 – Planting Notes and Details	

10 copies of the plan set have also been submitted in a larger plan format.

I. PROJECT DESCRIPTION

Moda Health requests Type III Downtown Design Review and Willamette River Greenway Conditional Use review approval to redevelop and expand the existing western parking lot at their offices located on SE Lava Drive. The parking lot is located west of the MODA Health building, south of SE Lava Drive, and east of SE Riverway Lane. A portion of the proposed parking lot expansion area is on a portion of the Moda Health site which is zoned with the Willamette Greenway overlay zone.

Currently, the existing eastern and western parking lots at the Moda Health offices include a total of 206 parking spaces. The eastern lot, which is located between the building and SE 17th Avenue, provides 56 parking spaces. The western parking lot includes 150 parking spaces. Moda Health would like to restripe portions of the western parking lot and expand it southward to increase the parking in that lot to 180 parking spaces, or a total of 236 parking spaces on the site.

In order to accomplish this, Moda Health requests approval of a Parking Quantity Modification to the maximum allowed parking standard which allows a maximum of 3.4 parking spaces for every 1,000 square feet of office use. That standard would typically allow a maximum of 199 parking spaces for the 58,529 square feet of office facilities within the MODA Health offices building. The provisions of Section 605.3.A. allows fleet parking spaces and designated carpool parking spaces to be exempted from the calculation of total parking count which is subject to the maximum onsite parking standard. Utilizing those exemptions, there would be a total of 206 parking spaces that would be subject to the maximum parking standard (236 total parking spaces – 2 fleet parking spaces – 24 carpool spaces = 210 parking spaces). Based upon that number, there would be 3.52 parking spaces for every 1,000 square feet of building area – which is excess of the maximum allowed parking standard. Zoning Code Section 605.2 allows the decision-making body to approve modifications to the minimum and maximum parking ratio standards subject criteria included within that section.

Expansion of the western parking lot would encroach 2,646 square feet into the approximately 62,5981 square feet of the designated Natural Resource overlay zone area on the site, and will require the removal of two regulated trees (trees of 6-inches or more caliper dbh) and four relatively young landscape trees. Type I Natural Resource District review will be requested separately to allow for the proposed encroachment into this area, and to specify mitigation planting measures to compensate for that encroachment.

It is anticipated that the proposed parking lot construction will occur in summer 2014.

Sheet P0.0 illustrates the location of the site. Sheet P1.0, Existing Conditions, illustrates the existing site improvements as well as the location, types, and sizes of mature trees. Sheets P2.0 through L1.1 illustrate the proposed development plan.

II. EXISTING CONDITIONS/PROJECT HISTORY

The 8.23-acre site is bounded by SE 17th Avenue on the east, SE Lava Drive on the north, and SE Riverway Lane, a private street, on the west. Johnson Creek is located to the south. The site is tax lot 3500 of Clackamas County Assessors map 1S 1E 35AA.

The site is developed with the two-story, 58,529 square foot MODA Health building, two associated parking lots, and substantial site landscaping. In addition, a wooded area to the south of the western parking lot provides a buffer between the uses on parcel I and single-family homes to the south. A portion of the site is located on the east side of Johnson Creek which passes through the site.

Establishment of Moda Health's health insurance office use of the site was approved by the City of Milwaukie Planning Commission on April 11, 2000 through Casefile MU-00-01 (see Appendix B for a copy of the notice of decision). Moda Health (formerly ODS, i.e., Oregon Dental Service) redeveloped the vacant former Pendleton Woolen Mills industrial building into an office building. The project involved substantial remodeling and three small expansions of the building.

III. COMPLIANCE WITH APPLICABLE APPROVAL CRITERIA OF TITLE 19 – THE MILWAUKIE ZONING ORDINANCE (MZO)

A. Compliance with Development Standards of the Downtown Zones - Chapter 19.304

The subject site is zoned Downtown Office (DO). The purpose of the Downtown Office district is to provide for office, entertainment, and hotel uses along high visibility major arterial streets, according to MZO Section 19-304.A.C. Table 19.304.3 lists primary uses which are intended to be allowed in the DO. Offices, professional and administrative are listed as a permitted use in the DO district. Moda Health's existing insurance administrative office use of the site is therefore consistent with the primary use standards of the DO zoning district. Section 19-304.E allows uses that are accessory to a primary use if they comply with the development standards. The onsite parking areas serving the Moda Health professional office use are an accessory use to the primary office use on the site, and therefore are permitted.

Section 19-304.4 provides a table of Development Standards in the Downtown Zones. None of these standards is applicable to the proposed parking area expansion, but instead apply to buildings on the site (e.g., floor area ratio, building setbacks, min./max. building height, ground floor use and development standards) or apply to the creation of new parcels (min. lot size and frontage). Compliance of the existing Moda Health building with any of these standards is not affected by the proposed parking lot expansion. Therefore, these standards are not addressed in this report. The Design Standards of Section 19-304.6 also relate only to buildings, and therefore do not apply to the proposal to expand the parking area at Moda Health.

B. Compliance with Section 19.600 - Parking

Sheet P1.2, the Western Parking Lot Plan, illustrates the proposed revisions to the western parking area including provisions for lighting, landscaping, walkways, and the parking lot layout. Sheet P1.1 provides an overall site plan.

Minimum/maximum parking space standards

MZO Section 19.605.1 provides minimum and maximum parking space standards for various uses. These standards are based upon either square footage of individual uses or number of employees. The existing Moda Health office is 58,529 square feet in area and is occupied by an administrative office use. Table 19.605.1 requires a minimum of 2 parking spaces per every 1,000 feet of gross floor space for office uses. Therefore, a minimum of 117 parking spaces are required. The proposed revised site plan provides for 236 parking spaces, thereby exceeding the minimum number of parking spaces that are required.

Table 19.605.1 allows a maximum 3.4 parking spaces for every 1,000 square feet of gross floor area for an office use. At that ratio, the 58,529 square feet of building area occupied by Moda Health would be allowed 199 parking spaces. The total parking area proposed will provide for 236 parking spaces, or 4.03 parking spaces for every 1,000 square feet of gross floor area in the office building – prior to allowed exemptions of certain categories of parking spaces.

Twenty-four of the parking spaces will be reserved for employee carpool vehicles and can be exempted from this calculation, per Section 19.605.3.A.4. Three of the parking spaces will be reserved for MODA fleet vehicles and can be exempted from this calculation, per Section 19.605.3.A.5. Utilizing those exemptions, there would be a total of 209 parking spaces that would be subject to the maximum parking standard (236 total parking spaces -24 carpool spaces -3 fleet parking spaces = 209 parking spaces). Based upon that number, there would be 3.57 parking spaces for every 1,000 square feet of building area – which is still in excess of the maximum allowed parking standard. Zoning Code Section 605.2 allows the decision-making body to approve modifications to the minimum and maximum parking ratio standards subject criteria included within that section. It is requested that the Planning Commission approve a Parking Quantity Modification to the maximum allowed parking space standard for the Moda Health project to instead allow a rounded up maximum ratio of 3.6 parking spaces per 1,000 square feet of office area, as will be discussed in the following paragraphs.

MZO subsection 19.605.2 allows for the modification of minimum and maximum parking ratios from Table 19.605.1 as well as the determination of minimum and maximum parking requirements. This subsection says that parking determinations shall be made when the proposed use is not listed in Table 19.605.1 and for developments with large parking demands. In this case, Moda Health has found that their operations are rather labor intensive with a large parking demand. Currently, the Moda Health Milwaukie facility has 236 occupied work stations plus 9 other employees, which totals 245 employees at the building at the current employment level. In addition, the facility may have visitors such as clients, vendors, building machine repair personnel, and meeting visitors from Moda Health's Portland office. Together,

those numbers of employees and visitors can cause the existing Moda Health parking lots to be filled to capacity, and for some visitors or employees to park on SE Lava Drive. In response to neighbors concerns, the City of Milwaukie has recently decided to post SE Lava Drive as a “no parking” area. In addition to the intensive use of the building and its parking lots, Moda Health plans on expanding employment at their Milwaukie facility to occupy all of the existing work stations at the facility and to increase the number of total employees to 281 at the Moda Health Milwaukie facility. Based upon the current parking lots currently often being filled to capacity; the planned removal of on-street parking on SE Lava Drive; and Moda Health’s plans to expand employment at this facility to fully utilize the existing available work stations, Moda Health requests a Parking Quantity Modification to the maximum allowed parking standard for office uses.

MZO subsection 19.605.2.B says that the determination of a modification to a minimum or maximum parking standard should be based upon the following factors (with response provided):

1. *Describe the proposed uses of the site, including information about the size and types of the uses on site, and information about site users (employees, customers, etc.).*

Response: Moda Health currently employs approximately 245 employees on a typical daytime shift at their Milwaukie office building. Moda Health utilizes flex-time scheduling with employees arriving at times between 7:30 AM and noon. Moda Health is planning on adding another 36 employees to occupy existing work stations at the Moda Health Milwaukie facility. On a typical day, Moda health anticipates that another 20 to 25 visitors may be at their facility for meetings; to make sales calls on Moda; to service office machines; and to take care of their business with Moda Health. All of these employees and visitors create a substantial demand for onsite parking beyond what is normally allowed by the City of Milwaukie’s maximum allowed parking standard. This significant parking demand often fills the existing parking lot and leads to on-street parking on SE Lava Drive. In reaction to neighbors’ complaints, the City of Milwaukie has recently decided to post SE Lava Drive adjacent to the Moda Health facility as a no parking area. This will also undoubtedly lead to an increase in the demand for onsite parking.

2. *Identify factors specific to the proposed use and/ or site, such as the proximity of transit, parking demand management programs, availability of shared parking, and/ or special characteristics of the customer, client, employee or resident population that affect parking demand.*

Response: The Moda Health Milwaukie site is relatively well served by public transit. TriMet Bus route 70 has a stop on SE 17th Avenue right in front of Moda Health’s eastern parking lot and a north-bound stop across the street. Moda Health provided for the installation of bus waiting shelters at these stops as part of their original improvements related to moving onto the former Pendleton Woolen mills site. In addition, the TriMet Milwaukie transit center, which is located approximately 1,500 feet away on SE Jackson Street, is served by routes 28, 29, 30, 31, 32, 33, 34, 70, 75, 99, and 152. Routes 31, 32, and 33

make stops on SE Main Street approximately 1,500 feet from the primary Moda Health facility entrance.

Moda Health already provides transit pass subsidies for employees, provides transit information, provides carpool parking, and provides sufficient bicycle parking spaces and amenities – including indoor bike parking spaces along with lockers and employee showers. Moda Health encourages employees to use alternative means of transportation other than single-occupant vehicles through a monthly prize drawing for users of alternative means of transportation. Even with the relatively good transit service available to the site and the transit subsidies and other measures, Moda Health still finds that enough employees drive to work such that the current demand for onsite parking exceeds the current supply, and is anticipated to be further exceeded when employees are added.

3. *Provide data and analysis specified in Subsection 19.605.2.B.3 to support the determination request. The Planning Director may waive requirements of Subsection 19.605.2.B.3 if the information is not readily available or relevant, so long as sufficient documentation is provided to support the determination request.*

a. *Analyze parking demand information from professional literature that is pertinent to the proposed development. Such information may include data or literature from the Institute of Transportation Engineers, American Planning Association, Urban Land Institute, or other similar organizations.*

b. *Review parking standards for the proposed use or similar uses found in parking regulations from other jurisdictions.*

c. *Present parking quantity and parking use data from existing developments that are similar to the proposed development. The information about the existing development and its parking demand shall include enough detail to evaluate similarities and differences between the existing development and the proposed development.*

Response: A quick review of available studies related to maximum parking demand for offices uses in professional literature found nothing really on-point for this specific type of use. There is plenty of discussion on the topic of maximum parking standards in professional literature, but it primarily showed that maximum allowed parking standards vary greatly nationwide to the point of not being usable in the current case. Instead, we would like to point out that other jurisdictions in the Portland Metropolitan area have greater maximum parking standards for office uses than does the City of Milwaukie, while the Clackamas County Zoning Ordinance allows a maximum parking standard of 3.4 per 1,000 square feet of office area within parking zone A (which has 20-minute peak hour transit service within one-quarter mile walking distance for bus transit or one-half mile walking distance for light rail transit. 20-minute peak hour transit service is available on several Tri Met routes to/from the Milwaukie Transit Center, but only route 70 provides 20-minute peak hour transit service within one-quarter mile of the site. The Milwaukie Transit Center is more than one-quarter mile from the Moda Health offices. Clackamas County allows a maximum of 4.1 parking spaces per 1,000 square feet for offices beyond a one-quarter mile distance from peak transit service. Washington County and the Cities of

Gladstone and Beaverton have identical maximum parking standards to Clackamas County's. The City of Happy Valley does not have a maximum parking standard which applies to office developments. The City of Portland has a more stringent maximum parking standard for office uses.

4. *Propose a minimum and maximum parking ratio. For phased projects, and for projects where the tenant mix is unknown or subject to change, the applicant may propose a range (low and high number of parking spaces) for each development phase and both a minimum and maximum number of parking spaces to be provided at buildout of the project.*

Response: Because of the intensive employee and visitor use of the Moda Health parking facilities, it is requested that the City consider allowing a maximum parking standard for the employee intensive office use of Moda Health of 3.6 parking spaces per 1,000 square feet of office space.

5. *Address the approval criteria in Subsection 19.605.2.C. [cited below]*

C. *Approval Criteria*

The Planning Director shall consider the following criteria in deciding whether to approve the determination or modification. The Planning Director, based on the applicant's materials and other data the Planning Director deems relevant, shall set the minimum parking requirement and maximum parking allowed. Conditions of approval may be placed on the decision to ensure compliance with the parking determination.

1. *All modifications and determinations must demonstrate that the proposed parking quantities are reasonable based on existing parking demand for similar use in other locations; parking quantity requirements for the use in other jurisdictions; and professional literature about the parking demands of the proposed use.*

Response: As noted above, Moda Health requests that the City of Milwaukie apply a maximum parking standard for their use of 3.6 parking spaces per 1,000 square feet. As described above, a maximum 3.6 per 1,000 square feet ratio would be less than Clackamas County's, Washington County's, Gladstone's and Beaverton's maximum parking standards for areas which are not served by high frequency peak period transit. The requested 3.6 per 1,000 ratio would be slightly greater than Milwaukie's current maximum parking space ratio as well as other jurisdiction's maximum allowed parking ratios for sites which are well served by transit. However, the Moda Health site is served by just one high frequency peak transit period transit route making this case different than other situations where the facilities subject to a more stringent standard really are served by high frequency transit service.

2. *In addition to the criteria in Subsection 19.605.2.C.1, requests for modifications to decrease the amount of minimum required parking shall meet the following criteria:*

Response: The current request is a request to increase the maximum allowed parking standard, not decrease the minimum. Therefore, this subsection does not apply.

3. *In addition to the criteria in Subsection 19.605.2.C.1, requests for modifications to increase the amount of maximum allowed parking shall meet the following criteria:*
 - a. *The proposed development has unique or unusual characteristics that create a higher-than-typical parking demand.*
 - b. *The parking demand cannot be accommodated by shared or joint parking arrangements or by increasing the supply of spaces that are exempt from the maximum amount of parking allowed under Subsection 19.605.3.A.*
 - c. *The requested increase is the smallest increase needed based on the specific circumstances of the use and/or site.*

Response: The text above has demonstrated that Moda Health has a very high employee per 1,000 square feet ratio of 4.19 employees per 1,000 square feet and a significant number of site visitors – all of whom pose a significant parking demand. In addition, Moda Health plans to add employees to utilize existing work stations to increase that ratio to slightly over 4.8 employees per 1,000 square feet. This high employee per 1,000 square foot ratio helps create a higher than typical parking demand for an office use, thereby helping justify the need for relief from the current maximum parking standard.

Moda Health has already instituted a transit subsidy plan, provides transit information to employees, and provides preferred carpool, motorcycle and bicycle parking. Moda Health is committed to continuing to encourage carpooling and the use of alternative means of transportation. Moda Health has committed to provide 24 parking spaces as reserved carpool spaces as part of the current plans to expand their western parking lot. This represents 10 percent of the total number of parking spaces on the site – a very high ratio. (The reserved carpool spaces have been exempted from the parking which is subject to the maximum parking ratio standard.) In summary, Moda Health is already encouraging the use of alternatives to employees utilizing their personal vehicles for commuting purposes.

Moda Health has explored the possibility of a long-term shared parking agreement, but there are no conveniently located parking areas available. Although the Moda Health offices are located just about 650 feet across SE McLoughlin Boulevard from the Kellogg Bowl bowling facility as the crow flies (or as a pedestrian jaywalks), the lineal distance on public street sidewalks between the Kellogg Bowl and Moda Health is approximately 2,000 lineal feet using sidewalks and marked crosswalks across SE McLoughlin Boulevard and SE 17th Avenue. For the reason of distance alone, it is not practical for Moda Health to seek to enter into a long-term offsite parking agreement with the only nearby parking lot which would appear to have excess capacity during normal office hours. Additionally, it is believed that this distance would be too great

for long-term use by Moda Health employees to use over the long term (especially in winter) if no additional on-site parking spaces are developed. There are no other closer parking facilities which would appear to be relatively unused during daytime hours which might be available for a shared parking agreement.

The requested modification to the maximum allowed parking standard to allow a 3.6 spaces per 1,000 square foot standard would permit Moda Health to provide 210 non-exempt parking spaces for its 58,529 square foot facility. The requested increased ratio is the smallest increase needed based on the specific circumstances of Moda's use and the constraints of the site.

Parking lot design standards

Seven disabled person parking spaces will be provided in the east and west parking lots in locations convenient to main building entrances. The number of disabled person spaces is consistent with the Uniform Building Code standards and thus is also consistent with Section 503.10.

The parking lot is designed consistent with the dimensional standards specified and illustrated by MZO Section 19.606. Minimum parking lot aisle widths are consistent with the 22-foot minimum width standard so as to assure adequate emergency vehicle access throughout the site. All exterior portions of the existing building will be located within 150 feet of an accessway accessible to emergency vehicles. Parking areas will be constructed of asphaltic concrete in accordance with the standards of Section 19.606.3.A.

Loading

MZO Section 19.608.2 requires that two loading spaces be provided for non-residential uses of over 50,000 square feet. The Moda Health building is 58,529 square feet in size. Two 30 by 12 foot wide existing loading spaces are provided consistent with this standard. The loading spaces are adequately sized for the type of delivery vehicles that typically visit the Moda Health facility. The loading spaces are located so as not to interfere with parking lot aisles and are located distant from the site's boundaries.

Section 19.609 sets standards for required bicycle parking for commercial uses. Section 19.609.2.A.1 requires that bicycle parking spaces be provided in a number that is at least 10 percent of the number of automobile parking spaces which are required for the use. In this case, 117 vehicle parking spaces are required; therefore, a minimum of 12 bicycle parking spaces are required. The existing site improvements provide for 29 bicycle parking spaces, thereby satisfying this requirement. Five bicycle parking spaces are located in a bicycle rack located outside adjacent to the building's southern entrance, with 24 bicycle parking spaces located inside the building just inside that entrance. Lockers and employee shower facilities are provided. Existing bicycle parking facilities are illustrated on the site plan. Photos of the existing facilities are include in Appendix E.

Six carpool spaces are currently provided at the Moda Health facility. The carpool spaces will be increased to 24 parking spaces or 20% of the minimum required parking spaces provided. This amount of carpool space will exceed the requirement of Section 19.610.2 that a minimum of 10 percent of the required parking spaces in a commercial development be designated as carpool/van pool parking spaces. Consistent with Section 19.610.3, the designated carpool/vanpool parking spaces are conveniently located near main entrances to the building. Carpool spaces will be clearly designated by either pavement markings or signs as required.

C. Compliance with Chapter 19.500 Supplementary Development Regulations

The standards of MZO Chapter 19.500 were reviewed. There do not appear to be any standards from this chapter which would apply to the proposed parking lot expansion project. It is pointed out that the Transition Area measures prescribed by Section 19.504.6 where commercial or industrial uses are located adjacent to properties zoned for low density residential use do not apply within the downtown area zones, as specifically called out by that Section. The properties to the west of the site across SE Riverway Lane are zoned R2 which is classified as a medium and high density zone by Table 19.302.2 of the MZO. Therefore, these neighboring properties are not low density zones which would require the imposition of the Transition Area standard. Nevertheless, an attempt has been made to provide a sufficient buffer of landscaping and existing trees between the parking area's boundary with SE Riverway Lane and the properties across the street – some of which are developed with low density uses even if they are not zoned that way.

D. Compliance with Chapter 19.401 Willamette Greenway Zone

A portion of the site is designated with the Willamette Greenway (WG) overlay zone, including all of the area proposed for expansion of the western parking lot. The boundary of the Willamette Greenway overlay zone is illustrated on the site plans. The general purpose of the Greenway overlay zone is to protect, conserve, enhance and maintain the natural, scenic, historic, economic, and recreational qualities of lands along the Willamette River and its tributaries according to MZO Section 19.401.

Section 19.401.3 states that changes or intensifications of use, or development permitted in the underlying zone are conditional uses subject to Section 19.905. Although the existing Moda Health building is not located within the Greenway zoned portion of the site, the western parking lot and areas where additional landscaping are to be placed are located within the Greenway zone. To the extent that these activities are a change of use or development, Greenway conditional use approval is required. The proposed parking lot expansion within the Willamette Greenway overlay zone is not one of the prohibited uses listed in Section 19.401.3.

The general approval criteria for a conditional use are found in Section 19.905.4. Since the proposed parking lot expansion into the area of the site covered by the Willamette River Greenway is substantially larger than currently exists with the existing parking areas, we are assuming that the City will treat this as a major modification to an existing conditional use

rather than as a minor modification. As such, we have addressed the approval criteria for a major modification of Section 19.905.4.A. The criteria are cited below in *italics* with a response following each criteria:

1. *The characteristics of the site are suitable for the proposed use considering size, shape, location, topography, existence of improvements and natural features.*

Response: The Greenway zoned portion of the site which is proposed to be utilized for parking, a walkway, and landscaping is already used for those sorts of improvements. These areas within the Greenway zone are located outside of the banks of Johnson Creek and are in areas which do not have existing natural vegetation, steep slopes, or other characteristics which might make these areas inappropriate for the sorts of improvements which are proposed.

2. *The operating and physical characteristics of the proposed use will be reasonably compatible with, and have minimal impact on, nearby uses.*

3. *All identified impacts will be mitigated to the extent practicable.*

Response: The proposed parking area will take its access from the existing parking area driveway from SE Lava Drive. As such, traffic related to the expanded parking area should not be anticipated to have much in the way of impacts upon nearby uses – especially the single-family uses on SE Riverview. Storm drainage from the expanded parking lot will be collected in stormwater catch basins with storm filter type treatment equipment, piped under the parking area, and then being discharged to the tree covered slope above Johnson Creek, and then flowing to the Creek. Tree removal will be minimized so that only three existing mature trees will be removed.

The proposed Landscaping Plan, Sheet L1.0, proposes the addition of 18 trees within and adjacent to the parking area to mitigate for the loss of two mature trees and four parking area trees which were planted within the past 14 years. In addition, a hedge of native shrubs will be planted along the southern and western edges of the area of proposed parking lot expansion to provide low level screening of the expanded parking area from views from neighboring properties, and (remotely) possibly from the river. The proposed additional landscaping should also provide additional wildlife habitat and cover to mitigate for the existing plants and trees to be removed.

As such, the operating characteristics of the proposed expanded parking lot use within the greenway area will have minimal impacts upon nearby uses; such impacts will be mitigated to the extent practicable by landscaping, stormwater collection and treatment measures, and by landscaping providing a buffer between the parking lot and adjacent uses. No potential impacts upon the Willamette greenway uses or values have been identified.

4. *The proposed use will not have unmitigated nuisance impacts, such as from noise, odor, or vibrations greater than uses allowed outright at the proposed location.*

Response: No nuisance impacts have been identified related to the proposed expanded parking lot use.

5. *The proposed use will comply with all applicable development standards and requirements of the base zone, any overlay zones or special areas and the standards in Section 19.905.*

Response: The existing administrative office use of the site is a permitted use in the underlying DO zone applied to the site, as addressed elsewhere in this report. The proposed parking area expansion would be considered as accessory to the primary office use. This application narrative addresses the applicable development standards and requirements of the base DO zone, the Willamette Greenway and Natural Resource overlay zones, and this section addresses the requirements of Section 19.905.

6. *The proposed use is consistent with applicable policies of the Comprehensive Plan related to the proposed use.*

Response: The Comprehensive Plan was reviewed for any Plan policies applicable to the proposed expanded parking lot use within the Willamette Greenway of the site. No Objectives or Policies seem to really be on point with regard to the proposed parking lot expansion on an upland portion of the Willamette greenway zoned area. The Objectives and Policies of the Willamette Greenway element of the Comprehensive Plan primarily direct City actions to maintain the Greenway boundary, develop Zoning Ordinance permitting processes for activities within the Greenway, direct further Greenway planning by the City, and address public access and private property rights. Establishing that the proposed development is consistent with the Greenway Conditional Use approval criteria which implement the regulatory Plan policies is tantamount to satisfaction of the Comprehensive Plan goals and policies.

7. *Adequacy public transportation facilities and public facilities will be able to serve the proposed use prior to occupancy pursuant to Chapter 19.700.*

Response: The site is already well served by public roadways, transit service, and public utilities necessary to serve the proposed use. No additional transportation or public facilities will be necessary to serve the proposed parking area expansion

In addition to the general conditional use approval criteria of Section 19.905.4.A, the following criteria of Section 19.401.6 also apply to a proposal in the Willamette Greenway overlay zone:

- a. *Whether the land to be developed has been committed to an urban use, as defined under the State Willamette River Greenway Plan.*

Response: The area of the site within the greenway which is to be utilized under the proposed plan already includes a paved parking lot and landscaping which are associated with an existing urban type of building located elsewhere on the site, along with some wooded area. Therefore, the land is already committed to an urban use.

b. Compatibility with the scenic, natural, historic, economic, and recreational character of the River;

Response: The proposed parking lot expansion, addition of additional walkway, and the addition of vegetation as illustrated on the Landscape Plan, Sheet L1.0, should not have significant effects upon the scenic and natural characteristics of the river since the proposed parking lot expansion area will be separated from the river by existing single- and multi-family residential development and the improved SE Riverway Lane. The proposed parking lot expansion will remove two large trees whose tops may be visible from the river, but again, most of the trees' structure is currently not visible from the river because of the intervening homes and multi-family buildings. In addition, the majority of the existing trees on the Moda Health site will be retained so while views of the trees on the site may be altered slightly, views of the trees from the river will not be eliminated. [Note, it is noted that this Code provision echoes Comprehensive Plan, Willamette Greenway Element, Objective #5, Policy 5].

The proposed changes should not affect the historic or economic character of the Greenway or the river.

c. Protection of views both toward and away from the River; . [It is noted that this Code provision echoes Comprehensive Plan, Willamette Greenway Element, Objective #5, Policy 3].

and

d. Landscaping, aesthetic enhancement, open space, and vegetation between the activity and the River, to the maximum extent practicable;

Response: The proposed site improvements should have little if any effect upon views toward or from the river. As noted above, this area is physically and visually separated from the river by existing development and a road.

e. Public access to and along the River, to the greatest possible degree, by appropriate legal means;

Response: There is currently no public access to the river on the subject site, or on other nearby properties. No additional public access is proposed; nor will any existing public access be affected by the proposed parking area development.

f. Emphasis on water-oriented and recreational uses;

Response: The proposed parking area improvements within the Greenway area are intended to support a non-water oriented, non-recreational use in a building outside of the Greenway. [It is noted that this Code provision echoes Comprehensive Plan, Willamette Greenway Element, Objective #3, Policy 4].

g. Maintain or increase views between the Willamette River and downtown;

Response: The proposed lower elevation parking area related site improvements should have no effect upon views between downtown and the river.

h. Protection of the natural environment according to regulations in Section 19.402;

Response: Compliance with the requirements of the Natural Resource overlay zone, Section 19.402, is addressed below.

i. Advice and recommendation of the Design and Landmark Committee, as appropriate;

Response: No response is necessary at this time.

j. Conformance to applicable Comprehensive Plan policies;

Response: Compliance with Willamette Greenway related Plan policies is addressed within the responses to Section 19.401 is closely related to a Plan Policy. The entire plan was reviewed for policies which might impact the proposed parking lot expansion. All of the policies which were applicable have been cited within this application narrative.

k. The request is consistent with applicable plans and programs of the Division of State Lands;

Response: We are not aware of any plans or programs of the Division (now Department) of State Lands which would be affected by the proposed site improvements.

l. A vegetation buffer plan meeting the conditions of subsection 19.401.8A through C.

Response: Section 19.401.8.A provides that a buffer strip of vegetation shall be identified along the river, which shall include the land area between the river and a location 25 feet upland from the ordinary high water line and this section further requires that this area be preserved, enhanced or re-established. The subject parking area expansion is located far from the River and is separated from it by residential development and SE Riverview Lane.

Section 19.401.7 suggests that non water-related or water-dependent uses be directed away from the Willamette River. The proposed expanded parking area is non water-related and water-dependent, and will be appropriately be located away from the river.

IV. CONSISTENCY WITH APPLICABLE DOWNTOWN DESIGN GUIDELINES

The subject property is zoned with the Downtown Office zoning district. As such, the proposed parking lot expansion project has been determined to be subject to Downtown Design Review. It is noted that the subject site has not been included on the following plans from the Milwaukie Downtown Design Guidelines document: Fundamental Concepts Diagram; Storefront Main Street Area; and Amenities and Open Space Framework Map. The only map within the Guidelines which includes the subject site is a segment of the City of Milwaukie Zoning Map.

Below are responses to the applicability of the broad categories of Guidelines listed in the Milwaukie Downtown Design Guidelines document, with responses to specific guidelines which appear to be applicable to the current proposal.

Milwaukie Character Guidelines

The Milwaukie Character Guidelines were reviewed with regard to the proposed Moda Health parking lot expansion. None of these guidelines would appear to relate directly to this project. The only one which indirectly relates says *“Buildings should be “good neighbors.” They should be compatible with surroundings buildings by avoiding disruptive excesses. New buildings should not attempt to be the center of attention.”*

The proposed parking lot expansion does not involve the construction or renovation of a building. However, it does involve the expansion of an existing parking lot use within a developed neighborhood. The proposed expanded parking lot’s design is limited in scale in order to not stand out in the neighborhood and to be as compatible as possible with neighboring single-family and multi-family uses. Landscape materials to be used within and adjacent to the parking lot will not only screen views of the parking lot from neighboring uses, but will also add to the well landscaped character of the Moda Health project site and to the neighborhood.

Pedestrian Emphasis Guidelines

The Pedestrian Emphasis Guidelines were reviewed with regard to the proposed Moda Health parking lot expansion. None of these guidelines would appear to relate directly to this project, other than the following Guidelines:

Barriers to pedestrian movement and visual and other nuisances should be avoided or eliminated, so that the pedestrian is the priority in all development projects.

Accommodate handicap access in a manner that is integral to the building and public right-of-way and not merely designed to meet minimum building code standards.

The proposed parking lot expansion project includes an extension of the on-site pedestrian sidewalk so as to provide a convenient and safe paved pedestrian connection into the Moda Health building, including provisions for disabled person access ramps.

Architecture Guidelines

The Architecture Guidelines were reviewed with regard to the proposed Moda Health parking lot expansion. None of these guidelines would appear to relate directly to this project.

Lighting Guidelines

The Lighting Guidelines were reviewed with regard to the proposed Moda Health parking lot expansion. None of these guidelines would appear to relate directly to this project. The proposed parking lot project will involve adding a few parking lot lights of the same general type as are currently in use in the parking area. Parking lot light pole locations are illustrated on the Preliminary Landscape Plan, Sheet L1.0. A cut sheet for the new light fixtures is included in Appendix D.

Sign Guidelines

The Sign Guidelines were reviewed with regard to the proposed Moda Health parking lot expansion. None of these guidelines would appear to relate directly to this project. No additional signs will be placed on the site as part of the current parking lot expansion project.

V. CONCLUSION

This application narrative and the attached plans demonstrate that all applicable provisions of the City of Milwaukie Zoning Ordinance, Comprehensive Plan, and Downtown Design Guidelines are satisfied by the plans of Moda Health for the redevelopment of the Pendleton Woolen Mills property. Moda Health and Otak, Inc. therefore respectfully request approval of the requested Type III Downtown Design Review, Willamette River Greenway conditional use, and Parking Modification applications.

Appendix

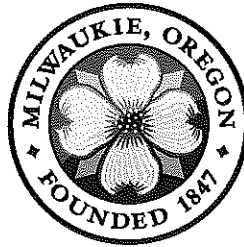


HanmiGlobal Partner

Appendix A



HanmiGlobal Partner



April 17, 2014

Jerry Offer
OTAK, Inc.
808 SW Third Avenue, Suite 300
Portland, OR 97204

Re: Preapplication Report

Dear Jerry:

Enclosed is the Preapplication Report Summary from your meeting with the City on April 3, 2014, concerning your proposal for action on property located at 10505 SE 17th Avenue.

A preapplication conference is required prior to submittal of certain types of land use applications in the City of Milwaukie. Where a preapplication conference is required, please be advised of the following:

- Preapplication conferences are valid for a period of 2 years from the date of the conference. If a land use application or development permit has not been submitted within 2 years of the conference date, the Planning Director may require a new preapplication conference.
- If a development proposal is significantly modified after a preapplication conference occurs, the Planning Director may require a new preapplication conference.

If you have any questions concerning the content of this report, please contact the appropriate City staff.

Sincerely,

Blanca Marston
Administrative Specialist II

Enclosure

cc: Pete Miller, OTAK
David Shaffer, MODA
File

CITY OF MILWAUKIE

PreApp Project ID #: 14-005PA

PRE-APPLICATION CONFERENCE REPORT

This report is provided as a follow-up to a meeting that was held on 4/3/2014 at 10:00am

Applicant Name: Jerry Offer
Company: Otak, Inc.
Applicant 'Role': Other
Address Line 1: 808 SW 3rd Ave Ste 300
Address Line 2:
City, State Zip: Portland OR 97204
Project Name: MODA Parking Expansion
Description: Downtown Design Review and Natural Resource Review for adding parking to existing parking lot.
ProjectAddress: 10505 SE 17th Ave
Zone: Downtown Office (DO)
Occupancy Group: NA
ConstructionType: NA
Use: Town Center (TC), with the southeastern-most portion of the site designated as High Density (HD)
Occupant Load: NA
AppsPresent: Jerry Offer, David Shaffer, Peter Miller
Staff Attendance: Brett Kelter, Brad Albert, Philip Kolb, John Stelzenmueller

BUILDING ISSUES

ADA: Parking stalls shall be dimensioned on submittal for permits. ADA ramp at top of side aisle encroaches into walking path. Provide a clear and flat aisle with a minimum of 3 feet clear path. ADA signage shall comply with ANSI A117.1 and Chapter 11 of the Oregon Structural Specialty Code.

Structural: NA
Mechanical: NA
Plumbing: NA
Plumb Site Utilities: Provide a complete site drainage (underground storm) at time of permit submittal
Electrical: TBD. The parking lot lighting electrical permit shall go through Clackamas Co for review and

permits

Notes: Fire Department has submitted comments separately

Please note all drawings must be individually rolled. If the drawings are small enough to fold they must be individually folded.

FIRE MARSHAL ISSUES

Fire Sprinklers: NA

Fire Alarms: NA

Fire Hydrants:

Turn Arounds:

Addressing:

Fire Protection:

Fire Access:

Hazardous Mat.:

Fire Marshal Notes: See attached.

PUBLIC WORKS ISSUES

Water: Not Applicable at this time in the development process.

Sewer: Not Applicable at this time in the development process.

Storm: Submission of a storm water management plan by a qualified professional engineer is required as part of the proposed development. The plan shall conform to Section 2 - Stormwater Design Standards of the City of Milwaukie Pubic Works Standards.

The storm water management plan shall demonstrate that the post-development runoff does not exceed the pre-development, including any existing storm water management facilities serving the development property. Also, the plan shall demonstrate compliance with water quality standards. The City of Milwaukie has adopted the City of Portland 2008 Stormwater Management Manual for design of water quality facilities.

All new impervious surfaces, including replacement of impervious surface with new impervious surfaces, are subject to the water quality standards. See City of Milwaukie Public Works Standards for design and construction standards and detailed drawings.

The storm SDC is based on the amount of new impervious surface constructed at the site. One storm SDC unit is the equivalent of 2,706 square feet of impervious surface. The storm SDC is currently \$765 per unit. The storm SDC will be assessed and collected at the time the building permits are issued.

Street: Not Applicable at this time in the development process.

Frontage: Not Applicable at this time in the development process.
Right of Way: Not Applicable at this time in the development process.
Driveways: Not Applicable at this time in the development process.
Erosion Control: Not Applicable at this time in the development process.
Traffic Impact Study: Not Applicable at this time in the development process.
PW Notes: OTHER SYSTEM DEVELOPMENT CHARGES

The Transportation SDC will be based on the increase in trips generated by the new use per the Trip Generation Handbook from the Institute of Transportation Engineers. The SDC for transportation is \$1,741.00 per trip generated. Credits will be given for any demolished structures, which shall be based upon the existing use of the structures.

The Parks and Recreation SDC is assessed for both residential and nonresidential development. Currently, the Parks and Recreation SDC for residential development is \$3,985.00 for each single-family dwelling unit and \$3,608.00 for each multi-family dwelling unit. The Parks and Recreation SDC of nonresidential development is calculated on the number of employees according to type of business and building square feet and is \$60.00 per employee.

PLANNING ISSUES

Setbacks: No new building is proposed, so there are no relevant setback standards.

Landscape: There is no minimum required landscaping for the Downtown Office zone, though there are landscaping requirements for off-street parking areas. See the "Parking" section for more details.

Parking: Milwaukie Municipal Code (MMC) Chapter 19.600 provides standards for off-street parking areas. MMC Table 19.605.1 establishes minimum and maximum numbers of spaces. For general office uses, a minimum of 2 spaces are required per 1,000 square feet of area; a maximum of 3.4 spaces are allowed per 1,000 square feet of area. According to past documentation, there are approximately 57,000 square feet of office, which results in a minimum requirement of 114 spaces and a maximum allowance of 194 spaces.

In 2004, the City approved a request to exceed the maximum amount of allowed parking by 8 spaces, to a total of 202. According to the applicant, the site currently provides 206 parking spaces, with 56 spaces in a lot on the east side of the office building and 150 spaces in a lot on the west side of the building. The proposed development would increase the total number of spaces to approximately 236. In MMC 19.605.2, the current code provides a process for modifying the maximum allowed number of spaces, which involves providing data and analysis to support the request. MMC 19.605.3.A provides exemptions to the maximum quantity allowance. Parking spaces that are designated specifically for employee carpool parking or for fleet vehicles are not counted toward the maximum allowance.

As per MMC 19.602.4.A, parking areas developed to serve existing uses must meet the various standards for such features as dimensions, landscaping, and pedestrian access. For perpendicular (90-degree) spaces, stalls must be at least 9 feet wide and 18 feet deep, with a 22-foot drive aisle. In downtown zones, there is no perimeter landscaping required adjacent to other properties; a 4-foot perimeter buffer is required where adjacent to public right-of-way. At least 25 square feet per parking space must be provided as interior landscaping, with each planting area at least 6 feet wide and at least

120 square feet in area. One tree is required along each lineal 40 feet of landscaping areas

Transportation Review: Please see the Public Works notes for more information about the applicability of MMC 19.700 Public Facility Improvements and any required right-of-way dedication and street improvements.

Application Procedures: The proposed development would involve the following land use applications:
Parking Determination
Downtown Design Review
Willamette Greenway
Natural Resources
Development Review

Parking Quantity Determination (MMC 19.605.2) = The proposed development must obtain approval to exceed the maximum number of allowed parking spaces. Such a request requires Type II review, with the Planning Director as the deciding authority. The application requirements and approval criteria are provided in MMC 19.605.2.B and 2.C, respectively.

Downtown Design Review (MMC 19.907) = Construction of an expanded parking lot triggers Type III review and a public hearing before the Planning Commission. A recommendation meeting with the Design and Landmarks Committee (DLC) will be required prior to a hearing and decision by the Planning Commission. The DLC regularly meets on the first Monday of each month. As new construction, the proposed development is subject to the design standards of MMC 19.304.6.C; however, those standards are intended for buildings and are not applicable to the proposed parking area. The application narrative should address all relevant design guidelines (e.g., for Milwaukie Character, Pedestrian Emphasis, Lighting, etc.). A completed Design Review checklist should be submitted with the application, to indicate which standards and guidelines are addressed in the application narrative.

Willamette Greenway (MMC 19.401) = Most of the western parking lot is within the Willamette Greenway overlay. Construction to expand the parking lot requires Type III review against the approval criteria of MMC 19.401.6. The applicant should address all relevant Willamette Greenway approval criteria, such as those relating to urban use, the State Willamette River Greenway Plan, and protection of the environment as per MMC 19.402 Natural Resources.

Natural Resources (MMC 19.402) = A portion of the western parking lot includes a designated Habitat Conservation Area (HCA) and is subject to the protections of MMC 19.402. As per MMC 19.402.11.D.1.b, up to 10 percent of the total HCA-only areas on the site can be disturbed using the Type I nondiscretionary review process, as long as the required number of mitigation plantings are provided as per MMC 19.402.11.D.2. Where the HCA on site overlaps with any water quality resource (WQR) area, those overlapping areas do not count as HCA-only. Mitigation includes tree replacement based on either the size of trees removed or the size of area disturbed, whichever results in requiring more trees. "Disturbance" includes both permanent disturbance (i.e., loss of HCA to development) and temporary disturbance for construction activity.

Development Review (MMC 19.906) = If the proposed parking improvements and associated downtown design and greenway reviews are approved, Type I Development Review will be required with the submittal of any needed development permits (such as for grading and erosion control), to ensure that all relevant standards are met and that any required conditions of approval are carried out.

The City allows multiple land use applications to be processed either concurrently or individually, as per MMC 19.1001.6.B. The applicant has indicated an interest in expediting the process and may choose to bundle the Downtown Design Review (Type III), Willamette Greenway (Type III), and Parking Quantity Determination (Type II) applications for concurrent review through the Type III process. The Type I Natural Resources review for HCA disturbance can be handled separately from that

bundle of applications but would be processed in conjunction with the necessary Type I Development Review application at the time of building permit submittal.

For multiple applications, the most expensive fee is collected in full, with a 25-percent discount for each additional application. For the current fiscal year (until June 30, 2014), the following fees are in effect for the various levels of land use application review: Type I (\$200), Type II (\$1,000), and Type III (\$2,000). There is a \$500 fee for appealing any decision to the relevant appeal authority listed in MMC Table 19.1001.5

Natural Resource Review: Johnson Creek passes through the subject property, resulting in several designated areas of Water Quality Resource (WQR) and Habitat Conservation Area (HCA).

Lot Geography: The subject property is approximately 314,000 square feet in area (7.2 acres) and is irregularly shaped. Johnson Creek runs along the eastern boundary in meandering fashion, isolating one small piece of the property from the rest of the site. On the north, the lot is bounded by SE Lava Drive; on the west, it is bounded by SE Riverway Lane. Extending approximately 400 feet from the intersection with Lava Dr, Riverway Ln is public right-of-way with a 15-foot width; along the southern boundary of the site, Riverway Ln is a private access road.

Planning Notes: As part of making the case for approval to exceed the maximum allowed parking quantity, the applicant is encouraged to address any existing transportation demand management (TDM) practices employed by the current business at the site (Moda Health). The Planning Commission will likely want to know whether Moda currently offers transit passes, carpooling programs, bicycle amenities (such as inside bike parking, lockers, showers, etc.), shuttle services, or other incentives to reduce parking demand. With the imminent opening of light rail (Fall 2015) and the soon-to-come construction of a multiuse path on 17th Ave near the subject property, there may be questions from the Commission about whether it is necessary to approve the requested parking increase.

ADDITIONAL NOTES AND ISSUES

County Health Notes:

Other Notes:

This is only preliminary preapplication conference information based on the applicant's proposal and does not cover all possible development scenarios. Other requirements may be added after an applicant submits land use applications or building permits. City policies and code requirements are subject to change. If you have any questions, please contact the City staff that attended the conference (listed on Page 1). Contact numbers for these staff are City staff listed at the end of the report.

Sincerely,

City of Milwaukie Development Review Team

BUILDING DEPARTMENT

John Stelzenmueller - Building Official - 503-786-7611

Bonnie Lanz - Permit Specialist - 503-786-7613

ENGINEERING DEPARTMENT

Jason Rice - Engineering Director - 503-786-7605

Brad Albert - Civil Engineer - 503-786-7609

Adriana Slavens - Civil Engineer - 503-786-7602

Philip Kolb - Engineering Technician II - 503-786-7610

Alex Roller - Engineering Technician I - 503-786-7695

COMMUNITY DEVELOPMENT DEPARTMENT

Stephen Butler - Community Development Director - 5

Marcia Hamley - Admin Specialist - 503-786-7656

Alicia Martin -Admin Specialist - 503-786-7600

Blanca Marston -Admin Specialist - 503-786-7600

PLANNING DEPARTMENT

Dennis Egner - Planning Director - 503-786-7654

Ryan Marquardt - Senior Planner - 503-786-7658

Brett Kelter - Associate Planner - 503-786-7657

Li Alligood - Associate Planner - 503-786-7627

Vera Kolias - Associate Planner - 503-786-7653

CLACKAMAS FIRE DISTRICT

Mike Boumann - Lieutenant Deputy Fire Marshal - 503-742-2673

Clackamas County Fire District #1

Fire Prevention Office



E-mail Memorandum

To: City of Milwaukie Planning Department
From: Shawn Olson, Clackamas Fire District #1
Date: 04/16/2014
Re: 10505 SE 17th Ave, Moda Health Parking Lot Expansion

This review is based upon documents submitted by the applicant. The comments we provide are intended as an advisory to the applicant until final design documents are submitted for formal review at the County. While the scope of this review is typically limited to fire apparatus access and water supply, the applicant must comply with all applicable Fire Code requirements. The following access and water supply requirements shall be addressed on the formal submittal.

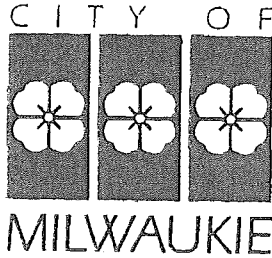
Fire Apparatus Access

1. "No Parking Fire Lane" signs shall be placed on one or both sides of a fire apparatus access road when road widths are less than 26 feet wide and 26 feet to 32 feet in width. Signs shall be placed on both sides of a fire apparatus access road when roads are less than 26 feet in width and on one side when roads are 26 feet to 32 feet in width. Red painted curbs can be used in lieu of signs.
2. Provide turning radius dimensions on submitted plans. The inside turning radius and outside turning radius for a 20 feet wide road shall be not less than 28 feet and 48 feet respectively, measured from the center point

Appendix B



HanmiGlobal Partner



April 12, 2000

File(s): MU-00-01

NOTICE OF DECISION

This is official notice of action taken by the Milwaukie Planning Commission on April 11, 2000.

Applicant(s): Oregon Dental Service
Location(s): 10505 SE Lava Drive
Tax Lot(s): 11E 35AA 03500
Application Type(s): Mixed Use Overlay, Variance, Willamette Greenway Conditional Use, Natural Resource Overlay, Transportation Plan Review
Decision: APPROVED
Review Criteria: Milwaukie Zoning Ordinance:

- 318-Mixed Use Overlay Zone MU
- 320-Willamette Greenway Zone WG
- 322-Natural Resource Overlay Zone NR
- 500-Offstreet Parking and Loading
- 600-Conditional Uses
- 700-Variances, Exceptions, and Home Improvement Exceptions
- 1011.3- Minor Quasi-Judicial Review
- 1400-Transportation Planning, Design Standards and Procedures

The Planning Commission's decision on this matter may be appealed to the Milwaukie City Council. An appeal of this action must be filed within 15 days of the date of this notice. All appeals must be accompanied by the applicable fee, be submitted in the proper format, address applicable criteria, and be made on forms provided by the Planning Department. Milwaukie Planning staff (786-7600) can provide information regarding forms, fees, and the appeal process.

Appeal expiration date: 5:00 p.m., April 27, 2000

FINDINGS IN SUPPORT OF APPROVAL

1. The applicant is proposing to make building improvements of 58,000 square feet for office use and related parking and site improvements for office occupancy at 10505 SE

COMMUNITY DEVELOPMENT DEPARTMENT
Public Works • Planning • Building • Fleet • Facilities
6101 SE Johnson Creek Blvd., Milwaukie, Oregon 97206
PHONE: (503) 786-7600 • FAX: (503) 774-8236

Lava Drive. The improvements involve architectural alterations including enclosing approximately 15,000 square of covered parking, demolition of 2,200 square feet, new offices spaces, executive offices, conference center, lunchroom, new windows, skylights, and exterior finishes. Site improvements include parking, lighting, and landscaping, pedestrian walkways, half-street improvements, and right-of-way dedications on Lava Drive.

2. All applications have been processed, and public notice provided, in accordance with Zoning Ordinance (ZO) Section 1011.3-Minor Quasi Judicial Review.
3. All applications have been processed in accordance with specific procedural requirements of each applicable zoning section.
4. The applicant has requested a variance of the requirements of ZO Section 318.8.A.2 and 318.9.D to provide first floor retail or service uses and to provided housing as part of the development proposal.
5. Application VR-00-01 meets ZO Section 702.1-Criteria for Granting Variances as follows:
 - a. The Commission accepts the applicant's explanation of compliance with 702.1.A and finds that the characteristics of building design and building placement are unusual conditions over which the applicant has no control. The existing building is unsuitable for development of required first floor and residential uses due to its physical constraints, and significant cost of renovation to make it suitable for the required first floor and residential uses.
 - b. The Commission finds that there are no economically feasible alternatives to the variance and that the variance does not allow greater use of the property than similarly situated properties. Reuse of the building for any occupancy is expected to require improvements as needed to meet code requirements or make the building functional for the intended occupancy. Since any building improvement that either changes the exterior of the building or results in an intensification of use, requires compliance with first floor and residential use provisions of Section 318.8.A.2 and 318.8.D, there are no alternatives that allow reasonable economic use of the building.
 - c. The Commission finds that there will be no discernable impacts on other properties as a result of granting the variance of the requirement to provide first floor retail or service uses and residential uses.
6. Except for the first floor and residential use requirements, for which a variance has been granted, application MU-00-01 meets the requirements of the ZO Section 318-Mixed Use Zone.
7. Application WG-00-01 meets the requirements of ZO Section 320-Willamette Greenway Zone and ZO Section 600-Conditional Uses.

8. As conditioned, application NR-00-02 meets, or can meet, the requirements of ZO Section 322-Natural Resource Overlay. With proper location and design of structures and control of construction activities, setbacks are adequate to ensure protection of resources. Compliance with Section 322.7.A.5 provisions that require protection of natural resource areas from site lighting is feasible with proper design and location of lighting. Compliance with Section 322.7.B.8 requirements to minimize adverse impacts on natural features is feasible with proper design and placement of stormwater drainage out falls.
9. The applicant has proposed improvements that include the following:
 - a. pedestrian connection between the building and 17th Avenue located along the southern side of the parking lot;
 - b. half street improvements along the property frontage on Lava Drive that include curb, curb tight sidewalk, paving, striping, dedicated left and right turn lanes;
 - c. relocation of a TRI-MET bus stop and construction of a bus shelter, bench and other reasonable transit facilities;
 - d. landscape planting to establish a buffer between the building and Johnson Creek;
 - e. site lighting that does not create glare or shine on adjacent properties;
 - f. required design elements of Section 318.8.19 as follows:
 1. pedestrian scale lighting along walkways,
 2. historical plaque recognizing Pendelton's historical use of the site,
 3. street furniture (benches) on Lava Drive,
 4. enhanced transit amenities,
 5. enhanced pedestrian walkways, and
 6. decorative drinking fountain along Lava Drive.
 - g. interior storage of refuse and recycling containers ; and
 - h. covered bicycle parking.
10. As conditioned, the parking plan complies with parking lot design, lighting, and landscaping requirements of Section 500-Offstreet Parking and Loading. The proposed parking plan does not meet requirements for interior landscaping islands. Modification of the parking plan to meet interior island requirements of Section 503.19 is feasible. Parking lot lighting is required to be located and designed to ensure safety while not resulting in impacts to adjoining properties pursuant to ZO Section 503.15. Site lighting required by Section 503.15 can be provided reasonably and feasibly.
11. As conditioned the site plan complies with ZO Section 503.12 concerning number of curb cuts. The existing curb cut on 17th Avenue is deemed potentially unsafe due to its proximity to Lava Drive. Under congested conditions the driveway intersection may

cause traffic, pedestrian, or cyclist safety hazards. Accordingly, abandonment of the driveway curb cut is warranted.

12. The applicant is proposing to save certain existing trees located within parking areas. Preservation of existing trees is encouraged under ZO 503.19.D.
13. Section 1405-Transportation Planning, Design Standards, and Procedures, Sidewalks allows for sidewalks to be curb-tight when adequate right-of-way is not available. The applicant has indicated right-of-way will be dedicated as needed for required half-street improvements. Curb tight sidewalks are acceptable along Lava Drive in order to minimize the area of dedication for right-of-way.

CONDITIONS OF APPROVAL

1. Final site and architectural development plans shall be in substantial conformance with plans submitted for land use approval. Reference is made to development plans entitled Figure 4 through Figure 11, architectural drawings entitled Figures A through E (composite), application submission materials, and minutes of the Planning Commission's public hearing.
2. Prior to issuance of a building permit the applicant shall submit to the satisfaction of the Planning Director, a detailed narrative description of all actions taken to comply with conditions of this approval. The report shall address each condition and describe changes made in response. The report shall also describe all changes that were made, but which were not required by this approval. Any changes deemed to be in excess of, or that are inconsistent with this approval may require review by the Planning Commission subject to the Planning Director's discretion.
3. Prior to issuance of a building permit, final development plans shall be submitted to the satisfaction of the Planning Director for determination of consistency with the plans approved by the Planning Commission.
4. Prior to issuance of a building permit, a grading plan showing existing and proposed grades and elevations of all retaining walls shall be submitted to the satisfaction of the Planning Director.
5. Prior to issuance of a building permit, development plans showing final design details of the southern sidewalk and its intersection with the 17th Avenue sidewalk and the existing vehicle barrier along the Lava Drive sidewalk, shall be submitted to the mutual satisfaction of the Public Works Director, Planning Director, and if required, Oregon Department of Transportation. The plans shall indicate all required fill, structures and drainage facilities. Fill within the Natural Resource area shall be minimized to the greatest extent practicable. Construction impacts shall be mitigated by erosion control methods. Any earth disturbance within the NR boundary shall be restored with native plant materials, including but not limited to native ground cover.

6. Prior to issuance of a building permit the applicant shall submit approval of Tri-Met to relocate the bus stop.
7. Prior to issuance of a building permit, the applicant shall submit transit stop construction plans to the mutual satisfaction of the Public Works Director, Planning Director, and if needed, Tri-Met. The facility shall include ADA improvements.
8. Prior to the issuance of building permit, the applicant shall submit a detailed and dimensioned parking plan. All spaces, aisles, planter islands, wheels stops, and other required features shall be provided and designed in accordance with ZO Section 500- Offstreet Parking and Loading.
9. Prior to issuance of a building permit, the applicant shall submit a detailed parking lot and pedestrian walkway lighting plan. Lighting shall be provided along the southern sidewalk pursuant to Section 500. Trespass lighting on the Natural Resource area is prohibited pursuant to Section 322-Natural Resource Overlay Zone. Glare and trespass lighting impacts on adjoining properties is prohibited pursuant to Section 503.15. The lighting plan shall specify mounting height, fixture and lamp types. Photometrics shall be provided for each type of installation.
10. Prior to issuance of a building permit the applicant shall submit a tree preservation plan for all areas affected by parking lot construction. The plan is to identify appropriate tree protection measures, excavations, fills, utility trenches, material and equipment storage, and tree protection measures.
11. Prior to issuance of a building permit, the applicant shall submit an erosion control and construction barrier plan to the mutual satisfaction of the Planning Director and Public Works Director. Construction barriers shall be located at appropriate locations along the top-of-bank of Johnson Creek to prevent encroachment within the natural resource area. Construction barriers shall be of substantial construction and shall be securely installed.
12. Prior to issuance of a building permit, the applicant shall submit construction plans to the mutual satisfaction of the Fire Marshal and Building Official.
13. Prior to issuance of a building permit, all required permit and system development charges shall be paid.
14. Prior to issuance of a building permit, the applicant shall submit instruments for dedication of Lava Drive right-of-way to the mutual satisfaction of the Public Works Director, Planning Director, and City Attorney.
15. Prior to issuance of a building permit, the applicant shall submit to the mutual satisfaction of the Planning Director and Public Works Director, construction plans for Lava Drive half street improvements including curb tight sidewalk, curb, drainage, paving, striping, and dedicated north and south bound turn lanes. The sidewalk shall be no less than 6 feet in width exclusive of the curb pursuant to ZO Section 1405. The plans shall also indicate appropriate tree preservation measures.

16. Prior to issuance of a building permit the applicant shall submit a plan to the satisfaction of the Public Works Director showing abandonment of the 17th Avenue curb cut and restoration of curb and sidewalk.
17. Prior to issuance of building permit, all public improvements shall be bonded to the satisfaction of the Public Works Director.
18. Prior to issuance of a building permit, the applicant shall submit detailed construction plans for all elements proposed under Section 318.8.A.19-Design Standards, "essential elements".
19. Prior to inspections for final occupancy, the dedication of land for public right-of-way shall be executed. Copies of all related maps and filings be submitted to the Planning Director, and Public Works Director, and City Attorney.
20. Prior to any inspection for final occupancy, all landscaping improvements shall be completed unless, bonding of landscaping improvements is submitted to the satisfaction of the Planning Director pursuant to Section 503.19.E.
21. Prior to issuance of a building permit, the applicant shall submit instruments for dedication of Lava Drive right-of-way to the mutual satisfaction of the Public Works Director, Planning Director, and City Attorney.
22. Prior to any inspections for temporary occupancy, all public improvements shall be completed and inspected to the satisfaction of the Public Works Director.
23. Prior to approval being issued for temporary or final certificate of occupancy, the applicant shall submit to the Planning Director a detailed narrative of all actions taken to comply with all conditions of this approval. The Planning Director shall conduct a site inspection and other investigations as needed to ensure compliance with conditions of this approval prior to granting occupancy approvals.
24. Prior to issuance of final occupancy approvals, all improvements proposed to meet design criteria of Section 318.8.A.19 ("essential requirements") shall be completed and inspection to the satisfaction of the Planning Director.



Alice Rouyer
Planning Director

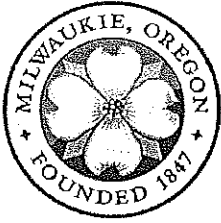
cc: Applicant
Planning Commission
Martha Bennett, Assistant City Manager, CD
Sue Stepan, Public Works Director
Paul Roeger, P.E., Civil Engineer
John Wheeler, Building Official

Bonnie Lanz, Permit Specialist
Tom Crowder, Deputy Fire Marshal
Historic Milwaukie NDA
Interested Persons
File

Appendix C



HanmiGlobal Partner



PLANNING DEPARTMENT
6101 SE Johnson Creek Blvd
Milwaukie OR 97206

PHONE: 503-786-7630
FAX: 503-774-8236
E-MAIL: planning@milwaukieoregon.gov

Application for Land Use Action

Master File #: _____

Review type*: I II III IV V

CHECK ALL APPLICATION TYPES THAT APPLY:		
<input type="checkbox"/> Amendment to Maps and/or Ordinances:	<input type="checkbox"/> Land Division:	<input type="checkbox"/> Planned Development
<input type="checkbox"/> Comprehensive Plan Text Amendment	<input type="checkbox"/> Final Plat	<input type="checkbox"/> Residential Dwelling:
<input type="checkbox"/> Comprehensive Plan Map Amendment	<input type="checkbox"/> Lot Consolidation	<input type="checkbox"/> Accessory Dwelling Unit
<input type="checkbox"/> Zoning Text Amendment	<input type="checkbox"/> Partition	<input type="checkbox"/> Duplex
<input type="checkbox"/> Zoning Map Amendment	<input type="checkbox"/> Property Line Adjustment	<input type="checkbox"/> Manufactured Dwelling Park
<input type="checkbox"/> Code Interpretation	<input type="checkbox"/> Replat	<input type="checkbox"/> Temporary Dwelling Unit
<input type="checkbox"/> Community Service Use	<input type="checkbox"/> Subdivision	<input type="checkbox"/> Sign Review
<input type="checkbox"/> Conditional Use	<input type="checkbox"/> Miscellaneous:	<input type="checkbox"/> Transportation Facilities Review
<input type="checkbox"/> Development Review	<input type="checkbox"/> Barbed Wire Fencing	<input type="checkbox"/> Variance:
<input type="checkbox"/> Director Determination	<input type="checkbox"/> Bee Colony	<input type="checkbox"/> Use Exception
<input checked="" type="checkbox"/> Downtown Design Review	<input type="checkbox"/> Mixed Use Overlay Review	<input type="checkbox"/> Variance
<input type="checkbox"/> Extension to Expiring Approval	<input type="checkbox"/> Modification to Existing Approval	<input checked="" type="checkbox"/> Willamette Greenway Review
<input type="checkbox"/> Historic Resource:	<input type="checkbox"/> Natural Resource Review	<input type="checkbox"/> Other: _____
<input type="checkbox"/> Alteration	<input type="checkbox"/> Nonconforming Use Alteration	Use separate application forms for:
<input type="checkbox"/> Demolition	<input checked="" type="checkbox"/> Parking:	• Annexation and/or Boundary Change
<input type="checkbox"/> Status Designation	<input type="checkbox"/> Quantity Determination	• Compensation for Reduction in Property Value (Measure 37)
<input type="checkbox"/> Status Deletion	<input checked="" type="checkbox"/> Quantity Modification	• Daily Display Sign
	<input type="checkbox"/> Shared Parking	• Appeal
	<input type="checkbox"/> Structured Parking	

RESPONSIBLE PARTIES:

APPLICANT (owner or other eligible applicant—see reverse): Moda Health	
Mailing address: 601 SW Second Avenue, Portland, OR	Zip: 97204
Phone(s): 503-948-5564	E-mail: david.shaffer@modahealth.com
APPLICANT'S REPRESENTATIVE (if different than above): Otak, Inc. (Jerry Offer)	
Mailing address: 808 SW Third Avenue, Suite 300, Portland, OR	Zip: 97204
Phone(s): 503-415-2330	E-mail: jerry.offer@otak.com

SITE INFORMATION:

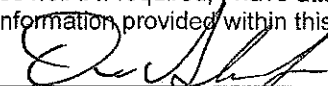
Address: 10505 SE Lava Drive	Map & Tax Lot(s): 11E 35AA TL 03500
Comprehensive Plan Designation: Downtown Zoning: DO/WG/NR Size of property:	

PROPOSAL (describe briefly):

A request for the following approvals: (1) Downtown Design Review; (2) Willamette Greenway Use; and (3) a Modification to the maximum permitted parking for a 58,500 sq. ft. office use. Moda Health seeks to add thirty parking spaces to their existing western parking area.

SIGNATURE:

ATTEST: I am the property owner or I am eligible to initiate this application per Milwaukie Municipal Code (MMC) Subsection 19.1001.6.A. If required, I have attached written authorization to submit this application. To the best of my knowledge, the information provided within this application package is complete and accurate.

Submitted by:  Date: 4-7-14

IMPORTANT INFORMATION ON REVERSE SIDE



PLANNING DEPARTMENT
6101 SE Johnson Creek Blvd
Milwaukie OR 97206

PHONE: 503-786-7630
FAX: 503-774-8236
E-MAIL: planning@milwaukieoregon.gov

For all Land Use Applications
(except Annexations and Development Review)

Submittal Requirements

All land use applications must be accompanied by a signed copy of this form (see reverse for signature block) and the information listed below. The information submitted must be sufficiently detailed and specific to the proposal to allow for adequate review. Failure to submit this information may result in the application being deemed incomplete per the Milwaukie Municipal Code (MMC) and Oregon Revised Statutes.

Contact Milwaukie Planning staff at 503-786-7630 or planning@milwaukieoregon.gov for assistance with Milwaukie's land use application requirements.

1. **All required land use application forms and fees**, including any deposits.
Applications without the required application forms and fees will not be accepted.
2. **Proof of ownership or eligibility to initiate application** per MMC Subsection 19.1001.6.A.
Where written authorization is required, applications without written authorization will not be accepted.
3. **Detailed and comprehensive description** of all existing and proposed uses and structures, including a summary of all information contained in any site plans.
Depending upon the development being proposed, the description may need to include both a written and graphic component such as elevation drawings, 3-D models, photo simulations, etc. Where subjective aspects of the height and mass of the proposed development will be evaluated at a public hearing, temporary on-site "story pole" installations, and photographic representations thereof, may be required at the time of application submittal or prior to the public hearing.
4. **Detailed statement** that demonstrates how the proposal meets all applicable application-specific approval criteria (check with staff) and all applicable development standards (listed below):
 - a. **Base zone standards** in Chapter 19.300.
 - b. **Overlay zone standards** in Chapter 19.400.
 - c. **Supplementary development regulations** in Chapter 19.500.
 - d. **Off-street parking and loading standards and requirements** in Chapter 19.600.
 - e. **Public facility standards and requirements**, including any required street improvements, in Chapter 19.700. *N.A.*
5. **Site plan(s), preliminary plat, or final plat** as appropriate.
See Site Plan, Preliminary Plat, and Final Plat Requirements for guidance.
6. **Copy of valid preapplication conference report**, when a conference was required.

Have not yet received this

APPLICATION PREPARATION REQUIREMENTS:

- Five copies of all application materials are required at the time of submittal. Staff will determine how many additional copies are required, if any, once the application has been reviewed for completeness.
- All application materials larger than 8½ x 11 in. must be folded and be able to fit into a 10- x 13-in. or 12- x 16-in. mailing envelope.
- All application materials must be collated, including large format plans or graphics.

ADDITIONAL INFORMATION:

- Neighborhood District Associations (NDAs) and their associated Land Use Committees (LUCs) are important parts of Milwaukie's land use process. The City will provide a review copy of your application to the LUC for the subject property. They may contact you or you may wish to contact them. Applicants are strongly encouraged to present their proposal to all applicable NDAs prior to the submittal of a land use application and, where presented, to submit minutes from all such meetings. NDA information: <http://www.milwaukieoregon.gov/communityservices/neighborhoods-program>.
- Submittal of a full or partial electronic copy of all application materials is strongly encouraged.

As the authorized applicant I, (print name) Jerry D. Offer of Otak, Inc., attest that all required application materials have been submitted in accordance with City of Milwaukie requirements. I understand that any omission of required items or lack of sufficient detail may constitute grounds for a determination that the application is incomplete per MMC Subsection 19.1003.3 and Oregon Revised Statutes 227.178. I understand that review of the application may be delayed if it is deemed incomplete.

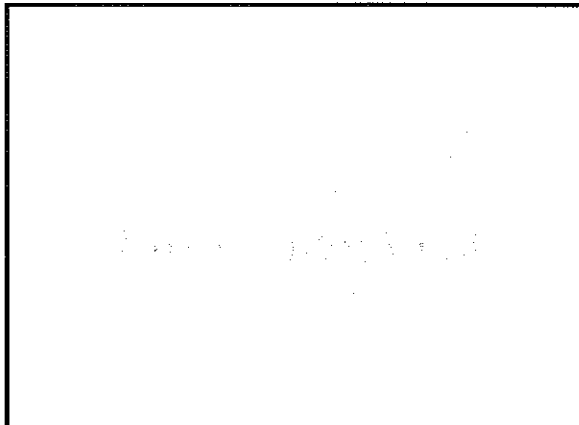
Furthermore, I understand that, if the application triggers the City's sign-posting requirements, I will be required to post signs on the site for a specified period of time. I also understand that I will be required to provide the City with an affidavit of posting prior to issuance of any decision on this application.

Applicant Signature: Jerry Offer Otak, Inc.

Date: April 21, 2014

Official Use Only

Date Received (date stamp below):



DOWNTOWN DESIGN REVIEW CHECKLIST

Project/Applicant Name: MODA Health Parking Revisions / MODA Health

Project Address: 10505 SE 17th Avenue.

Application Submission Date: April 11, 2014

Zoning: DO Willamette Greenway and Natural Resource Overlay Zones on portion

Building Use: Administrative Offices for health insurance firm.

Completed By: Jerry Offer, Otak, Inc. on: April 21, 2014

STANDARDS AND GUIDELINES

A. Development and Design Standards Complies
Yes No NA

1. Development Standards

a. Permitted Use	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Minimum Lot Size	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Floor Area Ratio	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d. Building Height	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e. Residential Density	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Street Setbacks	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
g. Side and Rear Setbacks	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
h. Ground-floor Retail	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. Ground-floor Windows/Doors	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j. Drive-through Facilities	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
k. Off-street Parking Requirements	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
l. Landscaping	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

2. Design Standards

a. Residential Entries and Porches	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Garages and Parking Areas	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Courtyards	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Balconies	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Walls	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Windows	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Roofs	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

subject to modification req

B. Design Guidelines

1. Milwaukie Character

a. Reinforce Milwaukie's Sense of Place	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Integrate the Environment	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Promote Linkages to Horticultural Heritage	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Establish or Strengthen Gateways	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Consider View Opportunities	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Consider Context	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Promote Architectural Compatibility	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h. Preserve Historic Buildings	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. Use Architectural Contrast Wisely	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j. Integrate Art	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

sort of

DOWNTOWN DESIGN REVIEW CHECKLIST

Complies

- | | Yes | No | NA |
|--|-------------------------------------|--------------------------|-------------------------------------|
| 2. Pedestrian Emphasis | | | |
| a. Reinforce and Enhance the Pedestrian System | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b. Define the Pedestrian Environment | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c. Protect the Pedestrian from the Elements | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d. Provide Places for Stopping and Viewing | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| e. Create Successful Outdoor Spaces | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| f. Integrate Barrier-Free Design | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 3. Architecture | | | |
| a. Corner Doors | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b. Retail and Commercial Doors | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c. Residential Doors | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d. Wall Materials | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| e. Wall Structure | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| f. Retail Windows | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| g. Residential Bay Windows | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| h. Silhouette and Roofline | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| i. Rooftops | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| j. Green Architecture | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| k. Building Security | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| l. Parking Structures | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 4. Lighting* | | | |
| a. Exterior Building Lighting | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b. Parking Lot Lighting | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c. Landscape Lighting | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d. Sign Lighting | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 5. Signs | | | |
| a. Wall Signs | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b. Hanging or Projecting Signs | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c. Window Signs | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d. Awning Signs | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| e. Information and Guide Signs | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| f. Kiosk Monument Signs | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| g. Temporary Signs | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Notes:

Appendix D



HanmiGlobal Partner



FEATURES & SPECIFICATIONS

INTENDED USE — Ideal for car lots, street lighting or parking areas.

CONSTRUCTION — Rugged, .063" thick, aluminum rectilinear housing. Formed for weather-tight seal and integrity. Naturally anodized, extruded aluminum door frame with mitered corners, is retained with two .188" diameter hinge pins and secured with one quarter-turn, quick-release fastener. Weatherproof seal between housing and door frame is accomplished with an integrally designed, extruded silicone gasket that snaps into the door frame, and another gasket applied to the housing.

Finish: Standard finish is dark bronze (DDB), polyester powder finish with other architectural colors available.

OPTICS — Reflectors are anodized and segmented for superior uniformity and control. Reflectors attach with tool-less fasteners and are rotatable and interchangeable. Five cutoff distributions available: Type II (roadway), Type III (asymmetric), Type IV (forward throw, sharp cutoff), Type IV (wide, forward throw), Type V (square). Lens is .125" thick impact-resistant tempered glass with thermally applied silk-screened shield.

ELECTRICAL — Ballast: Constant wattage autotransformer for 250-400W. Super CWA pulse-start ballast required for 320W and 350W (SCWA option). Super CWA (pulse start ballast), 88% efficient and EISA legislation compliant, is required for 250-400W (SCWA option) for U.S. shipments only, CSA or INTL required for probe-start shipments outside the U.S.

Ballasts are 100% factory-tested.

Socket is porcelain, horizontally mounted mogul base socket with copper alloy, nickel-plated screw shell and center contact. UL listed 1500W-600V.

INSTALLATION — Extruded aluminum arm for pole or wall mounting is shipped in fixture carton. Optional mountings available.

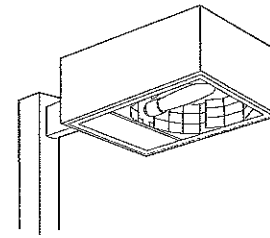
LISTING — UL listed (standard). CSA Certified (see Options). NOM Certified (see Options). UL listed for 25°C ambient temperature and wet locations. IP65 rated.

WARRANTY — 1-year limited warranty. Complete warranty terms located at

www.acuitybrands.com/CustomerResources/Terms_and_conditions.aspx

Note: Specifications subject to change without notice.

Catalog Number
Notes
Type



Specifications

EPA: 2.0 ft² (.28 m²) (includes arm)
 Length: 24-19/32 (62.5)
 Width: 17-25/32 (62.5)
 Depth: 8-5/16 (21.1)
 Arm: 4 (10.2)
 *Weight: 52 lbs (23.6 kg)
 *Weight as configured in example below.
 All dimensions are inches (centimeters) unless otherwise specified.

Area Lighting
KSF2
 METAL HALIDE: 250-400W
 HIGH PRESSURE SODIUM: 200-400W
 15' to 25' Mounting



Mounting option	Drilling template
SPxx, RPxx, DA12P	5
WBxx, DA12WB	6
WWxx	7

ORDERING INFORMATION For shortest lead times, configure product using bolded options.

Example: KSF2 400M R3 TB SCWA SP04 LPI

KSF2 Series	Wattage	Distribution		Voltage		Ballast		Mounting					
		R2	Type II roadway	120	347	(blank)	Magnetic	Type	Arm length ⁹	Shipped separately			
KSF2	Metal halide	R3	Type III asymmetric	208 ⁶	480 ⁶	CWI	Constant wattage Isolated	SP	Square pole	04	4" arm	DA12P	Degree arm, pole
	250M ¹ 350M ^{2,3,4}	R4SC	Type IV forward throw, sharp cutoff	240 ⁶	TB ⁷	Pulse Start		RP	Round pole	06	6" arm	DA12WB	Degree arm, wall
	High pressure sodium⁵	R4W	Type IV wide, forward throw	277	23050HZ ⁸	SCWA	Super CWA pulse-start ballast	WB	Wall bracket	09	9" arm	KMA	Mast arm adapter
	200S 400S	R5S	Type V square					WW	Wood pole or wall bracket	12	12" arm	KTMB	Twin mounting bar
	250S												

NOTE: For shipments to U.S. territories, SCWA must be specified to comply with EISA.

Options				Finish ¹⁶				Lamp ¹⁸			
Shipped installed in fixture		EC	Emergency circuit ¹¹	Shipped separately¹²		(blank)	Dark bronze	DNAXD	Natural aluminum	LPI	Lamp included
PER	NEMA twist-lock receptacle only (no photocontrol)	CSA	CSA Certified	PE1	NEMA twist-lock PE (120, 208, 240V)	DWH	White	DWHXD	White	L/LP	Less lamp
SF	Single fuse (120, 277, 347V) n/a TB ¹⁰	NOM	NOM Certified ⁹	PE3	NEMA twist-lock PE (347V)	DBL	Black	DDBTXD	Textured dark bronze		
DF	Double fuse (208, 240, 480V) n/a TB ¹⁰	INTL	Available for MH probe-start shipping outside the U.S.	PE4	NEMA twist-lock PE (480V)	DMB	Medium bronze	DBLBXD	Textured black		
QRS	Quartz restrrike system ¹¹	KW1	KiloWatch [®] 120V control relay ¹²	PE7	NEMA twist-lock PE (277V)	DNA	Natural aluminum	DNATXD	Textured natural aluminum		
QRSTD	QRS time delay ^{8,11}	KW4	KiloWatch [®] 277V control relay ¹²	SC	Shorting cap for PER option	CRT	Non-stick protective coating ¹⁷				
		REGC1	California Title 20 effective 1/1/2010	HS-	House side shield (R2, R3) ^{14,15}	Super Durable Finishes		DWHGXD	Textured white		
				VG	Vandal guard ¹⁴	DDBXD	Dark bronze				
						DBLXD	Black				

Accessories: Tenon Mounting Slipfitter						
Number of fixtures:						
Tenon O.D.	One	Two@180°	Two@90° ¹⁹	Three@120°	Three@90° ¹⁹	Four@90° ¹⁹
2-3/8" (6)	T20-190	T20-280	T20-290	T20-320	T20-390	T20-490
2-7/8" (7.3)	T25-190	T25-280	T25-290	T25-320	T25-390	T25-490
4" (10.2)	T35-190	T35-280	T35-290	T35-320	T35-390	T35-490

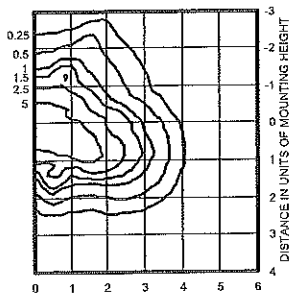
- Notes**
- These wattages require the REGC1 option to be chosen for shipments into California for Title 20 compliance. 250M REGC1 is not available in 347V or 480V.
 - Use reduced jacketed lamp.
 - Must be ordered with SCWA.
 - These wattages do not comply with California Title 20 regulations.
 - Not available with SCWA.
 - Must specify CWI for use in Canada.
 - Optional multi-tap ballast (120, 208, 240, 277V); (120, 277, 347V in Canada).
 - Consult factory for available wattages.
 - Use 9" arm when two or more luminaires are oriented on a 90° drilling pattern.
 - Must specify voltage. Not available with TB.
 - Maximum allowable wattage lamp included.
 - KiloWatch[®] controls are available only with 250S or 400S.
 - May be ordered as an accessory.
 - Prefix with KSF2 when ordering as an accessory.
 - Available with R2 and R3 distributions only.
 - See www.lithonia.com/archcolors for additional color options.
 - Black finish only.
 - Must be specified.
 - Must use RP09 or RP12.

KSF2 Arm-Mounted Rectilinear Cutoff Lighting

Coefficient of Utilization

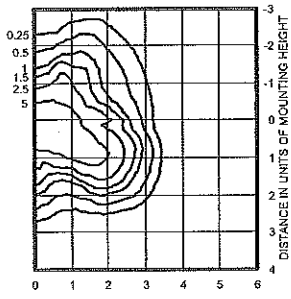
Initial Footcandles

KSF2 400M R2 TEST NO: 1193031801P
ISOILLUMINANCE PLOT (Footcandle)



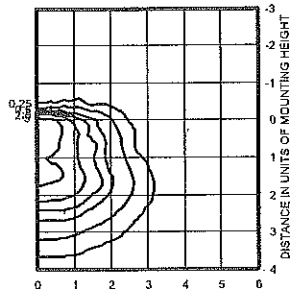
400W pulse start metal halide lamp, rated 38000 lumens. Footcandle values based on 20' mounting height.
Classification: Type II, Short, Full Cutoff

KSF2 400M R3 TEST NO: 1194100501P
ISOILLUMINANCE PLOT (Footcandle)



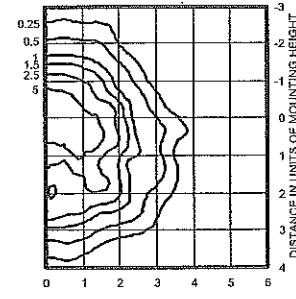
400W pulse start metal halide lamp, rated 38000 lumens. Footcandle values based on 20' mounting height.
Classification: Type III, Short, Full Cutoff

KSF2 400M R4SC TEST NO: 1193041301P
ISOILLUMINANCE PLOT (Footcandle)



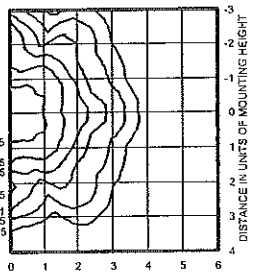
400W pulse start metal halide lamp, rated 38000 lumens. Footcandle values based on 20' mounting height.
Classification: Unclassified (Type IV, Very Short), Full Cutoff

KSF2 400M R4W TEST NO: LTL8509P
ISOILLUMINANCE PLOT (Footcandle)



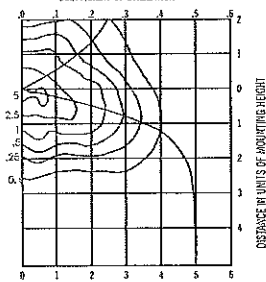
400W pulse start metal halide lamp, rated 38000 lumens. Footcandle values based on 20' mounting height.
Classification: Type IV, Short, Full Cutoff

KSF2 400M R5S TEST NO: 1193051801P
ISOILLUMINANCE PLOT (Footcandle)



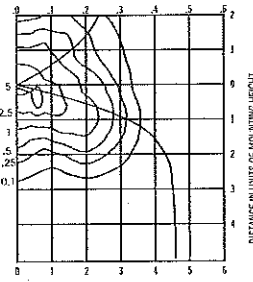
400W pulse start metal halide lamp, rated 38000 lumens. Footcandle values based on 20' mounting height.
Classification: Unclassified (Type IV, Very Short), Full Cutoff

KSF2 400S R2 Test No. 1193031601
COEFFICIENT OF UTILIZATION



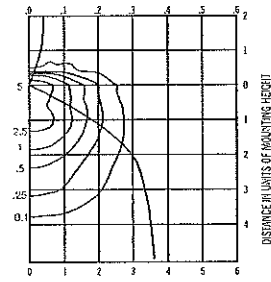
400W High Pressure Sodium lamp, 50000 rated lumens. Footcandle values based on 35' mounting height, Distribution II, cutoff.

KSF2 400S R3 Test No. TEST NO. 1194091501
COEFFICIENT OF UTILIZATION



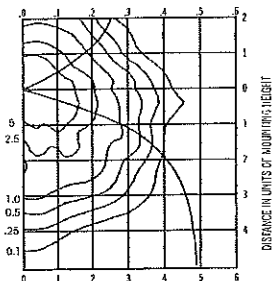
400W High Pressure Sodium lamp, rated lumens. Footcandle values based on 35' mounting height, Distribution III, cutoff.

KSF2 400S R4SC Test No. 1193042201
COEFFICIENT OF UTILIZATION



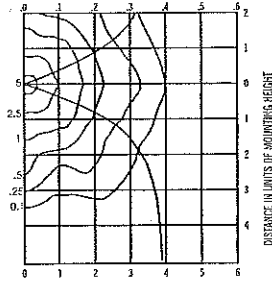
400W High Pressure Sodium lamp, 50000 rated lumens. Footcandle values based on 35' mounting height, Distribution IV, sharp cutoff.

KSF2 400S R4W Test No. LTL8506
COEFFICIENT OF UTILIZATION



400W High Pressure Sodium lamp, 50000 rated lumens. Footcandle values based on 20' mounting height, Distribution IV wide, froward throw.

KSF2 400S R5S Test No. 1193051704
COEFFICIENT OF UTILIZATION



400W High Pressure Sodium lamp, 50000 rated lumens. Footcandle values based on 35' mounting height, Distribution V, cutoff.

Mounting Height Correction Factor

(Multiply the fc level by the correction factor)

35' mounting height	20' mounting height
15 ft. = 5.4	15 ft. = 1.78
30 ft. = 1.36	30 ft. = 0.45
40 ft. = 0.77	40 ft. = 0.25

$$\left(\frac{\text{Existing Mounting Height}}{\text{New Mounting Height}} \right)^2 = \text{Correction Factor}$$

NOTES:

- Photometric data for other distributions can be accessed from the Lithonia Lighting website. (www.Lithonia.com)
- For electrical characteristics, consult outdoor technical data specification sheets on www.Lithonia.com.
- Tested to current IES and NEMA standards under stabilized laboratory conditions. Various operating factors can cause differences between laboratory and actual field measurements. Dimensions and specifications are based on the most current available data and are subject to change.



KSF2-M-S

Appendix E



HanmiGlobal Partner



2014 4 4



2014 4 6

Appendix F



HanmiGlobal Partner

Memorandum



808 SW Third Avenue, Suite 300
Portland, OR 97204
Phone (503) 287-6825
Fax (503) 415-2304

To: Brett Kelper (City of Milwaukie - Planning)
Philip Kolb (City of Milwaukie – Engineering)

From: Troy Kent, PE (Otak)
Matt Klym, EIT (Otak)

Copies: Pete Miller (Otak), Jerry Offer (Otak)

Date: April 22, 2014

Subject: MODA Plaza Parking Preliminary Stormwater Management

Project No.: Otak Project No. 17134

This memo has been prepared to identify the preliminary stormwater management of the proposed improvement conditions for the MODA Plaza Parking improvements. According to the City of Milwaukie (COM) Public Works Standards, Milwaukie has adopted the City of Portland's (COP) Stormwater Management Manual (SWMM). According to the SWMM section 1.3.2 "Flow and Volume Control" sub-section "Flow Control Exemptions" "New development and redevelopment projects may be exempt from flow control requirements if they discharge stormwater runoff directly into the Willamette River..." Since the project discharges directly to Johnson Creek in close proximity (+/- 400 ft) from the Willamette River, we assume that no flow control is needed for the proposed improvements. Pollution reduction must still be met regardless of exemption of flow control.

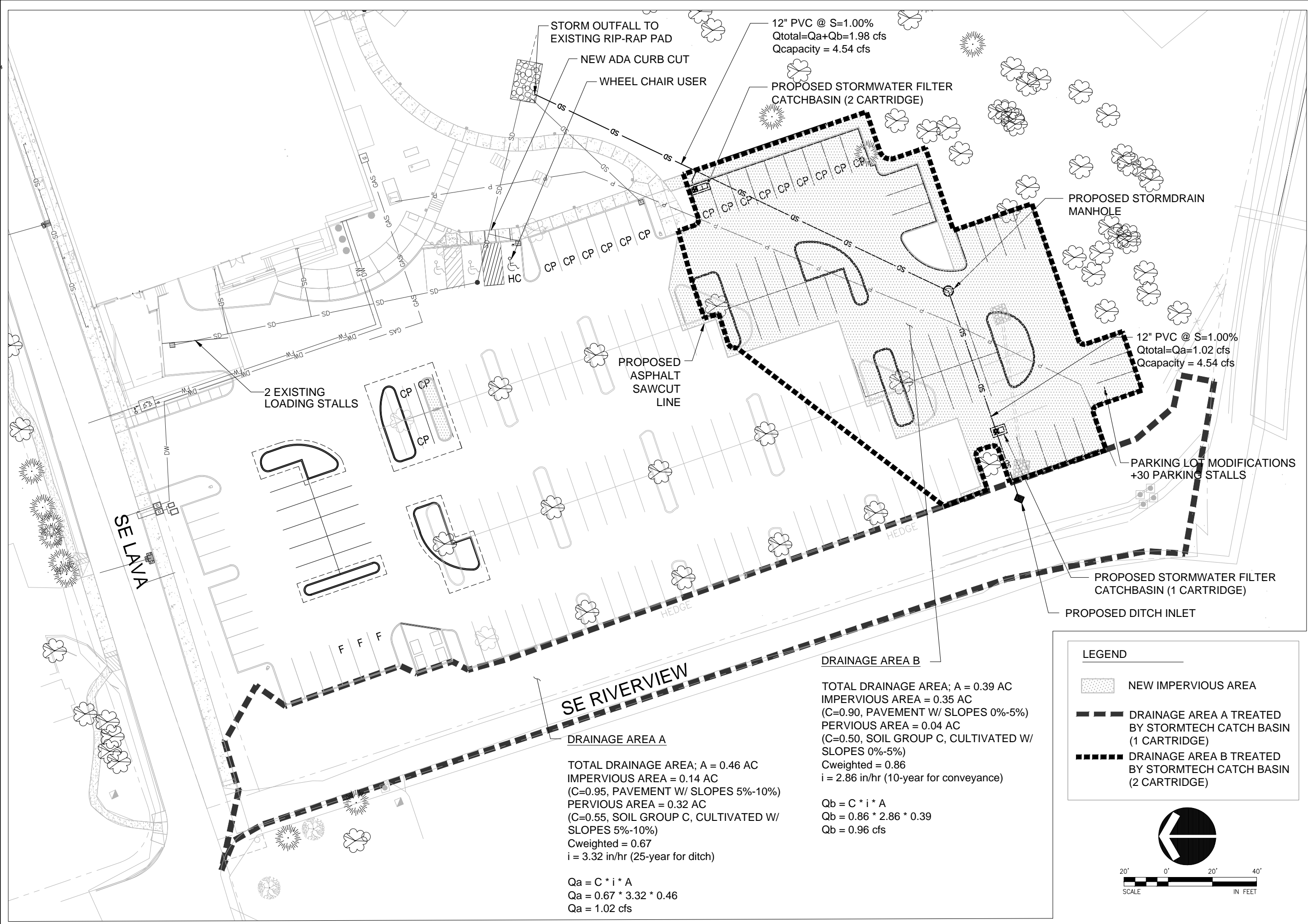
Pollution reduction for the MODA Plaza Parking will be met by using two Stormwater Management Stormfilter Catch Basins (as approved by the COP, per "Approved Proprietary Stormwater Treatment Technologies"). The Catch Basin serving Area A (see Exhibit EX-01) will consist of a one cartridge catch basin with a sloped, solid cover located in the southeast portion of the improvements. This catch basin will be serving the existing ditch flow for SE Riverview (private), which had been previously picked up by a culvert and treated by a swale on the MODA property. This swale will be impacted with the proposed improvements and will no longer be able to function as intended. The existing flow will be intercepted in the ditch by a proposed ditch inlet and conveyed to the stormwater catch basin. The second Catch Basin serving the parking lot (Area B) will consist of a two cartridge catch basin with a sloped, vaned inlet grate located in the northeast portion of the improvements. The stormwater will be conveyed by a 12" PVC pipe to an existing rip rap pad in the northeast section of the property.

To size the pollution reduction, the City of Portland's Presumptive Approach Calculator ver 1.2 was used (see attached exhibits A and B). The pollution reduction flow for Area A is $Q_{prA} = 0.024$ CFS and for Area B is $Q_{prB} = 0.063$ CFS. Using a cartridge flow rate of 15 GPM (or 0.033 CFS) for each catch basin, Area A would require one cartridge and Area B would require two cartridges. See attached details for Contech Stormwater Catch Basins.

For conveyance per the City of Portland's Sewer and Drainage Facilities Design Manual (SDFDM), the return period for designing a ditch inlet is 25 years and for a street inlet is 10 years (see Table 6.1 in the SDFDM). For conveyance calculations, please see EX-01. The total flow for Area A is 1.02 CFS and Area B is 0.96 CFS. The total flow for conveyance is 1.98 CFS. The capacity of the 12" PVC pipe with a minimum slope of 1% is 4.54 CFS (see Exhibit C), thus providing adequate capacity for the required design peak flows.

As shown, the treatment and conveyance for the MODA Plaza Parking improvements meet or exceed the design requirements set forth in the City of Portland's Stormwater Management Manual and the Sewer and Drainage Facilities Design Manual.

XREF LIST
 Ltscale: 1
 Resolved
 ParkL3A
 S246B190
 A246s102_50504
 D246X230
 A246X385
 D246X190
 A246XBDR
 RGTSTAMP
 P17134X001
 P17134X230
 P17134X600
 D246X600
 ---Images---
 HCA zones



DRAINAGE AREA A

TOTAL DRAINAGE AREA; A = 0.46 AC
 IMPERVIOUS AREA = 0.14 AC
 (C=0.95, PAVEMENT W/ SLOPES 5%-10%)
 PERVIOUS AREA = 0.32 AC
 (C=0.55, SOIL GROUP C, CULTIVATED W/
 SLOPES 5%-10%)
 Cweighted = 0.67
 i = 3.32 in/hr (25-year for ditch)

$Q_a = C * i * A$
 $Q_a = 0.67 * 3.32 * 0.46$
 $Q_a = 1.02 \text{ cfs}$

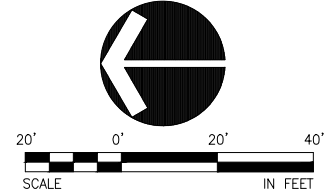
DRAINAGE AREA B

TOTAL DRAINAGE AREA; A = 0.39 AC
 IMPERVIOUS AREA = 0.35 AC
 (C=0.90, PAVEMENT W/ SLOPES 0%-5%)
 PERVIOUS AREA = 0.04 AC
 (C=0.50, SOIL GROUP C, CULTIVATED W/
 SLOPES 0%-5%)
 Cweighted = 0.86
 i = 2.86 in/hr (10-year for conveyance)

$Q_b = C * i * A$
 $Q_b = 0.86 * 2.86 * 0.39$
 $Q_b = 0.96 \text{ cfs}$

LEGEND

- NEW IMPERVIOUS AREA
- DRAINAGE AREA A TREATED BY STORMTECH CATCH BASIN (1 CARTRIDGE)
- DRAINAGE AREA B TREATED BY STORMTECH CATCH BASIN (2 CARTRIDGE)



DOWNTOWN DESIGN REVIEW SUBMITTAL - APRIL 10, 2014

MODA HEALTH PLAZA PARKING
 CITY OF MILWAUKIE, OREGON
 NEW IMPERVIOUS AREA



17134
 Project No. Drawing No.
EX-01
 Sheet No.
 © Otak, Inc. 2014

NO.	DATE	BY	REVISION COMMENTS

Design	Drawn	Checked	Date	Initial Issue Date:
MPK	KNY	TDK		



Presumptive Approach Calculator ver. 1.2

Catchment Data

Project Name: MODA Plaza Parking
 Project Address: City of Milwaukie

Catchment ID: A

Date: 04/21/14

Permit Number: _____

Designer: Matt Klym
 Company: OTAK, Inc.

Run Time 5/12/2010 8:41:09 AM

Drainage Catchment Information

Catchment ID	A	
	Catchment Area	
Impervious Area	5,926	SF
Impervious Area	0.14	ac
Impervious Area Curve Number, CN_{imp}	98	
Time of Concentration, T_c , minutes	5	min.

Site Soils & Infiltration Testing Data

Infiltration Testing Procedure:	Open Pit Falling Head	
Native Soil Field Tested Infiltration Rate (I_{test}):	1	in/hr
Bottom of Facility Meets Required Separation From High Groundwater Per BES SWMM Section 1.4:	Yes	

Correction Factor Component

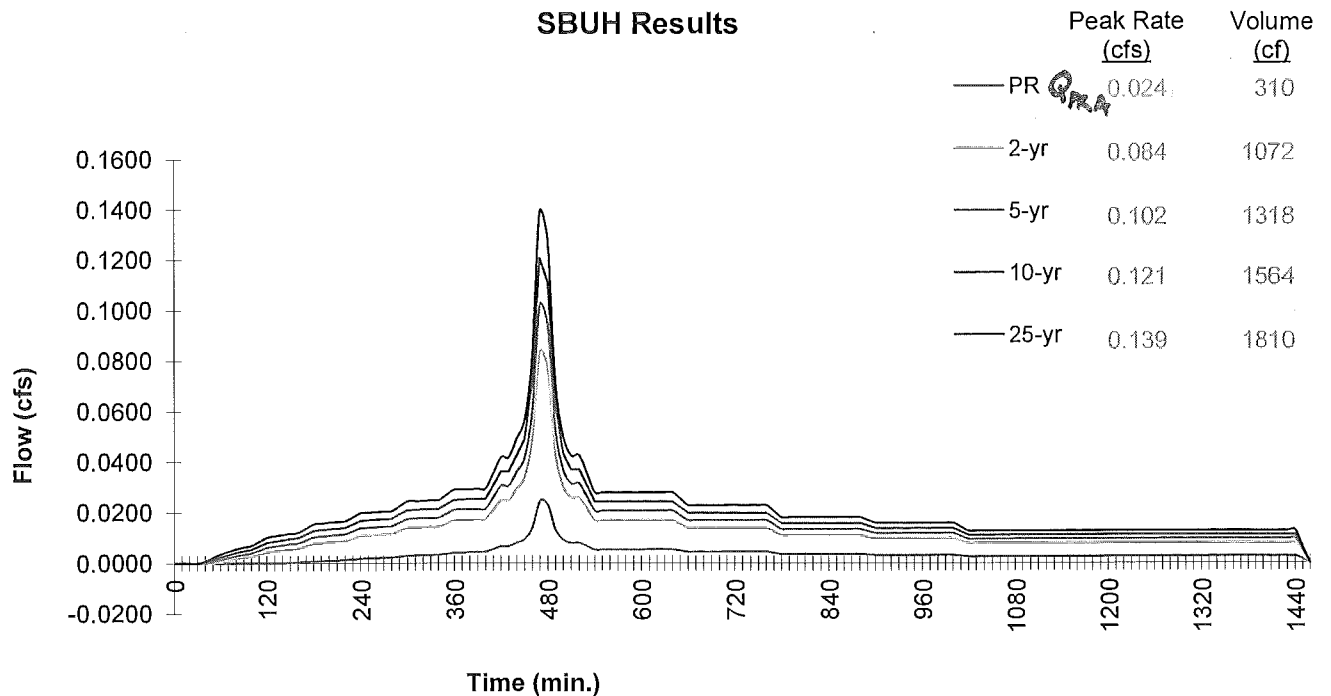
CF_{test} (ranges from 1 to 3)	2
----------------------------------	---

Design Infiltration Rates

I_{dsgn} for Native (I_{test} / CF_{test}):	0.50	in/hr
I_{dsgn} for Imported Growing Medium:	2.00	in/hr

Execute SBUH Calculations

SBUH Results





Presumptive Approach Calculator ver. 1.2

Catchment Data

Project Name: MODA Plaza Parking
 Project Address: City of Milwaukie

Catchment ID: B

Date: 04/21/14

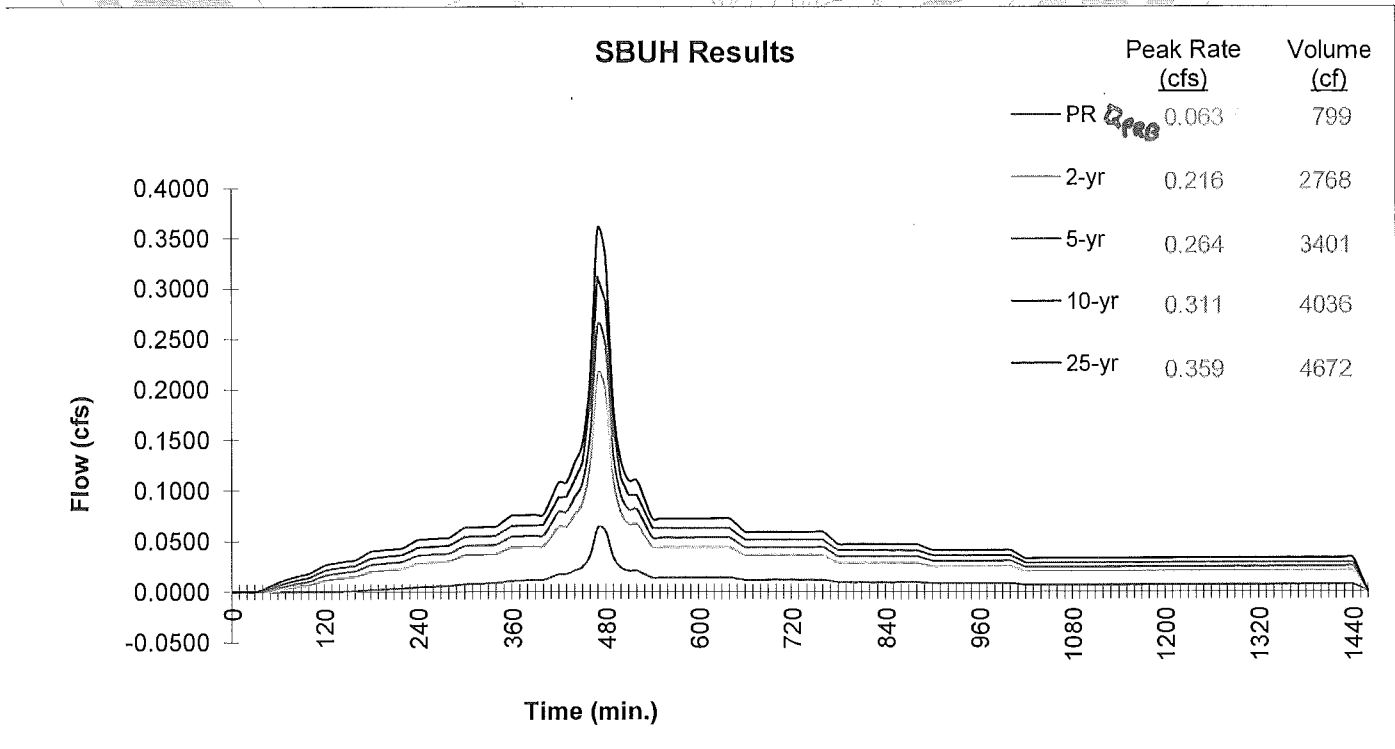
Permit Number: _____

Designer: Matt Klym
 Company: OTAK, Inc.

Run Time 5/12/2010 8:41:09 AM

Drainage Catchment Information	
Catchment ID	B
Catchment Area	
Impervious Area	15,295 SF
Impervious Area	0.35 ac
Impervious Area Curve Number, CN_{imp}	98
Time of Concentration, T_c , minutes	5 min.
Site Soils & Infiltration Testing Data	
Infiltration Testing Procedure:	Open Pit Falling Head
Native Soil Field Tested Infiltration Rate (I_{test}):	1 in/hr
Bottom of Facility Meets Required Separation From High Groundwater Per BES SWMM Section 1.4:	Yes
Correction Factor Component	
CF_{test} (ranges from 1 to 3)	2
Design Infiltration Rates	
I_{dsgn} for Native (I_{test} / CF_{test}):	0.50 in/hr
I_{dsgn} for Imported Growing Medium:	2.00 in/hr

Execute SBUH Calculations



Matt Klym

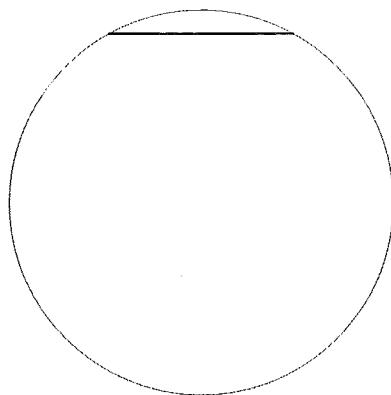
Matt Klym 13:29 21-Apr-14

Project 17134

MODA

GRAVITY PIPE FLOW (Chezy-Manning)

Main Pipe



□ 2"

diameter = 12.0"

slope = 1.00%

material: ABS, PVC

Manning's n = 0.011

depth of flow = 93.82% of diameter (max)

wetted perimeter = 2.64'

area = 0.77 s.f.

hydraulic radius = 0.29'

velocity = 5.93 fps

flow = 4.54 cfs

STORMFILTER CATCHBASIN DESIGN NOTES

STORMFILTER TREATMENT CAPACITY IS A FUNCTION OF THE CARTRIDGE SELECTION AND THE NUMBER OF CARTRIDGES. 1 CARTRIDGE CATCHBASIN HAS A MAXIMUM OF ONE CARTRIDGE. SYSTEM IS SHOWN WITH A 27" CARTRIDGE, AND IS ALSO AVAILABLE WITH AN 18" CARTRIDGE. STORMFILTER CATCHBASIN CONFIGURATIONS ARE AVAILABLE WITH A DRY INLET BAY FOR VECTOR CONTROL. PEAK HYDRAULIC CAPACITY PER TABLE BELOW. IF THE SITE CONDITIONS EXCEED PEAK HYDRAULIC CAPACITY, AN UPSTREAM BYPASS STRUCTURE IS REQUIRED.

CARTRIDGE SELECTION

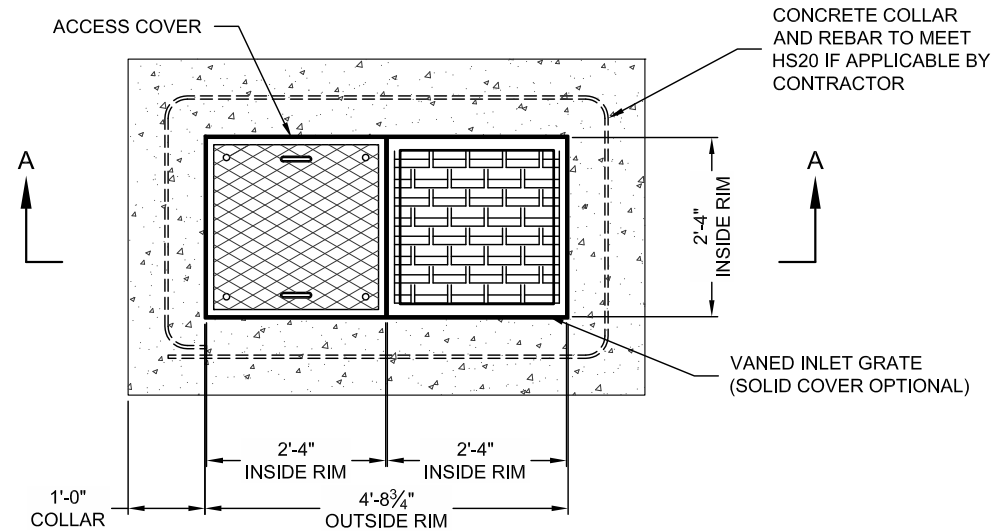
CARTRIDGE HEIGHT	27"		18"		18" DEEP	
MINIMUM HYDRAULIC DROP (H)	3.05'		2.3'		3.3'	
SPECIFIC FLOW RATE (gpm/sf)	2 gpm/ft ²	1 gpm/ft ²	2 gpm/ft ²	1 gpm/ft ²	2 gpm/ft ²	1 gpm/ft ²
CARTRIDGE FLOW RATE (gpm)	22.5	11.25	15	7.5	15	7.5
PEAK HYDRAULIC CAPACITY	1.0		1.0		1.8	
INLET PERMANENT POOL LEVEL (A)	1'-0"		1'-0"		2'-0"	
OVERALL STRUCTURE HEIGHT (B)	4'-9"		3'-9"		4'-9"	

GENERAL NOTES

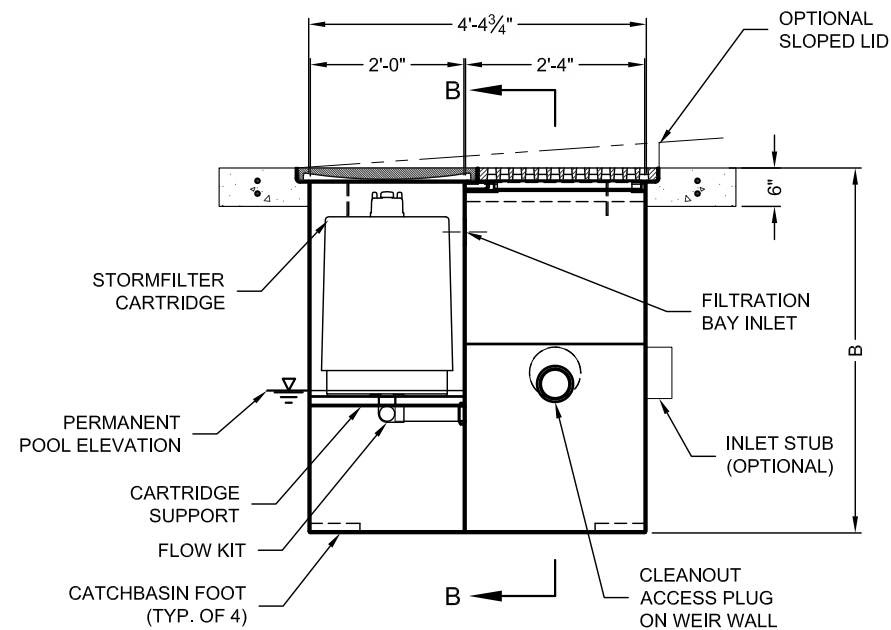
- CONTECH TO PROVIDE ALL MATERIALS UNLESS NOTED OTHERWISE.
- FOR SITE SPECIFIC DRAWINGS WITH DETAILED STORMFILTER CATCHBASIN STRUCTURE DIMENSIONS AND WEIGHTS, PLEASE CONTACT YOUR CONTECH ENGINEERED SOLUTIONS LLC REPRESENTATIVE. www.ContechES.com
- STORMFILTER CATCHBASIN WATER QUALITY STRUCTURE SHALL BE IN ACCORDANCE WITH ALL DESIGN DATA AND INFORMATION CONTAINED IN THIS DRAWING.
- INLET SHOULD NOT BE LOWER THAN OUTLET. INLET (IF APPLICABLE) AND OUTLET PIPING TO BE SPECIFIED BY ENGINEER AND PROVIDED BY CONTRACTOR.
- STORMFILTER CATCHBASIN EQUIPPED WITH 4 INCH (APPROXIMATE) LONG STUBS FOR INLET (IF APPLICABLE) AND OUTLET PIPING. STANDARD OUTLET STUB IS 8 INCHES IN DIAMETER. MAXIMUM OUTLET STUB IS 15 INCHES IN DIAMETER. CONNECTION TO COLLECTION PIPING CAN BE MADE USING FLEXIBLE COUPLING BY CONTRACTOR.
- STEEL STRUCTURE TO BE MANUFACTURED OF 1/4 INCH STEEL PLATE. CASTINGS SHALL MEET AASHTO M306 LOAD RATING. TO MEET HS20 LOAD RATING ON STRUCTURE, A CONCRETE COLLAR IS REQUIRED. WHEN REQUIRED, CONCRETE COLLAR WITH QUANTITY (2) #4 REINFORCING BARS TO BE PROVIDED BY CONTRACTOR.
- FILTER CARTRIDGES SHALL BE MEDIA-FILLED, PASSIVE, SIPHON ACTUATED, RADIAL FLOW, AND SELF CLEANING. RADIAL MEDIA DEPTH SHALL BE 7-INCHES. FILTER MEDIA CONTACT TIME SHALL BE AT LEAST 37 SECONDS.
- SPECIFIC FLOW RATE IS EQUAL TO THE FILTER TREATMENT CAPACITY (gpm) DIVIDED BY THE FILTER CONTACT SURFACE AREA (sq ft).

INSTALLATION NOTES

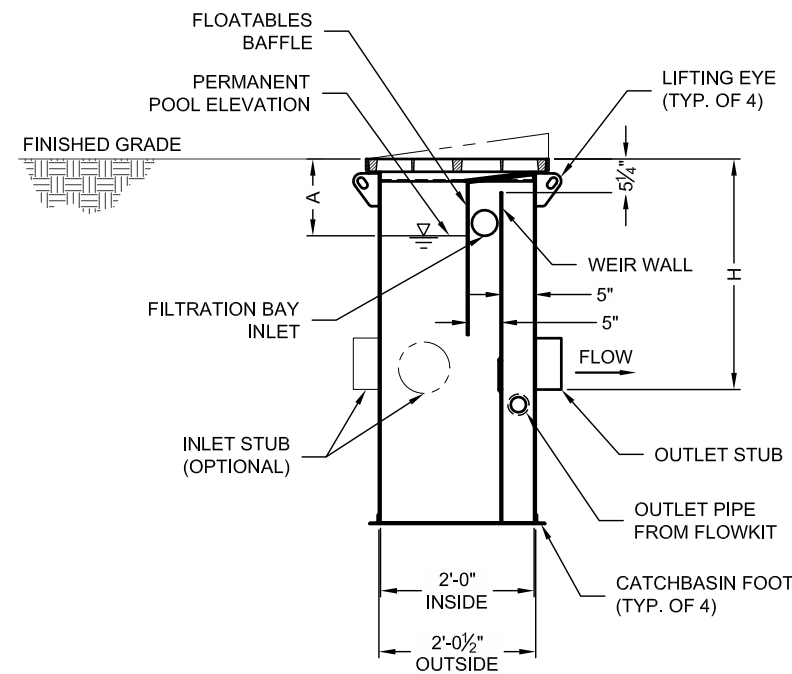
- ANY SUB-BASE, BACKFILL DEPTH, AND/OR ANTI-FLOTATION PROVISIONS ARE SITE-SPECIFIC DESIGN CONSIDERATIONS AND SHALL BE SPECIFIED BY ENGINEER OF RECORD.
- CONTRACTOR TO PROVIDE EQUIPMENT WITH SUFFICIENT LIFTING AND REACH CAPACITY TO LIFT AND SET THE CATCHBASIN (LIFTING CLUTCHES PROVIDED).
- CONTRACTOR TO TAKE APPROPRIATE MEASURES TO PROTECT CARTRIDGES FROM CONSTRUCTION-RELATED EROSION RUNOFF.



PLAN VIEW



SECTION A-A



SECTION B-B

1-CARTRIDGE CATCHBASIN STORMFILTER DATA	
STRUCTURE ID	XXX
WATER QUALITY FLOW RATE (cfs)	X.XX
PEAK FLOW RATE (<1 cfs)	X.XX
RETURN PERIOD OF PEAK FLOW (yrs)	XXX
CARTRIDGE FLOW RATE (gpm)	XX
MEDIA TYPE (CSF, PERLITE, ZPG, GAC, PHS)	XXXXX
RIM ELEVATION	XXX.XX'
PIPE DATA:	I.E. DIAMETER
INLET STUB	XXX.XX' XX"
OUTLET STUB	XXX.XX' XX"
CONFIGURATION	
SLOPED LID	YES/NO
SOLID COVER	YES/NO
NOTES/SPECIAL REQUIREMENTS:	

STORMFILTER CATCHBASIN DESIGN NOTES

STORMFILTER TREATMENT CAPACITY IS A FUNCTION OF THE CARTRIDGE SELECTION AND THE NUMBER OF CARTRIDGES. 2 CARTRIDGE CATCHBASIN HAS A MAXIMUM OF TWO CARTRIDGES. SYSTEM IS SHOWN WITH A 27" CARTRIDGE, AND IS ALSO AVAILABLE WITH AN 18" CARTRIDGE. STORMFILTER CATCHBASIN CONFIGURATIONS ARE AVAILABLE WITH A DRY INLET BAY FOR VECTOR CONTROL. PEAK HYDRAULIC CAPACITY PER TABLE BELOW. IF THE SITE CONDITIONS EXCEED PEAK HYDRAULIC CAPACITY, AN UPSTREAM BYPASS STRUCTURE IS REQUIRED.

CARTRIDGE SELECTION

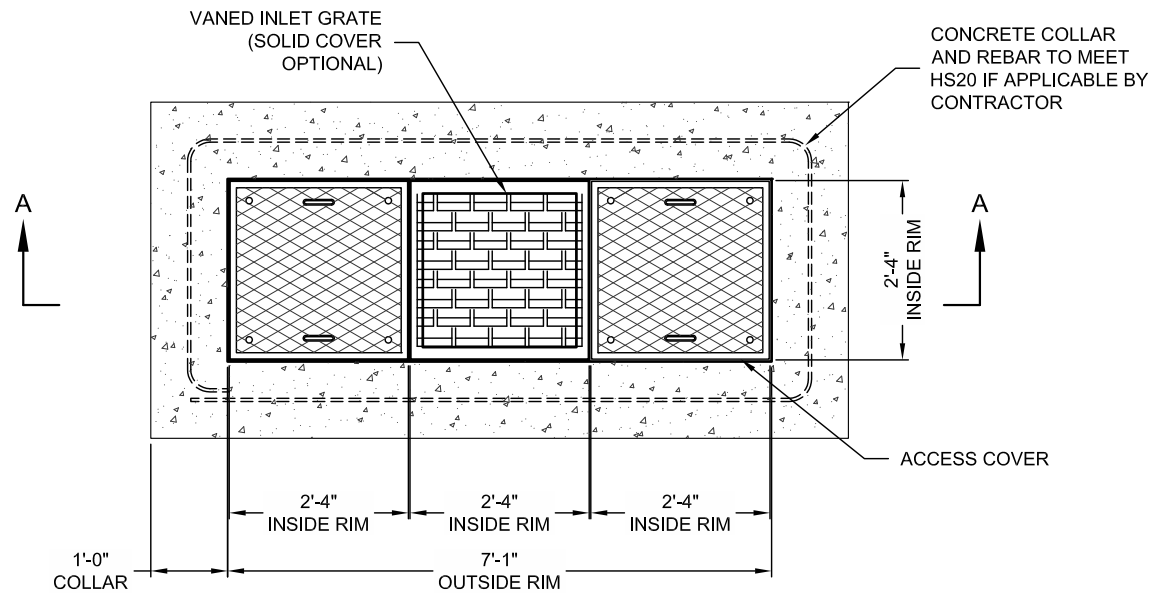
CARTRIDGE HEIGHT	27"		18"		18" DEEP	
MINIMUM HYDRAULIC DROP (H)	3.05'		2.3'		3.3'	
SPECIFIC FLOW RATE (gpm/sf)	2 gpm/ft ²	1 gpm/ft ²	2 gpm/ft ²	1 gpm/ft ²	2 gpm/ft ²	1 gpm/ft ²
CARTRIDGE FLOW RATE (gpm)	22.5	11.25	15	7.5	15	7.5
PEAK HYDRAULIC CAPACITY	1.0		1.0		1.8	
INLET PERMANENT POOL LEVEL (A)	1'-0"		1'-0"		2'-0"	
OVERALL STRUCTURE HEIGHT (B)	4'-9"		3'-9"		4'-9"	

GENERAL NOTES

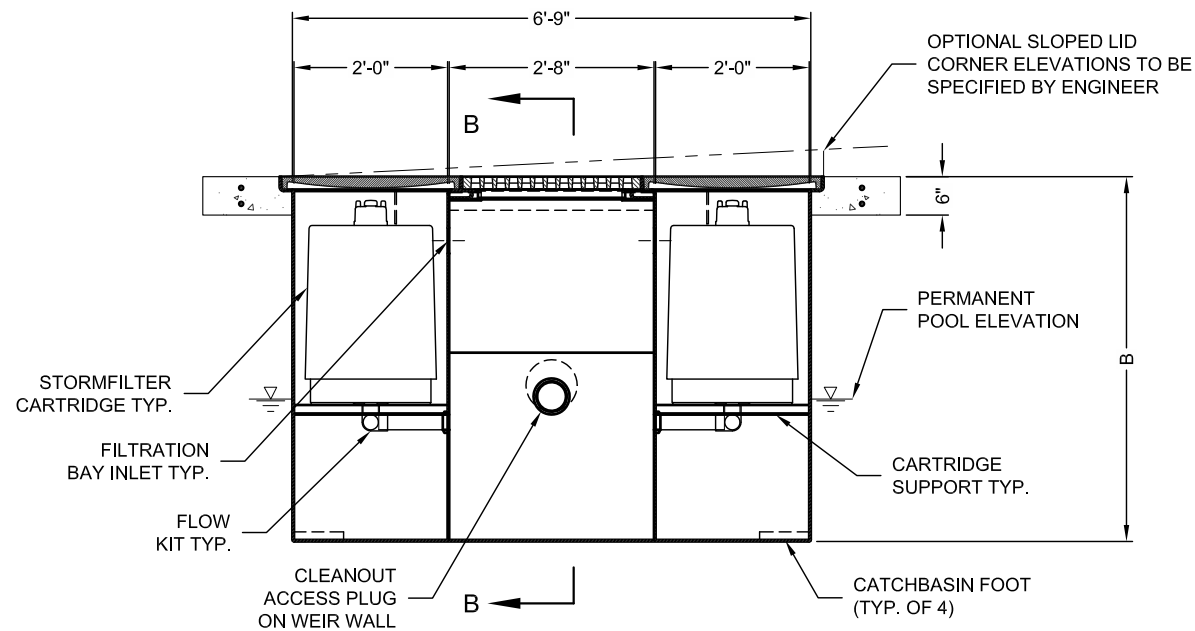
- CONTECH TO PROVIDE ALL MATERIALS UNLESS NOTED OTHERWISE.
- FOR SITE SPECIFIC DRAWINGS WITH DETAILED STORMFILTER CATCHBASIN STRUCTURE DIMENSIONS AND WEIGHTS, PLEASE CONTACT YOUR CONTECH ENGINEERED SOLUTIONS LLC REPRESENTATIVE. www.ContechES.com
- STORMFILTER CATCHBASIN WATER QUALITY STRUCTURE SHALL BE IN ACCORDANCE WITH ALL DESIGN DATA AND INFORMATION CONTAINED IN THIS DRAWING.
- INLET SHOULD NOT BE LOWER THAN OUTLET. INLET (IF APPLICABLE) AND OUTLET PIPING TO BE SPECIFIED BY ENGINEER AND PROVIDED BY CONTRACTOR.
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INSTALLATION NOTES

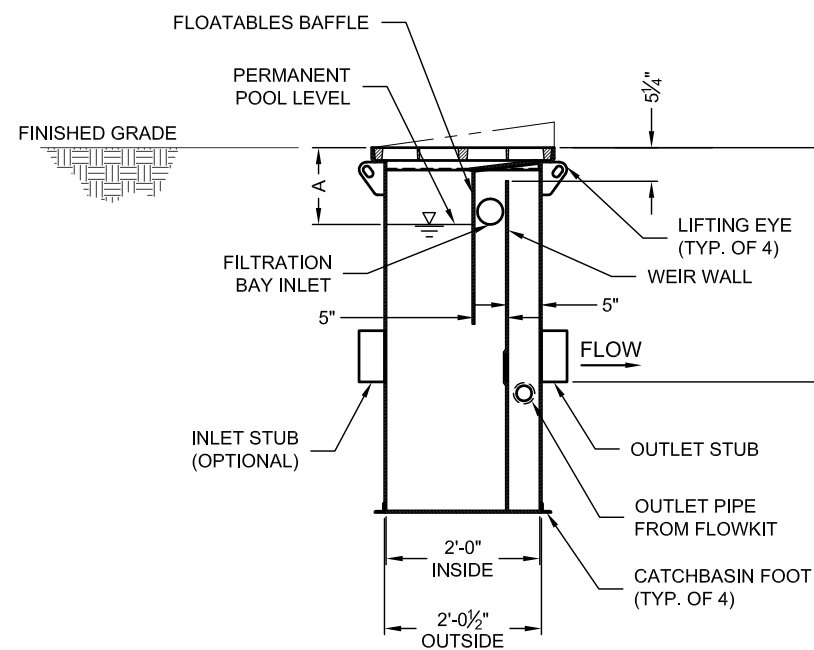
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PLAN VIEW



SECTION A-A

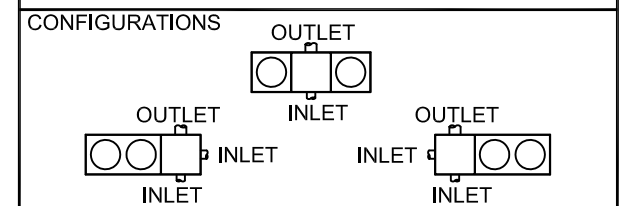


SECTION B-B

2-CARTRIDGE DEEP CATCHBASIN STORMFILTER DATA

STRUCTURE ID	XXX
WATER QUALITY FLOW RATE (cfs)	X.XX
PEAK FLOW RATE (<1.8 cfs)	X.XX
RETURN PERIOD OF PEAK FLOW (yrs)	XXX
CARTRIDGE FLOW RATE (gpm)	XX
MEDIA TYPE (CSF, PERLITE, ZPG, GAC, PHS)	XXXXX
RIM ELEVATION	XXX.XX'

PIPE DATA:	I.E.	DIAMETER
INLET STUB	XXX.XX'	XX"
OUTLET STUB	XXX.XX'	XX"



SLOPED LID	YES/NO
SOLID COVER	YES/NO
NOTES/SPECIAL REQUIREMENTS:	

Plan Set



HanmiGlobal Partner

XREF LIST
 Ltscale: 1
 Resolved
 ParkL3A
 S246B190
 A246s102_50504
 D246X230
 A246X385
 D246X190
 A246XBDR
 RGTSTAMP
 P17134X001
 ---Images---
 ge n
 VICINITY MAP

MODA HEALTH PLAZA PARKING

WESTERN PARKING LOT EXPANSION

CLACKAMAS COUNTY, OREGON

OWNER/APPLICANT

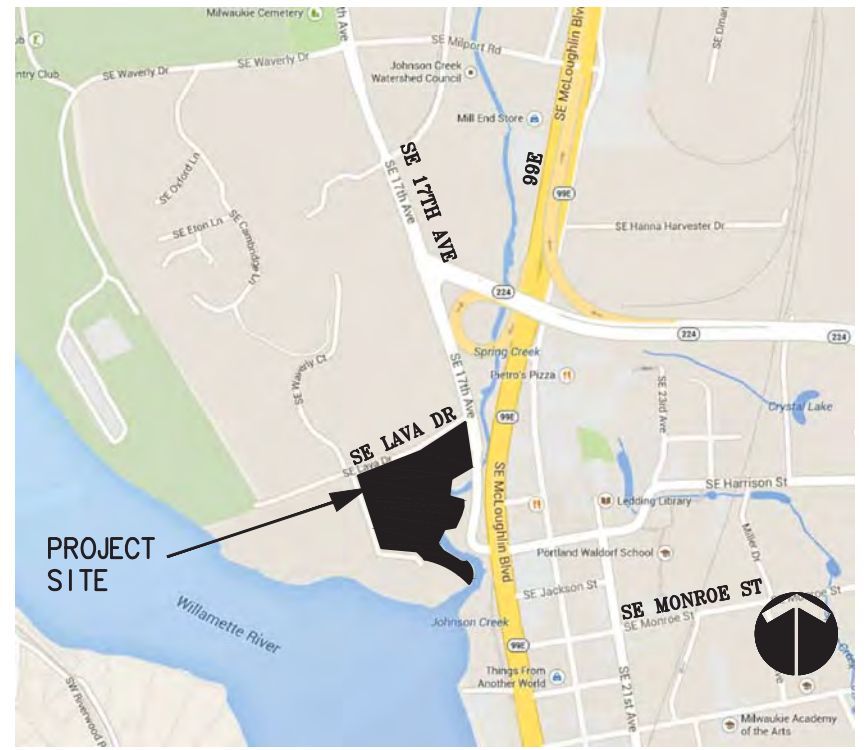
NAME: MODA HEALTH
 CONTACT: DAVID SHAPPER
 ADDRESS: 601 SW 2ND AVE
 PORTLAND, OR 97204-3156
 PHONE: (503) 948-5564
 EMAIL: david.shaffer@modahealth.com

CIVIL ENGINEER/SURVEYOR/PLANNER/ARCHITECT

NAME: OTAK INCORPORATED
 PLANNER: JERRY OFFER
 ENGINEER: TROY KENT, P.E.
 ARCHITECT: PETER MILLER
 ADDRESS: 808 SW THIRD AVENUE, SUITE 300
 PORTLAND, OR 97204
 PHONE: (503) 287-6825
 FAX: (503) 415-2304

SHEET INDEX

- P0.0 COVER SHEET
- P1.0 EXISTING CONDITIONS
- P1.1 OVERALL MODA HEALTH SITE PLAN
- P1.2 WESTERN PARKING LOT SITE PLAN
- P2.0 SITE GRADING AND TREE REMOVAL PLAN
- L1.0 LANDSCAPE PLAN
- L1.1 PLANTING NOTES AND DETAILS



VICINITY MAP
 N.T.S.



PROJECT MAP
 N.T.S.

SITE INFORMATION

CLACKAMAS COUNTY, OREGON
 1S 1E 35AA: TAX LOT 3500
 314,128 SF / 7.21 ACRES

LAND USE RESUBMITTAL - APRIL 25, 2014

MODA HEALTH PLAZA PARKING
 CITY OF MILWAUKIE, OREGON

COVER SHEET



otak
 HammiGlobal Partner
 808 SW 3rd Ave., Ste. 300
 Portland, OR 97204
 Phone: (503) 287-6825
 Fax: (503) 415-2304
 www.otak.com

17134 P17134P0.0
 Project No. Drawing No.

P0.0

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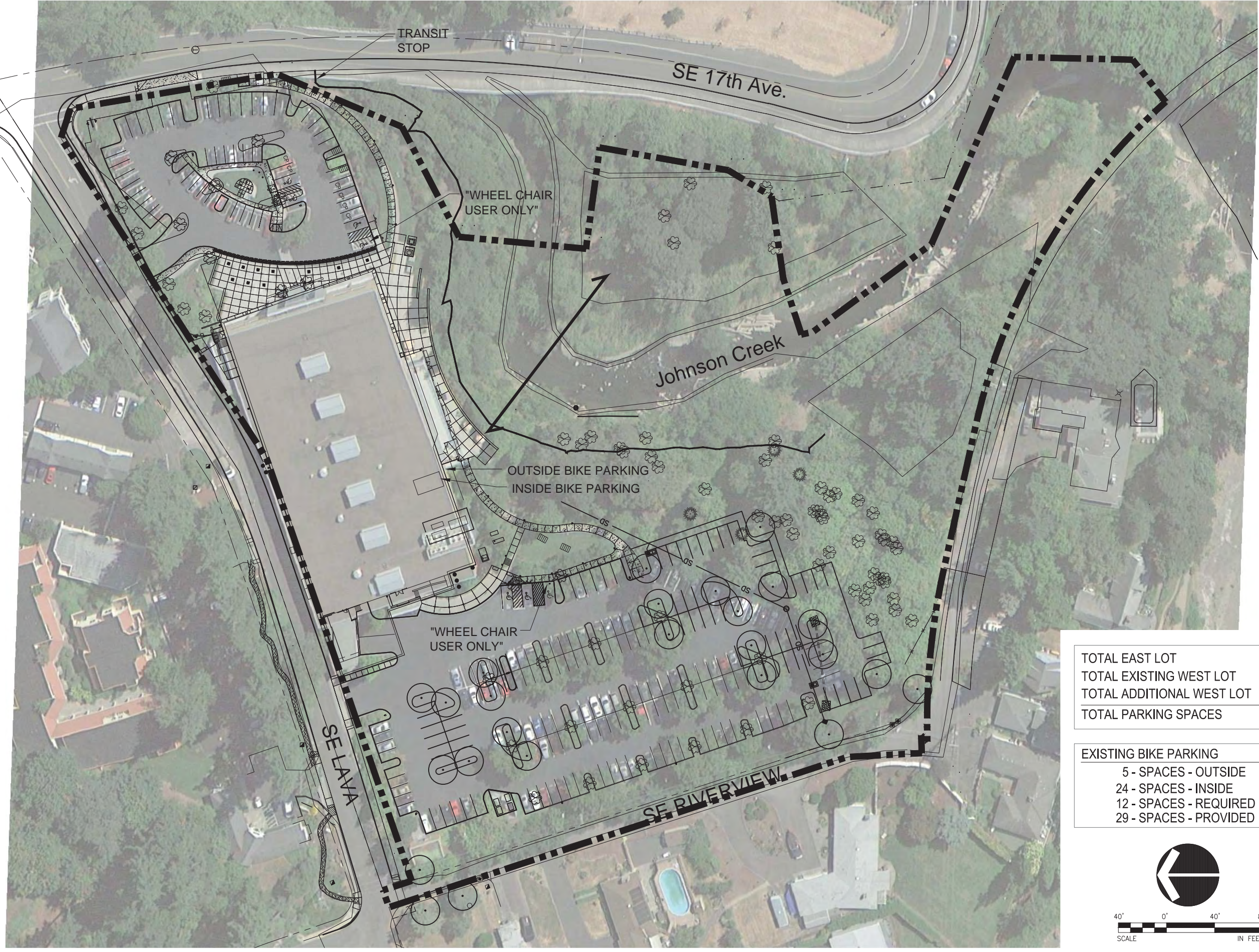
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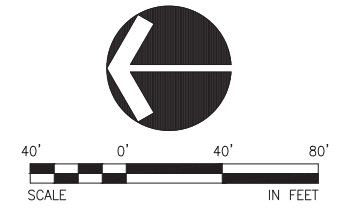
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 D246X230
 A246X385
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 P17134X600
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TOTAL EAST LOT	56
TOTAL EXISTING WEST LOT	150
TOTAL ADDITIONAL WEST LOT	30
TOTAL PARKING SPACES	236

EXISTING BIKE PARKING	
5 - SPACES - OUTSIDE	
24 - SPACES - INSIDE	
12 - SPACES - REQUIRED	
29 - SPACES - PROVIDED	



LAND USE RESUBMITTAL - APRIL 25, 2014

MODA HEALTH PLAZA PARKING
 CITY OF MILWAUKIE, OREGON

OVERALL MODA HEALTH SITE PLAN

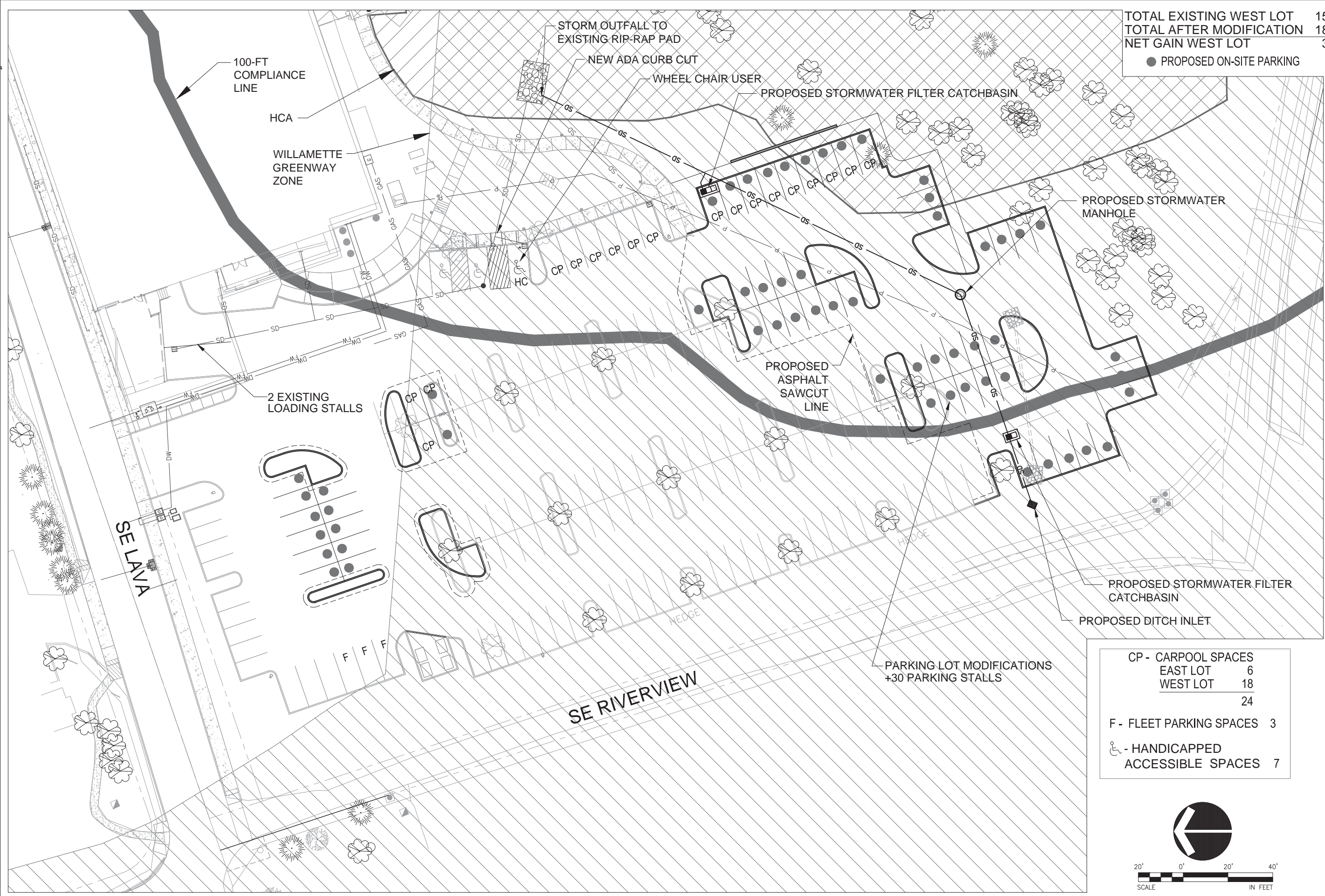


17134 P17134P1.1
 Project No. Drawing No.
P1.1
 Sheet No.
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 D246X600
 ---Images---
 HCA zones

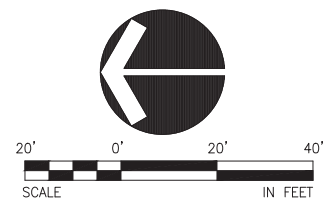


TOTAL EXISTING WEST LOT 150
 TOTAL AFTER MODIFICATION 180
 NET GAIN WEST LOT 30

● PROPOSED ON-SITE PARKING

CP - CARPOOL SPACES	
EAST LOT	6
WEST LOT	18
	24
F - FLEET PARKING SPACES	3
♿ - HANDICAPPED ACCESSIBLE SPACES	7

PARKING LOT MODIFICATIONS
 +30 PARKING STALLS



LAND USE RESUBMITTAL - APRIL 25, 2014

MODA HEALTH PLAZA PARKING
 CITY OF MILWAUKIE, OREGON

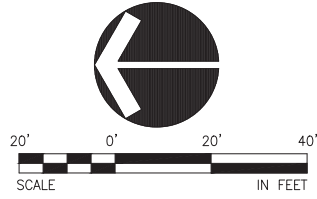
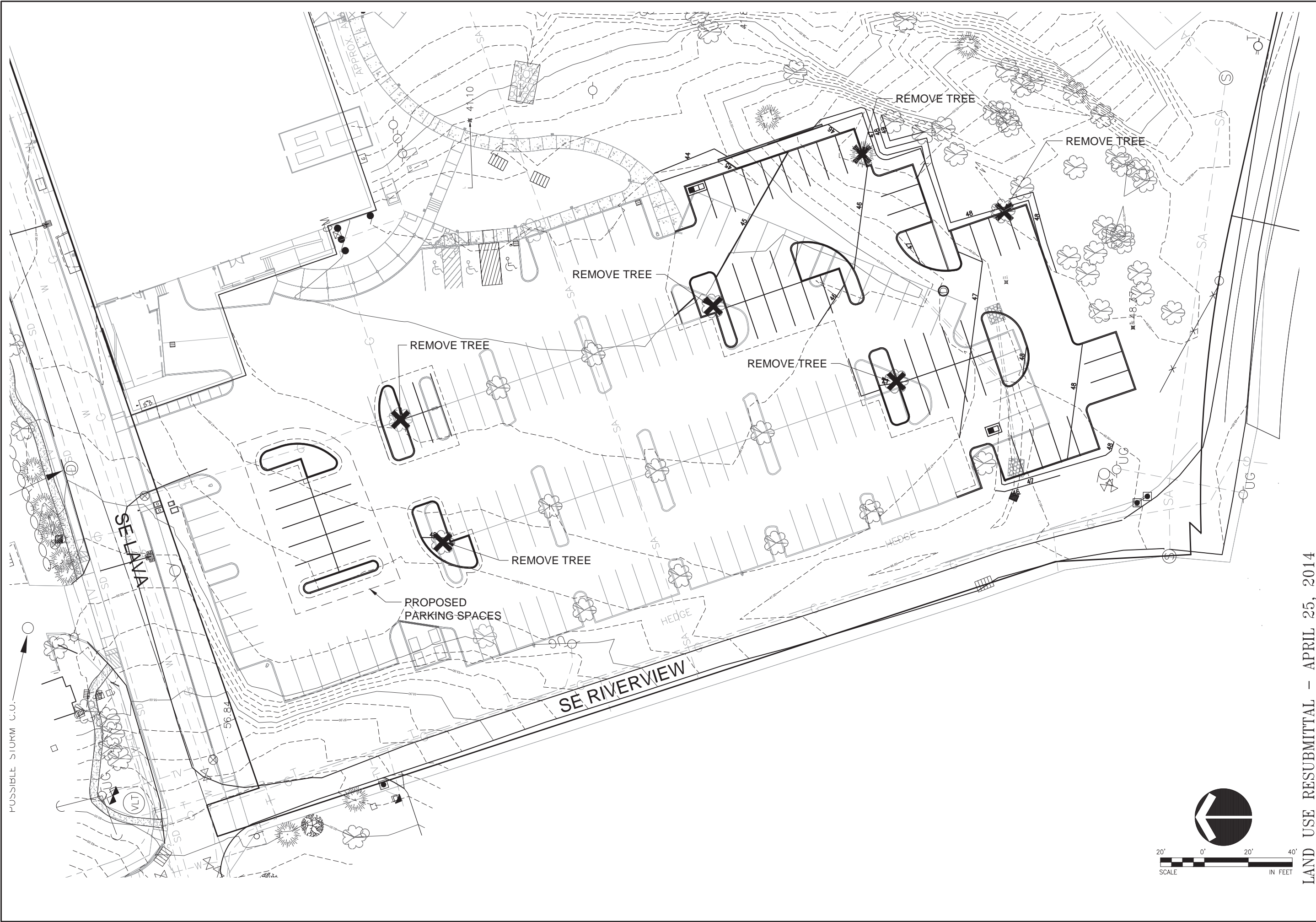
WESTERN PARKING LOT SITE PLAN

otak
 HammiGlobal Partner
 808 SW 3rd Ave., Ste. 300
 Portland, OR 97204
 Phone: (503) 287-6825
 Fax: (503) 415-2304
 www.otak.com

17134 P17134P1.2
 Project No. Drawing No.
P1.2
 Sheet No.
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NO.	DATE	BY	REVISION COMMENTS

Plotter: Apr 24, 2014 - 10:03am matk L:\Project\17100\17134\DWG\17134P1.2.dwg Layout Name: P1.2



LAND USE RESUBMITTAL - APRIL 25, 2014

MODA HEALTH PLAZA PARKING
CITY OF MILWAUKIE, OREGON

SITE GRADING AND TREE REMOVAL PLAN



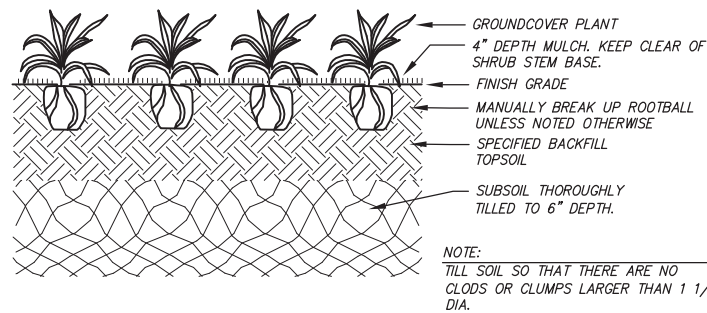
HammiGlobal Partner
808 SW 3rd Ave., Ste. 300
Portland, OR 97204
Phone: (503) 287-6825
Fax: (503) 415-2304
www.otak.com

17134 P17134P2.0
Project No. Drawing No.

P2.0
Sheet No.
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NO.	DATE	BY	REVISION COMMENTS

Design	Drawn	Checked	Date	Initial Issue Date:
MRK	KY	TDK		



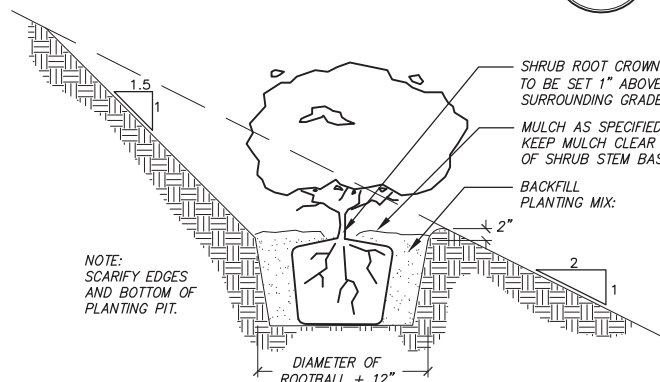
NOTE:
TILL SOIL SO THAT THERE ARE NO
CLODS OR CLUMPS LARGER THAN 1 1/2\"/>

GROUNDCOVER PLANTING DETAIL

5

L1.1

SCALE: N.T.S.



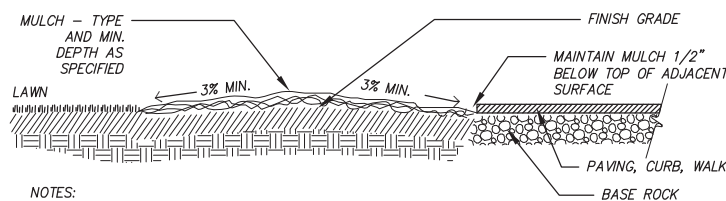
NOTE:
SCARIFY EDGES
AND BOTTOM OF
PLANTING PIT.

TREE/SHRUB PLANTING ON SLOPE

4

L1.1

SCALE: N.T.S.



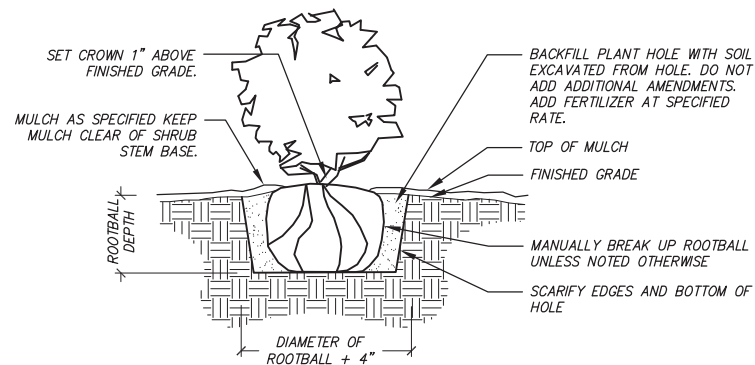
NOTES:
1. THE MINIMUM SLOPE FOR PLANTING AREAS IS 3%.

SECTION - PLANTING BED GRADING

3

L1.1

SCALE: N.T.S.



SHRUB PLANTING DETAIL

2

L1.1

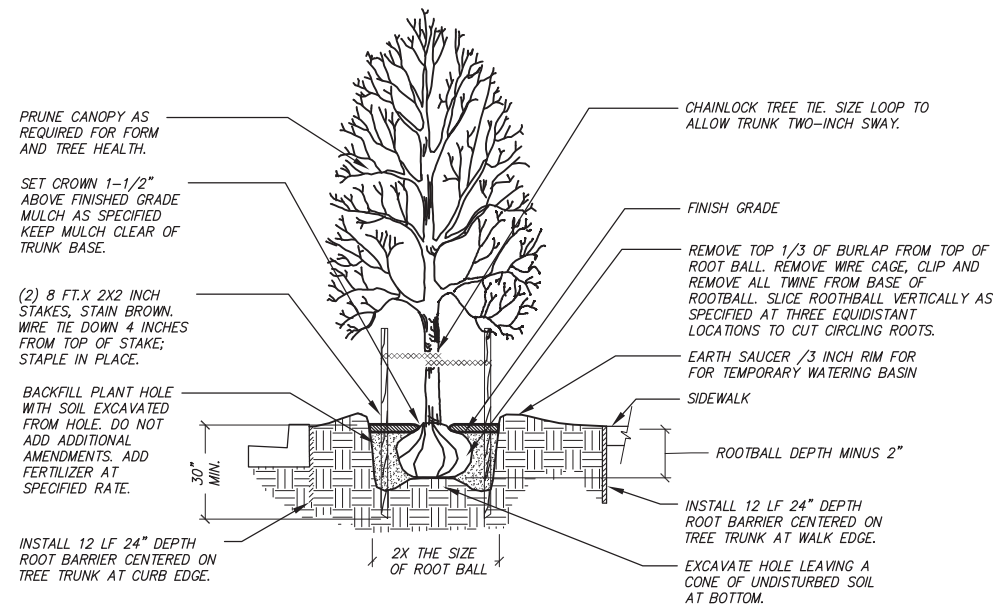
SCALE: N.T.S.

UTILITY NOTE:

CONTRACTOR IS CAUTIONED THAT EXISTING UNDERGROUND FACILITIES OCCUR THROUGHOUT THE WORK AREAS INCLUDING BUT NOT LIMITED TO POWER, GAS, TELEPHONE, WATER SUPPLY, AND IRRIGATION. CONTRACTOR SHALL MARK THE FACILITIES BEFORE WORK, POTHOLE WHERE NECESSARY, AND PROTECT DURING CONSTRUCTION. IMMEDIATELY NOTIFY OWNER'S REPRESENTATIVE IF ANY CONFLICTS ARE FOUND.

LANDSCAPE NOTES:

- GENERAL: SEVERAL OF THE FOLLOWING NOTES SUMMARIZE THE PROJECT SPECIFICATIONS FOR THE CONTRACTORS CONVENIENCE. IF A DISCREPANCY EXISTS BETWEEN THESE NOTES AND THE PROJECT SPECIFICATIONS THE SPECIFICATIONS SHALL OVERRIDE.
- THE LANDSCAPE CONTRACTOR IS TO THOROUGHLY REVIEW THE SITE. IF THERE ARE ANY DISCREPANCIES BETWEEN THE PLAN AND THE EXISTING CONDITIONS THE OWNERS REPRESENTATIVE IS TO BE NOTIFIED IMMEDIATELY.
- IF THE LANDSCAPE CONTRACTOR STARTS WORK BEFORE SITE CONDITIONS ARE READY OR CONTINUES WORK IN ADVERSE CONDITIONS WITHOUT PRIOR APPROVAL THEY WILL BE RESPONSIBLE FOR ANY ADDITIONAL COSTS RELATING TO THE CONDITION.
- IMMEDIATELY NOTIFY OWNERS REPRESENTATIVE CONCERNING ANY CONDITION AT ANY TIME DURING CONSTRUCTION THAT IS DETRIMENTAL TO THE HEALTH AND VIGOROUS GROWTH OF THE SPECIFIED PLANT MATERIAL.
- PROVIDE QUANTITY OF PLANT MATERIAL INDICATED IN PLANT LIST OR THE QUANTITY REQUIRED TO COVER AREAS INDICATED AT SPECIFIED SPACING, WHICHEVER IS GREATER.
- IF AN AREA DIFFERS SIGNIFICANTLY IN SIZE FROM THAT SCALED ON DRAWING AND REQUIRES MORE OR LESS MATERIAL THE OWNERS REPRESENTATIVE IS TO BE INFORMED.
- TOPSOIL: 8" DEPTH IMPORT TOPSOIL SHALL BE PREPARED AND AMENDED IN ACCORDANCE WITH THE DETAILS AND SPECIFICATIONS.
- SOIL ANALYSIS: CONTRACTOR SHALL OBTAIN A COPY OF OWNER-PROVIDED SOIL TEST AND AMEND AND FERTILIZE IN CONFORMANCE WITH RECOMMENDATIONS INDICATED IN THE REPORT.
- COMPOST: APPLY 3 INCH DEPTH SPECIFIED COMPOST OVER ALL PLANTING AREAS. TILL IN TO IMPORT TOPSOIL TO A DEPTH OF 6 INCHES.
- FERTILIZER: APPLY FERTILIZER TO ALL PLANT HOLES AND TURF AREAS OF THE TYPE, QUANTITY, APPLICATION METHOD, AND TIMING NOTED IN THE SPECIFICATIONS.
- BARK MULCH: SPREAD 3 INCH DEPTH FINE-MEDIUM GRADE FIR/HEMLOCK BARK OVER ALL SHRUB BEDS. KEEP BARK CLEAR OF TREE AND SHRUB STEM BASE.
- PLANTING POCKETS: BACK FILL PLANT HOLE WITH AMENDED SOIL EXCAVATED FROM HOLE. DO NOT ADD ADDITIONAL AMENDMENTS.
- PLANT MATERIAL: ALL PLANT MATERIAL SHALL MEET MINIMUM QUALITY AND SIZE REQUIREMENTS ESTABLISHED IN THE AMERICAN STANDARD FOR NURSERY STOCK GUIDELINES.
- LEAVE PLANT NAME IDENTIFICATION TAGS ON TEN PERCENT OF ALL TREES AND SHRUBS INSTALLED TO AID INSPECTORS IN VERIFYING THAT SPECIFIED PLANTS HAVE BEEN INSTALLED.
- PLANT CENTER OF SHRUBS A MINIMUM OF 24 INCHES FROM ADJACENT PAVING. PLANT CENTER OF GROUND COVERS A MINIMUM OF 18 INCHES FROM ADJACENT PAVING.
- WHERE PLANT BED SLOPE IS LESS THAN 3% MOUND PLANTING BED AREAS 3% MINIMUM FOR POSITIVE DRAINAGE.
- SEE SPECIFICATIONS FOR FINAL INSPECTION, MAINTENANCE, AND WARRANTY REQUIREMENTS UNIQUE TO THIS PROJECT.
- SEE SPECIFICATIONS FOR OTHER LANDSCAPE CONSTRUCTION REQUIREMENTS.

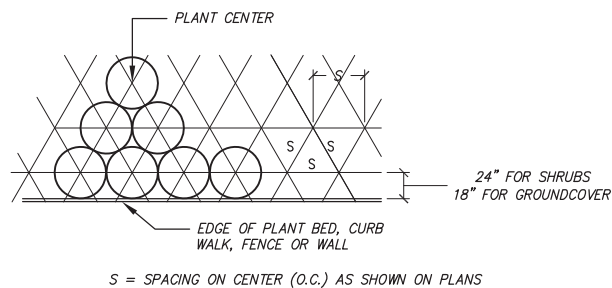


TREE PLANTING WITH ROOT BARRIER

7

L1.1

SCALE: N.T.S.

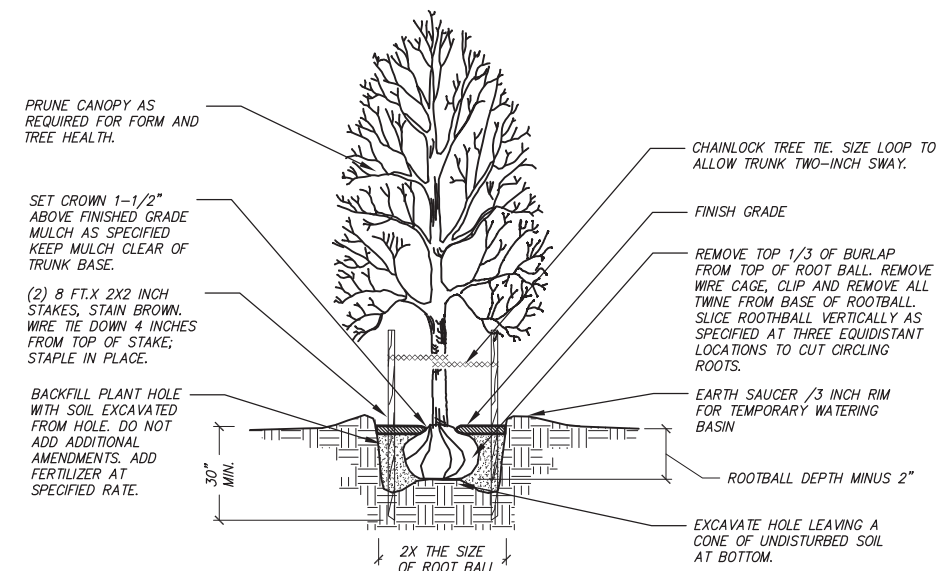


PLANT SPACING

6

L1.1

SCALE: N.T.S.



TREE PLANTING DETAIL

1

L1.1

SCALE: N.T.S.

LAND USE RESUBMITTAL - APRIL 25, 2014

MODA PLAZA PARKING
CITY OF MILWAUKIE, OREGON

PLANTING NOTES AND DETAILS



HammiGood Partner
808 SW 3rd Ave., Ste. 300
Portland, OR 97204
Phone: (503) 287-6825
Fax: (503) 415-2304
www.otak.com

17134 P17134L-SERIES
Project No. Drawing No.

L1.1
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NO.	DATE	BY	REVISION COMMENTS	Design	Drawn	Checked	Date	Initial Issue
				DDH	MLD	MAP		

Plotfile: Apr 24, 2014 - 10:54am dwdhd V:\PROJECTS\17100\17134\Draw\PT17134L1.dwg Layout Name: L1.1

**CITY OF MILWAUKIE
DESIGN AND LANDMARKS COMMITTEE
NOTES
Milwaukie City Hall
10722 SE Main St
MONDAY, MAY 19, 2014
6:30 PM**

COMMITTEE MEMBERS PRESENT

Greg Hemer, Chair
Sherry Grau, Vice Chair
Val Ballestrem
James Fossen

STAFF PRESENT

Li Alligood, Associate Planner (DLC Liaison)
Brett Kelper, Associate Planner

MEMBERS ABSENT

None

1.0 Call to Order – Procedural Matters

Chair Hemer called the meeting to order at 6:30 p.m. and read the conduct of meeting format into the record.

2.0 Design and Landmarks Committee Notes - None**3.0 Information Items**

Chair Hemer stated that he had been informed earlier in the day that he would be appointed to the Planning Commission on June 3. He felt confident that the remaining DLC members could carry the torch in his absence.

Li Alligood, Associate Planner, noted that with Chair Hemer's departure, the DLC now had two open positions, and encouraged members to share the word. She congratulated Chair Hemer on his appointment to the Planning Commission.

4.0 Audience Participation –This is an opportunity for the public to comment on any item not on the agenda. There was none.

5.0 Public Meetings

- 5.1 Summary: Moda Parking Lot Expansion
Applicant/Owner: Moda Health
Address: 10505 SE 17th Ave
File: DR-14-03
Staff Person: Brett Kelper, Associate Planner

Chair Hemer called the meeting to order and read the conduct of design review meeting format into the record.

Brett Kelper, Associate Planner, noted that Chair Hemer was sitting on the DLC to forward a recommendation to the Planning Commission and would be a member of the Planning Commission when the application came before that body. **Chair Hemer** stated that he would declare that situation at the Planning Commission hearing and allow the Commission to

determine whether he should recuse himself. **Ms. Alligood** noted that staff could request guidance from the City Attorney.

Mr. Kelper provided an overview of the application and staff recommendation via PowerPoint presentation.

- The applicant proposed to expand the existing parking lot on the site.
- Because the development was located within the Willamette Greenway Overlay WG and in the Downtown Office zone DO and Downtown Open Space DOS, the additions triggered Type III Willamette Greenway Review and Type III Downtown Design Review. Both applications required recommendations from the DLC to the Commission.
- The applicant had also proposed to use a contemporary parking lot light, similar to that already existing, rather than the ornamental style recommended by the Downtown Design Guidelines.

The Committee asked questions about the application.

- There were regulations that addressed the issue of light spill into natural resource areas.
- The off-street parking ratio was determined by the zoning ordinance; the applicant had requested approval to go over the maximum amount permitted.
- Staff did not know how long the additional parking would accommodate the activity on the site; the applicant could provide additional information.

Chair Hemer called for applicant testimony.

Jerry Offer, OTAK Incorporated, 808 SW 3rd Ave, Ste 300, Portland, spoke on behalf of the applicant. He had worked on the original adaptive reuse of the building on the site. The applicant agreed with the criteria identified by Mr. Kelper and with his recommendation for approval. Mr. Offer felt that the application met the approval criteria.

- He pointed out the guideline related to parking lot lights and stated that the applicant would like to continue to use the style of parking lot lights currently located on the site.
- The applicant preferred to use the existing lights because they did a better job of reducing light pollution than the recommended light fixtures. The site was not visible from the river and he did not believe removing trees on site would impact views to or from the river.

DLC Member James Fossen asked how long the expansion would serve the business. **Mr. Offer** said he was unable to project whether they would need more parking in the future. He noted that determining the amount of parking required for office uses was very difficult because different offices functioned differently. Moda did everything it could to encourage people to use alternative transportation but still required additional parking.

Chair Hemer called for testimony in support of the application.

Gary Klein, 10795 SE Riverway Ln, Milwaukie, stated his support for the application. He noted that the Moda building had previously be the Pendleton Woolen Mills building. He liked the current parking lot lighting because it did not spread the light as much as other fixtures. The trees provided screening. He stated that Moda had been excellent neighbors and partners to the Johnson Creek Watershed Council when they installed restoration plantings. He noted that the condominiums created more of a parking challenge than Moda.

Roger Forni, 10683 SE Riverway Ln, Milwaukie, stated his support for the application. Moda had been a great neighbor. He and his son loved the lighting the way it was, and they wouldn't want them to do anything differently.

Chair Hemer called for neutral testimony. There was none.

Chair Hemer called for testimony in opposition to the application. There was none.

Chair Hemer called for additional comments from staff. **Mr. Kelper** noted that the Planning Commission hearing on the application was scheduled for June 10.

Chair Hemer called for additional questions from the Committee. There were none.

Chair Hemer called for applicant rebuttal. There was none.

Chair Hemer closed the public testimony portion of the meeting.

The Committee discussed the proposal.

- Agreed that continuing the use of the existing lighting fixtures was appropriate, as the site was not adjacent to downtown Milwaukie.
- Felt that the application met the approval criteria.

DLC Member Sherry Grau moved to recommend approval of DR-14-03 and the recommended findings and conditions of approval. DLC Member Fossen seconded the motion. The motion was approved unanimously.

6.0 Worksession Items - None

7.0 Other Business/Updates

7.1 Officer Elections

Ms. Alligood noted that since Chair Hemer was being appointed to the Planning Commission, the Committee would need to elect a new Chair and Vice Chair.

DLC Member Fossen nominated DLC Member Grau for the position of Chair. DLC Member Val Ballestrem seconded the motion. The motion was approved unanimously.

DLC Member Hemer nominated DLC Member Ballestrem for the position of Vice Chair. DLC Member Fossen seconded the motion. The motion was approved unanimously.

7.2 June Meeting

Ms. Alligood noted that there were no agenda items scheduled for June 2, and suggested the DLC cancel the meeting. **The Committee** agreed.

8.0 Design and Landmarks Committee Discussion Items - None

9.0 Forecast for Future Meetings:

June 2, 2014	1. Cancelled
July 7, 2014	1. TBD

Meeting adjourned at approximately 8:33 p.m.

Respectfully submitted,

Li Alligood, Associate Planner

Sherry Grau, Chair

DRAFT