



# CITY OF MILWAUKIE

## AGENDA

### REVISED

May 14, 2019

#### PLANNING COMMISSION

City Hall Council Chambers  
10722 SE Main Street  
[www.milwaukieoregon.gov](http://www.milwaukieoregon.gov)

- 1.0 Call to Order - Procedural Matters** — 6:30 PM
- 2.0 Planning Commission Minutes** — None
- 3.0 Information Items**
- 4.0 Audience Participation** — This is an opportunity for the public to comment on any item not on the agenda
- 5.0 Public Hearings** — Public hearings will follow the procedure listed on reverse
  - 5.1 Summary: Conditional Use for Radiant Yoga  
Applicant/Owner: Michael Eisenberg/Lauren Eisenberg  
Address: 4000 SE International Way, Suite F202  
File: CU-2019-001  
Staff: Brett Kelter, Associate Planner
  - 5.2 Summary: Milwaukie High School Parking Lot  
Applicant/Owner: 3J Consulting/North Clackamas School District  
Address: 2301 SE Willard St  
File: CSU-2019-002  
Staff: Brett Kelter, Associate Planner
- 6.0 Worksession Items**
- 7.0 Planning Department Other Business/Updates**
- 8.0 Planning Commission Committee Updates and Discussion Items** — This is an opportunity for comment or discussion for items not on the agenda.
- 9.0 Forecast for Future Meetings:**
  - May 28, 2019
    - 1. Public Hearing: VR-2019-001 McFarland Site Height Variance
    - 2. Public Hearing: NR-2018-005 Elk Rock Estates
  - June 11, 2019
    - 1. Public Hearing: WG-2019-002 Proposed New Dock
  - June 25, 2019
    - 1. Public Hearing: VR-209-004 Home Occupation Variance
    - 2. Joint Session with DLC for Design Review Code Amendments

## Milwaukie Planning Commission Statement

The Planning Commission serves as an advisory body to, and a resource for, the City Council in land use matters. In this capacity, the mission of the Planning Commission is to articulate the Community's values and commitment to socially and environmentally responsible uses of its resources as reflected in the Comprehensive Plan

- 1. PROCEDURAL MATTERS.** If you wish to speak at this meeting, please fill out a yellow card and give to planning staff. Please turn off all personal communication devices during meeting. For background information on agenda items, call the Planning Department at 503-786-7600 or email [planning@milwaukieoregon.gov](mailto:planning@milwaukieoregon.gov). Thank you.
- 2. PLANNING COMMISSION and CITY COUNCIL MINUTES.** City Council and Planning Commission minutes can be found on the City website at [www.milwaukieoregon.gov/meetings](http://www.milwaukieoregon.gov/meetings).
- 3. FORECAST FOR FUTURE MEETING.** These items are tentatively scheduled, but may be rescheduled prior to the meeting date. Please contact staff with any questions you may have.
- 4. TIME LIMIT POLICY.** The Commission intends to end each meeting by 10:00pm. The Planning Commission will pause discussion of agenda items at 9:45pm to discuss whether to continue the agenda item to a future date or finish the agenda item.

### Public Hearing Procedure

Those who wish to testify should come to the front podium, state his or her name and address for the record, and remain at the podium until the Chairperson has asked if there are any questions from the Commissioners.

- 1. STAFF REPORT.** Each hearing starts with a brief review of the staff report by staff. The report lists the criteria for the land use action being considered, as well as a recommended decision with reasons for that recommendation.
- 2. CORRESPONDENCE.** Staff will report any verbal or written correspondence that has been received since the Commission was presented with its meeting packet.
- 3. APPLICANT'S PRESENTATION.**
- 4. PUBLIC TESTIMONY IN SUPPORT.** Testimony from those in favor of the application.
- 5. NEUTRAL PUBLIC TESTIMONY.** Comments or questions from interested persons who are neither in favor of nor opposed to the application.
- 6. PUBLIC TESTIMONY IN OPPOSITION.** Testimony from those in opposition to the application.
- 7. QUESTIONS FROM COMMISSIONERS.** The commission will have the opportunity to ask for clarification from staff, the applicant, or those who have already testified.
- 8. REBUTTAL TESTIMONY FROM APPLICANT.** After all public testimony, the commission will take rebuttal testimony from the applicant.
- 9. CLOSING OF PUBLIC HEARING.** The Chairperson will close the public portion of the hearing. The Commission will then enter into deliberation. From this point in the hearing the Commission will not receive any additional testimony from the audience, but may ask questions of anyone who has testified.
- 10. COMMISSION DISCUSSION AND ACTION.** It is the Commission's intention to make a decision this evening on each issue on the agenda. Planning Commission decisions may be appealed to the City Council. If you wish to appeal a decision, please contact the Planning Department for information on the procedures and fees involved.
- 11. MEETING CONTINUANCE.** Prior to the close of the first public hearing, any person may request an opportunity to present additional information at another time. If there is such a request, the Planning Commission will either continue the public hearing to a date certain, or leave the record open for at least seven days for additional written evidence, argument, or testimony. The Planning Commission may ask the applicant to consider granting an extension of the 120-day time period for making a decision if a delay in making a decision could impact the ability of the City to take final action on the application, including resolution of all local appeals.

*The City of Milwaukie will make reasonable accommodation for people with disabilities. Please notify us no less than five (5) business days prior to the meeting.*

#### **Milwaukie Planning Commission:**

Kim Travis, Chair  
John Henry Burns, Vice Chair  
Adam Argo  
Joseph Edge  
Greg Hemer  
Lauren Loosveldt  
Robert Massey

#### **Planning Department Staff:**

Denny Egner, Planning Director  
David Levitan, Senior Planner  
Brett Kelder, Associate Planner  
Vera Koliass, Associate Planner  
Mary Heberling, Assistant Planner  
Alicia Martin, Administrative Specialist II  
Patty Stewart, Administrative Specialist II



# CITY OF MILWAUKIE

**To:** Planning Commission

**Through:** Dennis Egner, Planning Director

**From:** Brett Kelter, Associate Planner

**Date:** May 10, 2019, for May 14, 2019, Public Hearing

**Subject:** **File:** CU-2019-001  
**Applicant:** Lauren Eisenberg (Radiant Yoga)  
**Property Owner:** PNWP LLC #2  
**Address:** 4000 SE International Way, Suite F202  
**Legal Description (Map & Tax Lot):** 1S1E36DA 0100  
**NDA:** (none)

---

## **ACTION REQUESTED**

Approve land use application master file #CU-2019-001 and adopt the recommended Findings and Conditions of Approval found in Attachments 1 and 2. This action would allow for a yoga studio to operate as indoor recreation in the Business Industrial (B-I) zone.

## **BACKGROUND INFORMATION**

According to City records, Radiant Yoga began operating as a business at the subject property in September 2018. In late January 2019, Planning staff was notified of a business registration submittal for Radiant Yoga and, as per its standard procedure to confirm that the new use was allowed by the underlying zone, contacted a representative to learn more about the operation. The business was described as a yoga instruction operation, with an office component, workshops and training, video preparation, and yoga classes open to the public.

The B-I zone allows professional offices as an outright use, and that classification is broad enough to encompass most of the operational aspects of Radiant Yoga apart from the yoga classes. While the classes can be understood to be part of the overall training that the business provides, because they are open to the public they are considered to be a form of indoor recreation, which is listed as a conditional use in the B-I zone and requires a formal land use application for conditional use approval.

**A. Site and Vicinity**

The project area is located on the second floor of the building located at 4000 SE International Way. The larger site is developed with 3 buildings that each include multiple suites, with a large off-street parking area shared by all tenants (see Figure 1).

**Figure 1.** Site and vicinity



The subject property is located on International Way in the western portion of the city’s primary business park area (zoned B-I), which stretches from Lake Rd on the east to 37<sup>th</sup> Ave on the west, between Railroad Ave to the north and Highway 224 to the south. The immediately surrounding area is comprised primarily of similar developments with multiple office-type buildings on large parcels with shared off-street parking.

**B. Zoning Designation**

Business Industrial (B-I, see Figure 2)

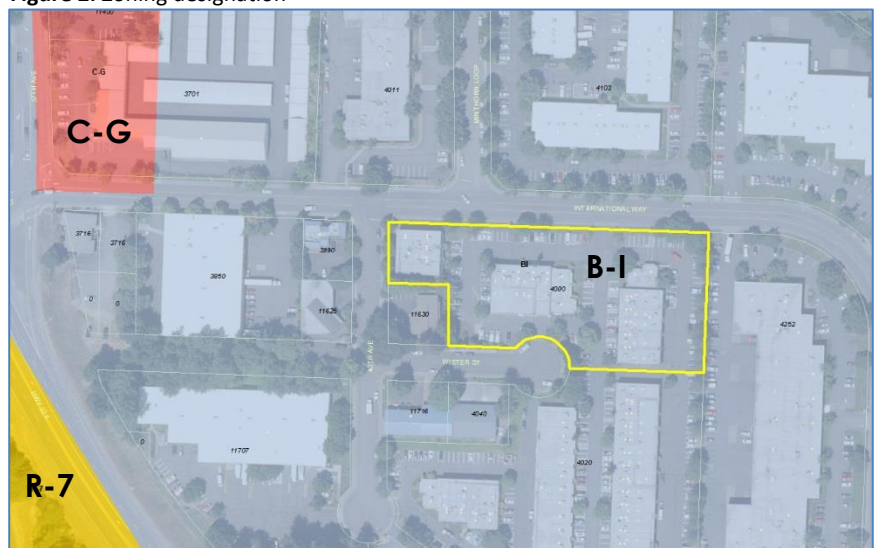
**C. Comprehensive Plan Designation**

Industrial (I)

**D. Land Use History**

City records indicate no previous land use actions for this site.

**Figure 2.** Zoning designation



## **E. Proposal**

As summarized by the applicant, the primary business of Radiant Yoga is to develop and implement therapeutic programs to teach yoga. The Radiant Yoga operation includes the following elements:

- Private yoga therapy (1-2 individuals at a time)
- Off-site retreats
- Weekend workshops
- Specific trainings on yoga anatomy, philosophy, and methodology
- Online yoga programs
- Blogging
- Video tutorials
- Teaching yoga at other businesses
- General office work
- Yoga classes

The applicant is seeking land use approval to officially allow the yoga classes as part of the overall operation of Radiant Yoga. Yoga classes are considered a form of indoor recreation, which is allowed in the underlying B-I zone as a conditional use.

The applicant estimates that the classes and training are less than 20% of the daily operation. The other aspects of the business can be understood as activities that would be allowed outright in the B-I zone as part of a business or professional office. For more detailed information on the proposal, see the Applicant's Materials in Attachment 2.

The project requires approval of the following application:

1. Conditional Use (file #CU-2019-001)

Indoor recreation is permitted as a conditional use in the B-I zone.

## **KEY ISSUES**

### **Summary**

Staff has identified the following key issues for the Planning Commission's deliberation. Aspects of the proposal not listed below are addressed in the Findings (see Attachment 1) and generally require less analysis and discretion by the Commission.

- A. Would the proposed yoga classes result in any impacts that require mitigation?

## Analysis

### A. Would the proposed yoga classes result in any impacts that require mitigation?

The Radiant Yoga operation includes a number of activities that are normal for certain types of business and professional offices. The approximately 1,150-sq-ft office space provides ample room for the small-scale trainings, workshops, and therapy sessions that are key services. The proposed yoga classes would easily be accommodated in that same space, with only 1 class at a time. The larger office park provides approximately 300 parking spaces (shared among 6 multi-tenant buildings) and is rarely used to full capacity, so ample parking is available on the site. The proposed class activity would not be very different in nature than the trainings and workshops that are otherwise understood to be an allowable part of a permitted professional office use in the B-I zone. No special impacts are anticipated, so staff does not believe any specific mitigation is required.

## CONCLUSIONS

### A. Staff recommendation to the Planning Commission is as follows:

1. Approve the Conditional Use application (CU-2019-001) to allow the indoor recreation aspect (yoga classes) of the existing Radiant Yoga business.
2. Adopt the attached Findings in Support of Approval.

Staff does not recommend any conditions of approval.

## CODE AUTHORITY AND DECISION-MAKING PROCESS

The proposal is subject to the following provisions of the Milwaukie Municipal Code (MMC):

- MMC Section 19.310 Business Industrial Zone (B-I)
- MMC Section 19.905 Conditional Uses
- MMC Section 19.1006 Type III Review

This application is subject to Type III review, which requires the Planning Commission to consider whether the applicant has demonstrated compliance with the code sections shown above. In Type III reviews, the Commission assesses the application against review criteria and development standards and evaluates testimony and evidence received at the public hearing.

The Commission has 4 decision-making options as follows:

- A. Approve the application subject to the recommended Findings and Conditions of Approval.
- B. Approve the application with modified Findings and Conditions of Approval. Such modifications need to be read into the record.
- C. Deny the application upon finding that it does not meet approval criteria.
- D. Continue the hearing.

The final decision on these applications, which includes any appeals to the City Council, must be made by July 23, 2019, in accordance with the Oregon Revised Statutes and the Milwaukie Zoning Ordinance. The applicant can waive the time period in which the application must be decided.

## COMMENTS

Notice of the proposed changes was given to the following agencies and persons: City of Milwaukie Building and Engineering Departments; City Attorney; Clackamas Fire District #1 (CFD#1); Clackamas County Department of Transportation and Development (DTD); Metro; TriMet; NW Natural; and properties within 300 ft of the subject site.

The following is a summary of the comments received by the City. See Attachment 4 for further details.

- Alex Roller, Engineering Tech II, City of Milwaukie Engineering Department:**  
 Comments related to the applicability of MMC Chapter 19.700 Public Facility Improvements to the proposed change in use. The proposed use corresponds with other uses classified as Business Park for purposes of evaluating traffic generation and would not result in an increase in trips. Therefore, MMC 19.700 is not applicable.

## ATTACHMENTS

Attachments are provided as indicated by the checked boxes. All material is available for viewing upon request.

|   | Early PC Mailing         | PC Packet                           | Public Copies                       | Packet                              |
|---|--------------------------|-------------------------------------|-------------------------------------|-------------------------------------|
| 1. Recommended Findings in Support of Approval  | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> |
| 2. Applicant's Narrative and Supporting Documentation (revisions stamped received on April 8, 2019) | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> |
| 3. Comments Received  | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> |

Key:

Early PC Mailing = paper materials provided to Planning Commission at the time of public notice 20 days prior to the hearing.

PC Packet = paper materials provided to Planning Commission 7 days prior to the hearing.

Public Copies = paper copies of the packet available for review at City facilities and at the Planning Commission meeting.

Packet = packet materials available online at <https://www.milwaukieoregon.gov/planning/cu-2019-001>.

**Recommended Findings in Support of Approval  
File #CU-2019-001  
Conditional Use for Radiant Yoga**

Sections of the Milwaukie Municipal Code (MMC) not addressed in these findings are found to be inapplicable to the decision on this application.

1. The applicant, Lauren Eisenberg, has applied for conditional use approval to allow a yoga studio (Radiant Yoga) as indoor recreation in the Business Industrial (B-I) zone. The project area is a suite (F202) within the building addressed as 4000 SE International Way, which is on Tax Lot 1S1E36DA00100 and zoned B-I. The land use application file number is CU-2019-001.
2. As summarized by the applicant, the primary business of Radiant Yoga is to develop and implement therapeutic programs to teach yoga. The Radiant Yoga operation includes the following elements:
  - Private yoga therapy (1-2 individuals at a time)
  - Off-site retreats
  - Weekend workshops
  - Specific trainings on yoga anatomy, philosophy, and methodology
  - Online yoga programs
  - Blogging
  - Video tutorials
  - Teaching yoga at other businesses
  - General office work
  - Yoga classes

The applicant is seeking land use approval to officially allow the yoga classes as part of the overall operation of Radiant Yoga. Yoga classes are considered a form of indoor recreation, which is allowed in the underlying B-I zone as a conditional use.

The applicant estimates that the classes and trainings are less than 20% of the daily operation. The other aspects of the business can be understood as activities that would be allowed outright in the B-I zone as part of a business or professional office.

3. The proposal is subject to the following provisions of the Milwaukie Municipal Code (MMC):
  - MMC Section 19.310 Business Industrial Zone (B-I)
  - MMC Section 19.905 Conditional Uses
  - MMC Section 19.1006 Type III Review



The applicant's submittal is an application for Conditional Use. The application has been processed and public notice provided in accordance with MMC Section 19.1006 Type III Review. The requirement for a preapplication conference was waived by the Planning Director, as allowed by MMC Subsection 19.1002.2.

Public notice was sent to property owners and current residents within 300 ft of the subject property on April 29, 2019. A public hearing was held on May 14, 2019, as required by law.

4. MMC Section 19.310 Business Industrial Zone (B-I)

MMC 19.310 establishes the allowable uses and development standards for the B-I zone.

a. MMC Subsection 19.310.2 Uses Permitted Outright

MMC 19.310.2 establishes the uses allowed outright in the B-I zone, including research or testing laboratories; manufacturing, processing, fabrication, or assembly; printing or publishing operations; trade schools primarily serving the local business community; business and professional offices; warehousing and distribution; contractors and related businesses; and other similar uses.

*As presented in the applicant's submittal materials, Radiant Yoga operates a professional office at the subject property. Most of the activities describing the operation, such as private therapy sessions, weekend workshops, specific trainings, online programming and blogging, production of video tutorials, and general office work can be understood as normal activities for a business or professional office.*

*The Planning Commission finds that much of the Radiant Yoga operation is an outright permitted use in the underlying B-I zone.*

b. MMC Subsection 19.310.5 Conditional Uses

MMC 19.310.5 establishes uses that are allowed with conditional use approval pursuant to MMC Section 19.905, including public and private community buildings, indoor and outdoor recreation facilities (such as health and exercise spas), and other similar uses developed to serve primarily the recreational needs of clients and employees of the district; mini-warehousing and mini-storage; limited uses that exceed 25% of the building's square footage as allowed by MMC Subsection 19.310.4; and marijuana producers and processors. This section also establishes approval criteria for conditional uses in the B-I zone, which are addressed with the conditional use findings in Finding 5.

*As described in the applicant's submittal materials and on the Radiant Yoga website ([www.radianttyogashala.com](http://www.radianttyogashala.com)), a regular schedule of yoga classes is available to the public with pre-registration. While not necessarily "recreational" in nature for all participants (e.g., the classes may be part of a meditational practice, therapy, or instruction and training for some participants), these classes constitute a form of indoor recreation.*

*The Planning Commission finds that the yoga classes proposed as part of the Radiant Yoga operation are subject to conditional use review for approval in the B-I zone.*

*As proposed, and as addressed elsewhere in these findings, the Planning Commission finds that the applicable standards of MMC 19.310 are met.*

5. MMC Section 19.905 Conditional Uses

MMC 19.905 establishes a process for evaluating certain uses that may be appropriately located in some zoning districts, if appropriate for the specific site on which they are proposed. MMC Subsection 19.905.2.A provides that the provisions of Section 19.905 apply to uses identified as a conditional use in the base zone in Chapter 19.300, such as those identified for the B-I zone in MMC Subsection 19.310.5. MMC Subsection 19.905.3.A provides that the establishment of a new conditional use shall be evaluated through a Type III review per Section 19.1006.

*The proposal to include yoga classes in the operation of Radiant Yoga at the subject property constitutes a request to provide an indoor recreational use in the B-I zone, which is identified as a conditional use in MMC 19.310.5 and subject to Type III review.*

a. MMC Subsection 19.905.4 Approval Criteria

MMC Subsection 19.905.4.A establishes the following approval criteria for establishment of a new conditional use:

- (1) The characteristics of the lot are suitable for the proposed use considering size, shape, location, topography, existing improvements, and natural features.

*The subject property is part of a larger 6.1-acre site developed with 6 multi-tenant office buildings and a large, shared, off-street parking area. Within the applicant's building (Building F) there are 8 tenant suites, including the applicant's second-floor project area (Suite F202), which is approximately 1,150 sq ft in area. The project area provides ample space for the Radiant Yoga operation, including the proposed yoga classes, and there is plenty of off-street parking available in the shared lot near the building.*

- (2) The operating and physical characteristics of the proposed use will be reasonably compatible with, and have minimal impact on, nearby uses.

*The subject property is located within the B-I zone along International Way and is surrounded by other similar business-industrial-type uses (e.g., light manufacturing, business and professional offices, etc.). As presented in the applicant's submittal materials, Radiant Yoga operates primarily during typical day-shift hours during the week, though with some activities offered in the evenings and on weekends. That schedule fits with that of other businesses in the area, while also providing opportunities for people who work during the day to access the services offered. Class sizes are relatively small (average of 5 people plus instructor) and would generate few conflicts over traffic or parking.*

- (3) All identified impacts will be mitigated to the extent practicable.

*No significant impacts are anticipated from the allowance of the proposed indoor recreation component (i.e., yoga classes) to the Radiant Yoga operation.*

- (4) The proposed use will not have unmitigated nuisance impacts, such as from noise, odor, and/or vibrations, greater than usually generated by uses allowed outright at the proposed location.

*The proposal to allow yoga classes as part of the Radiant Yoga operation would not result in any nuisance impacts.*

- (5) The proposed use will comply with all applicable development standards and requirements of the base zone, any overlay zones or special areas, and the standards in Section 19.905.

*The proposed conditional use would not physically change the subject property and would operate within the constraints of the existing development.*

*The quantity requirements for off-street parking established in MMC Table 19.605.1 include a requirement of 2 spaces per 1,000 sq ft general office uses and 3 spaces per 1,000 sq ft for indoor recreation. The project area is just over 1,100 sq ft in area, requiring 2-3 spaces. According to the applicant's materials, 28 parking spaces are located within 100 ft of the building and 57 total spaces are available within 250 ft. On the larger 6.1-acre office park site, 6 buildings with 60-65 tenant spaces share approximately 300 parking spaces. There is enough parking on the subject property to support the proposed conditional use.*

*MMC Subsection 19.310.5.A establishes the following approval criteria for conditional uses in the B-I zone:*

- (a) Will have minimal adverse impact on the appropriate development of uses permitted outright on abutting properties and the surrounding area considering location, size, design, and operating characteristics of the use.

*The proposal to allow yoga classes as part of the Radiant Yoga operation would not adversely impact the future development of other permitted uses elsewhere on the subject property or abutting properties. The classes would occur easily within the applicant's allotted floor area and there is adequate parking for permitted uses in the surrounding parking lot.*

- (b) Is compatible with the character and scale of uses allowed within the district and on a site no larger than necessary for the use and operational requirements of the use.

*The proposed yoga classes would occur within the parameters of the overall Radiant Yoga operation in Suite F202, intermixed with the trainings, workshops, and other office activities associated with the business. The scale of operation is similar to and compatible with that of other uses allowed in the B-I zone.*

- (c) Will provide vehicular and pedestrian access, circulation, parking, and loading areas which are compatible with uses on the same site or adjacent sites.

*Participants in the proposed yoga classes would use the existing, shared off-street parking lot provided for the larger office park. Participants would access the project area in the same way that other Radiant Yoga clients do, using the existing parking spaces and pedestrian walkways that serve the larger development. The proposed conditional use would not present any new or different impacts to the site.*

- (d) Is a needed service/product in the district, considering the mix of potential clientele and the need to maintain high-quality development in a highly visible area.

*The proposed yoga classes would be a welcome addition to the health and wellness services available in the B-I zone. The sample class schedule indicates that many classes would be offered during the 12noon to 1pm lunch hour as well as after 5pm, making them available to employees of other businesses within the district. The applicant's materials also indicate a commitment to doing outreach to other businesses in the community to make yoga classes available as an amenity at other workplaces.*

- (6) The proposed use is consistent with applicable Comprehensive Plan policies related to the proposed use.

*Chapter 4 (Land Use): Economic Base & Industrial/Commercial Land Use Element, Objective 2 (Employment Opportunity) – Policies in this section include those that encourage the retention of existing businesses and recruitment of new businesses that provide long-term employment opportunities, as well as those that encourage new professional and service-oriented employment opportunities to meet the needs of city residents.*

*Allowing the proposed yoga classes as part of the Radiant Yoga operation would allow the business to more fully actualize its programmatic philosophy and provide a wider range of services that would allow it to succeed in the community over the long term. The trainings and workshops that the business offers include opportunities for a variety of independent instructors to practice their craft as well as for new instructors to become certified and begin a professional career. The proposed yoga classes are an integral part of the holistic operation of Radiant Yoga.*

- (7) Adequate public transportation facilities and public utilities will be available to serve the proposed use prior to occupancy pursuant to Chapter 19.700.

*The proposed conditional use would not increase trips to the office park as a whole, which is assumed to have a mix of uses that may vary over time according to specific tenancies. International Way, the adjacent public street, is adequate to serve the proposed conditional use, as are all other existing public facilities in the area.*

*The Planning Commission finds that the proposed indoor recreation aspect of the Radiant Yoga business operation meets the approval criteria for conditional uses as established in MMC 19.905.4.A. This standard is met.*

*As proposed, the Planning Commission finds that the senior and retirement housing aspect of the proposed senior housing development meets the applicable standards of MMC 19.905 and is approvable as a conditional use.*

6. The application was referred to the following departments and agencies on March 26, 2019:

- Milwaukie Building Department
- Milwaukie Engineering Department
- City Attorney
- Clackamas Fire District #1 (CFD#1)
- Clackamas County Department of Transportation and Development (DTD)
- Metro
- TriMet
- NW Natural

In addition, notice of the public hearing was mailed to owners and residents of properties within 300 ft of the subject property on April 29, 2019.

The comments received are summarized as follows:

- **Alex Roller, Engineering Tech II, City of Milwaukie Engineering Department:** Comments related to the applicability of MMC Chapter 19.700 Public Facility Improvements to the proposed change in use. The proposed use corresponds with other uses classified as Business Park for purposes of evaluating traffic generation and would not result in an increase in trips. Therefore, MMC 19.700 is not applicable.

## ATTACHMENT 2

Radiant Yoga Conditional Use Application to the Milwaukie Planning Commission

TO: MILWAUKIE PLANNING  
FROM: RADIANT YOGA  
4000 SE INTERNATIONAL WAY, STE F202  
MILWAUKIE OREGON 97222

April 10, 2019

RE: APPLICATION FOR LAND USE ACTION – CONDITIONAL USE

- Application forms attached:

Application for Land Use Action – Conditional Use  
Submittal Requirements  
Preapplication Conference Waiver  
Floor Plan and Lease Information  
Check for Fee

- Ownership

Lauren Eisenberg is the sole proprietor of Radiant Yoga located at 4000 SE International Way, STE F202, Milwaukie OR 97222. The space is 1,152 square feet.

Ms. Eisenberg is a 1999 graduate of the University of Oregon with a Bachelor of Science degree as well as earning her 200-hour yoga alliance certification from the White Lotus Foundation.

- Business Description

Radiant Yoga is a new start-up business that chose to locate in the Milwaukie, Oregon area.

The primary business of Radiant Yoga is to teach yoga by developing and implementing therapeutic programs. Programs consist of Private Yoga Therapy with one or two individuals at a time; off-site Retreats; weekend Workshops; Specific Trainings on Yoga Anatomy, Yoga Philosophy, and Yoga Methodology; Classes; Online Yoga Programs; Blogging; Video Tutorials; and bringing yoga into the workplace (where Radiant goes and teaches at other businesses); and implementing community service projects.

The space at 4000 SE International Way will be used for office work – running the nuts and bolts of the Radiant Yoga business. Note that the office work described here is carried out by the owner (Lauren Eisenberg) with one assistant.

When these trainings and classes are in progress, average attendance is 5 students plus teacher. These sessions comprise less than 20% of the daily use with close to ½ of these sessions taking place either after 5 pm on weekdays or on the weekends.

The impact to the building (4000 SE International Way) and surrounding businesses is very low. Radiant Yoga has reached out to its neighboring businesses in the Industrial Park to introduce itself and to establish a friendly working relationship.

*revisions*  
RECEIVED

APR 08 2019

CITY OF MILWAUKIE  
PLANNING DEPARTMENT

## Radiant Yoga Conditional Use Application to the Milwaukie Planning Commission

This new business, Radiant Yoga, is creating current working opportunities for 10 rotating instructors that address different skills required to teach yoga therapy, yoga in the workplace, workshops, trainings, and offsite retreats.

- Base Zone Standards, Zone B1

Under Uses Permitted Outright (19.310.2), Radiant Yoga is permitted to use the office space for filming yoga training sessions for online use; for photography; for planning off-site retreats; for general office work.

The office serves as Lauren Eisenberg's (owner) Radiant Yoga Headquarters and primary work area where the use of the office space includes development of the Radiant Yoga web site and online yoga and scholastic programs; writing work related articles; blogging; vlogging; research; creating classroom curriculum; developing community outreach programs as well as developing community/volunteer opportunities.

Under Limited Uses (19.310.4), Radiant Yoga provides personal services through one-on-one private yoga sessions for clients that include certification training in yoga, yoga therapy, prenatal yoga, trauma informed yoga, yoga for mental health. This requires less than 25% of the office's square footage.

Under Conditional Uses (19.310.5), Radiant Yoga will have minimal adverse impact on the appropriate development of uses permitted outright on abutting properties and the surrounding area. It is also compatible with the character and scale of uses allowed within the district and on site.

The clientele we are serving includes a broad range of ages from young adults to seniors – men and women - and covers the Milwaukie community and Clackamas County. Radiant Yoga is open to the public with pre-registration required. A Community Scholarship program has also been established to provide funding for those in the community that want to participate in specific yoga programs but cannot afford the cost.

Classes and workshops address members of the community who are looking to train as yoga teachers, yoga therapists, and to learn new skill sets to better serve our community. These skill sets include but are not limited to anatomy, physiology, biomechanics, self-care, yoga for anxiety, yoga for depression, pre-natal yoga training, philosophy class, and breathwork. Some of the classes and workshops include:

Crystal Bowl Sound Bath – addresses the parasympathetic nervous system which is the relaxation response.

Spring Yin Yoga – improves sleep by stimulating the Liver and Gallbladder.

## Radiant Yoga Conditional Use Application to the Milwaukie Planning Commission

Trauma Sensitive Yoga and Sound Healing Senses – teaches survivors of trauma how to better cope with their symptoms.

Conscious Breathing for Healing and Awakening – resolves stress and anxiety.

### Sample of Monthly Schedule:

Monday: 6-9:30 am Lauren office work; 9:30-10:45 am class/training; 10:45-12 noon Lauren office work; 12-1 pm Class; 1-5:45 pm Lauren office work; 5:45-7 pm class

Tuesday: 6-10 am Lauren office work; 10-11:15 am class; 11:15-12 noon Lauren office work; 12-1 pm class; 1-5:15 pm Lauren office work; 5:15-6:15 pm class; 7:00-8:00 pm training/workshop event

Wednesday: 6-9:30 am Lauren office work; 9:30-10:45 am class; 10:45-12 noon Lauren Office work; 12-1 pm Class; 1-5:45 pm Lauren Office work; 5:45-7 pm class

Thursday: 6-10 am Lauren office work; 10-11:15 am class; 11:15-12 noon Lauren office work; 12-1 pm class; 1-5:15 pm Lauren office work; 5:15-6:15 pm class; 7-8 pm training/workshop event

Friday: 6-9:30 am Lauren office work; 9:30-10:45 am class; 10:45-12 noon Lauren office work; 12-1 pm Class; 1-5:45 pm Lauren Office work; 5:45-7 pm class

Saturday: 9-10:30 am class; 1-2:15 pm class; 2:15 -6 pm workshops, trainings, Lauren Office work

Sunday: 9-10:30 am class; 11-12:15 pm class; 12:15 -6 pm workshops, trainings, Lauren Office work

- Off-Street Parking

There is ample parking available for the described office (under business description). In Table 19.605.1 under section D, 3 to 5.5 spaces per 1,000 square feet are allowed. In the attached site plan, Exhibit B, ample parking is available for Radiant Yoga without impeding or interfering with neighboring businesses. In addition, there is an assigned space for F202.

- Conditional Uses

Radiant Yoga meets the approval criteria (19.905.4) as follows:

The 1,152 square feet of space are suitable for the described use by Radiant Yoga. It has ample and dedicated office space, an area for filming and photography, and the square footage needed for training and teaching sessions.

The operating and physical characteristics of the proposed use will be compatible with, and have minimal impact on, nearby businesses. As outlined above, "Radiant Yoga will have minimal adverse impact on the appropriate development of uses permitted outright on abutting properties and the surrounding area. It is also compatible with the character and scale of uses allowed within the district and on site."



## Radiant Yoga Conditional Use Application to the Milwaukie Planning Commission

Radiant Yoga has met with the surrounding businesses to ensure it has identified and mitigated any and all impacts to those businesses.

It should also be noted that the Radiant Yoga business will not have unmitigated nuisance impacts. It is not of a business type which produces dust, odor, smoke, fumes, noise, glare, heat, or vibrations which are incompatible with other uses allowed in this zone; and the use does not produce off-site impacts that create nuisance as defined by the Oregon D.E.Q. and the City Noise Ordinance.

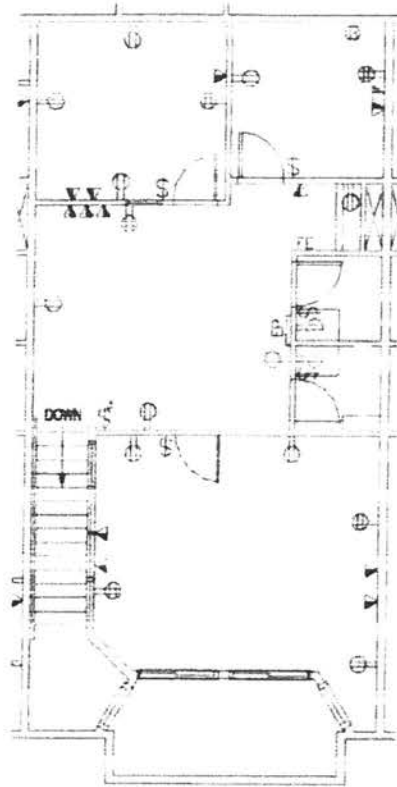
Radiant Yoga will not increase the intensity of the use at the 4000 SE International Way location.

- Economic Base and Industrial Land Use (Chapter 4)

Radiant Yoga is in step with the City's objective to actively attract new businesses, particularly those with having growth potential. It also supports the City's policy to review national, state and local trends for businesses that will locate in the City. To that end, Radiant Yoga helps to increase the tax base and provide for local employment.

Radiant Yoga is also supportive of the City's policy (under Employment Opportunity) for the recruitment of new businesses that can provide long-term employment opportunities. The City's policy of encouraging new service-oriented employment opportunities defines what Radiant Yoga will provide over the coming years. This will be accomplished by growing Radiant Yoga and providing increased employment opportunities as well as providing training for yoga teacher certification for those seeking meaningful work in the growing industry in Oregon.

EXHIBIT A  
FLOOR PLAN



**4000 SE International Way - Suite F202**

1,152 S.F. Office  
0 S.F. Warehouse  
1,152 S.F. Total  
Milwaukee Business Park - Milwaukee



*For illustration purposes only*

Initials *LE*  
*hb*

EXHIBIT B  
SITE PLAN



# Milwaukie Business Park

Milwaukie



*For illustration purposes only*

## ATTACHMENT 3

### MEMORANDUM

**TO:** Community Development Department

**THROUGH:** Kelly Brooks, Acting Engineering Director

**FROM:** Alex Roller, Engineering Technician II

**RE:** CU-2019-001 Staff Response  
4000 SE International Way

**DATE:** May 7, 2019

Change of use from a management company for janitorial services, to a yoga studio focused on training.

Site is in a business park. Traffic generation for Business Parks is an average of an assumed mix of office/commercial and industrial/warehousing. Uses can be high trip generation such as restaurants and recreation areas, or low uses such as warehousing. Proposed use corresponds to the uses for Business Park. There is not an increase in trips to the site as a whole, so MMC 19.700 is not applicable.

#### **Recommended conditions of approval:**

None.



# CITY OF MILWAUKIE

**To:** Planning Commission  
**Through:** Dennis Egner, Planning Director  
**From:** Brett Kelter, Associate Planner  
**Date:** May 10, 2019, for May 14, 2019, Public Hearing  
**Subject:** **File:** CSU-2019-002  
**Applicant/Owner:** North Clackamas School District  
**Address:** 2301 SE Willard St  
**Legal Description (Map & Tax Lot):** 1S1E36BC 05600  
**NDA:** Historic Milwaukie

---

## ACTION REQUESTED

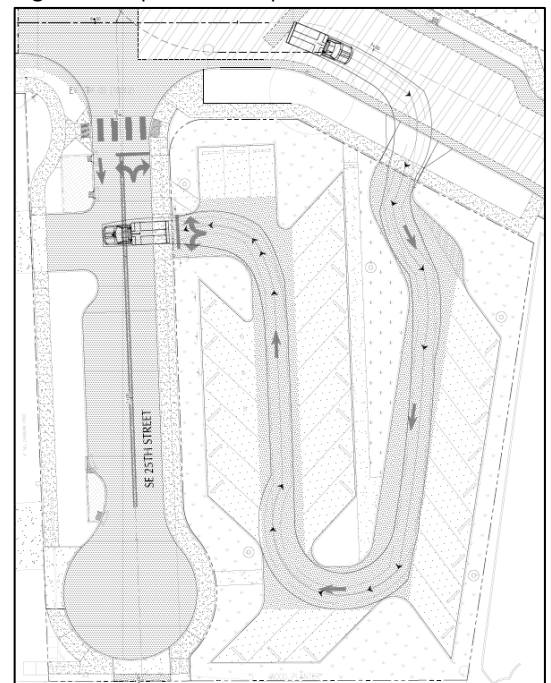
Approve land use application file #CSU-2019-002 and adopt the recommended Findings and Conditions of Approval found in Attachments 1 and 2. This action would allow for redevelopment of the existing tennis courts at the Milwaukie High School campus into an off-street parking lot.

## BACKGROUND INFORMATION

A large renovation of the Milwaukie High School campus was initiated in 2017, including demolition and replacement of the main classroom building and replacement of the varsity softball field with an off-street parking lot in the southeast portion of the campus north of Willard St. The original renovation proposal included replacement of the 3 existing tennis courts with another off-street parking lot, but that item was delayed and separated from the larger project while the applicant worked to address concerns raised by adjacent neighbors to the south.

The tennis court project is now ready to move forward. The applicant has proposed replacement of the tennis courts with a 30-space off-street parking lot, with traffic circulating in a one-way fashion, entering from Willard St and exiting onto 25<sup>th</sup> Ave (see Figure 1).

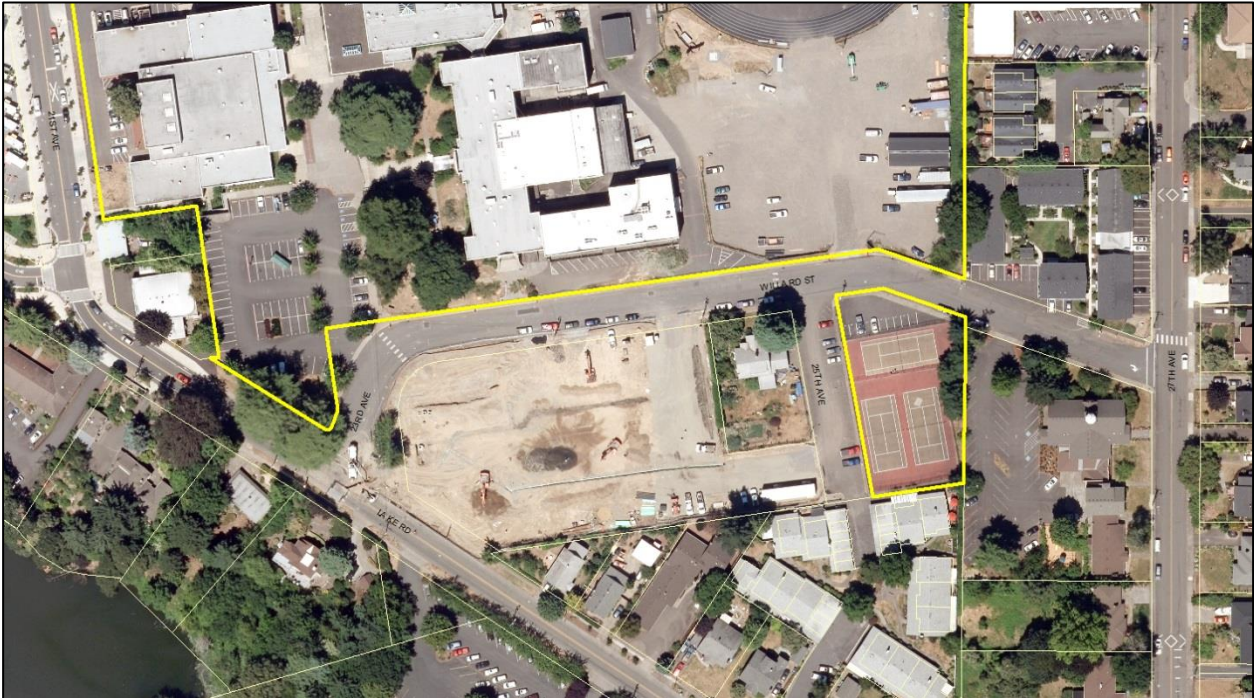
**Figure 1.** Proposed development



## A. Site and Vicinity

The Milwaukie High School campus is a nearly 15-acre site located downtown, roughly bounded by Washington St on the north, 21<sup>st</sup> Ave on the west, Willard St on the south, and 27<sup>th</sup> Ave on the east. The site is addressed as 2301 SE Willard St. The tennis courts are in the southeast corner of the campus, in the southeast corner of the intersection of Willard St and 25<sup>th</sup> Ave (see Figure 2).

Figure 2. Site and vicinity



The neighborhood immediately adjacent to the project area is primarily residential, though with a mix of uses. To the north is the bulk of the school campus; to the west is a single-family residential dwelling and the campus of Northwest Housing Alternatives (NHA), which is newly redeveloped with multifamily apartments and the NHA offices. To the south are single-family houses and attached condominiums; to the east is St. Stephens Serbian Orthodox Church; to the northeast are multifamily apartments.

**B. Zoning Designation**

Residential R-2

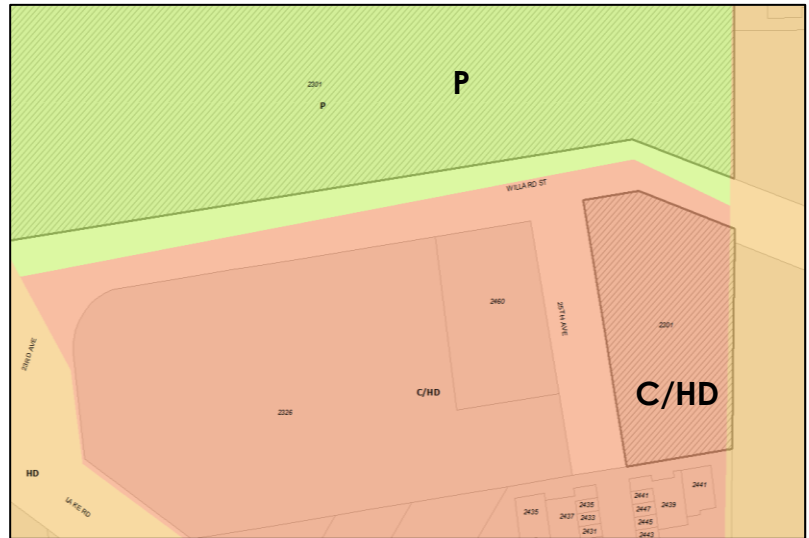
**C. Comprehensive Plan Designation**

Mixed Use (C/HD) [Note: Only the tennis court portion of the campus has this designation; most of the rest of the campus is Public (P).]

**D. Land Use History**

- **Original development:** The main classroom building of Milwaukie High School was first constructed in 1925. A grandstand and athletic fields were added in 1938. The gymnasium was rebuilt after a fire destroyed the original in 1963. The fine arts building was built in 1970-71 when the high school itself was remodeled to meet current fire and safety codes.
- **1986:** Approval of additional off-street parking (file #CSO-86-04)
- **1988:** Approval of temporary modular classrooms (file #CSO-88-02)
- **1993:** Approval to construct a new commons area (file #CSO-93-02)
- **1999:** Approval to remodel the entire school (file #CSO-99-05)
- **2007:** Approval to significantly upgrade the fine arts building, gymnasium, and athletic fields (file #CSU-07-05)
- **2009:** Minor modification approval to allow the replacement and expansion of an equipment shed near the athletic fields (file #CSU-09-07)
- **2011:** Minor modification approval to replace the scoreboard at the softball field (file #CSU-11-10)
- **December 2017:** Minor modification approval to allow temporary storage of 33 modular classrooms on the athletic field, in advance of the campus renovation project (file #CSU-2017-008)
- **February 2018:** Historic Review to delete the Milwaukie High School from the City's inventory of historic properties, approved by City Council with Ord. 2159 (file #HR-2017-002)
- **March 2018:** Approval of campus renovation, including demolition and reconstruction of main classroom building and replacement of varsity softball field with a new off-street parking lot (in southeast corner of campus)

Figure 3. Comp Plan designation



## E. Proposal

The applicant is seeking land use approval to replace the existing tennis courts in the southeast corner of the Milwaukie High School campus with a 30-space off-street parking lot. The project area is at the southeast corner of the intersection of Willard St and 25<sup>th</sup> Ave and includes the following elements:

- 30 off-street parking spaces (27 spaces configured at a 45° angle and 3 spaces at 90°)
- One-way circulation through the parking lot, entering from Willard St and exiting onto 25<sup>th</sup> Ave
- On-site pedestrian walkways, including a north-south walkway through the site connecting to the public sidewalk on Willard St and a shorter walkway connecting to the public sidewalk on 25<sup>th</sup> Ave
- Perimeter and interior landscaping areas planted with trees, shrubs, and ground cover, with screening to prevent vehicle headlights from shining onto adjacent residential properties
- Parking lot lighting, with limited light spill onto adjacent properties

Note: Late in the review process, staff realized that it had provided inaccurate information to the applicant regarding the required driveway spacing from the intersection. Staff had incorrectly used the figure of 45 ft, which is the standard for single-family residential projects on local streets; in fact, 100-ft spacing is required for all other uses accessing local streets. The applicant designed the parking lot to meet the 45-ft spacing standard—the Willard St driveway can be shifted the few feet necessary to meet the 100-ft standard, but staff agrees that the location of the 25<sup>th</sup> Ave driveway is acceptable as proposed. A variance to the 100-ft spacing standard is technically required and has been addressed by staff as part of the recommended findings.

For more detailed information on the proposal, see the Applicant's Materials in Attachment 3.

The project requires approval of the following applications:

1. Community Service Use (file #CSU-2019-002)

The proposed replacement of the existing tennis courts with a parking lot constitute a major modification to the high school's existing CSU approval.

## KEY ISSUES

### Summary

Staff has identified the following key issues for the Planning Commission's deliberation. Aspects of the proposal not listed below are addressed in the Findings (see Attachment 1) and generally require less analysis and discretion by the Commission.

- A. Does the proposed development present any special impacts that need mitigation?



## Analysis

### A. Does the proposed development present any special impacts that need mitigation?

The proposed new parking lot would replace 3 existing tennis courts and 10 off-street parking spaces with a new 30-space parking lot. The nature of use would change somewhat, with the occasional tennis court use being replaced by a parking lot that would likely see daily use during the school year. However, the parking use is a relatively passive one for long periods during the average day and presents minor impacts when compared with the recreational use of the tennis courts.

As proposed, the project includes a number of mitigating elements. The required improvements along the Willard St and 25<sup>th</sup> Ave frontages would result in improved sidewalks, safer pedestrian crossings, and improved access spacing from intersections. Within the 25<sup>th</sup> Ave right-of-way, the replacement of 90°-parking spaces with parallel spaces would make more efficient use of the street surface and would make 25<sup>th</sup> Ave seem more like a roadway and less like a parking lot itself. The code limits the amount of light spill onto other properties. The required tree planting and additional landscaping around the perimeter of the parking lot, including a continuous vegetated screen to prevent vehicle headlights from shining onto adjacent residential properties to the south and east, would provide a buffer between those properties and the main school use on the site.

Staff's assessment is that the proposed development would result in only minimal impacts and that no additional mitigation measures are needed.

## CONCLUSIONS

### A. Staff recommendation to the Planning Commission is as follows:

1. Approve the Community Service Use application (CSU-2019-002) for the proposed development. This would result in replacement of the existing tennis courts in the southeast corner of the Milwaukie High School campus with a 30-space off-street parking lot.
2. Adopt the attached Findings and Conditions of Approval.

### B. Staff recommends the following key conditions of approval (see Attachment 2 for the full list of Conditions of Approval):

- Shift the Willard St driveway to the east as needed to meet the minimum spacing standard of 100 ft from the intersection with 25<sup>th</sup> Ave.
- Revise a few details of the site plan to ensure that all dimensional requirements are met (e.g., drive-aisle width, perimeter landscaping width, tree spacing).
- Demonstrate that vehicle lights will be adequately screened and that light trespass is limited on adjacent residential properties.

## **CODE AUTHORITY AND DECISION-MAKING PROCESS**

The proposal is subject to the following provisions of the Milwaukie Municipal Code (MMC):

- MMC Title 12 Streets, Sidewalks, and Public Places
- MMC Section 19.302 Medium and High Density Residential Zones (including R-2)
- MMC Section 19.504 Site Design Standards
- MMC Chapter 19.600 Off-Street Parking and Loading
- MMC Chapter 19.700 Public Facility Improvements
- MMC Section 19.904 Community Service Uses
- MMC Section 19.911 Variances
- MMC Section 19.1006 Type III Review

This application is subject to Type III review, which requires the Planning Commission to consider whether the applicant has demonstrated compliance with the code sections shown above. In Type III reviews, the Commission assesses the application against review criteria and development standards and evaluates testimony and evidence received at the public hearing.

The Commission has 4 decision-making options as follows:

- A. Approve the application subject to the recommended Findings and Conditions of Approval.
- B. Approve the application with modified Findings and Conditions of Approval. Such modifications need to be read into the record.
- C. Deny the application upon finding that it does not meet approval criteria.
- D. Continue the hearing.

The final decision on these applications, which includes any appeals to the City Council, must be made by July 27, 2019, in accordance with the Oregon Revised Statutes and the Milwaukie Zoning Ordinance. The applicant can waive the time period in which the application must be decided.

## **COMMENTS**

Notice of the proposed changes was given to the following agencies and persons: City of Milwaukie Building, Engineering, Public Works, and Police Departments; City Attorney; Historic Milwaukie and Lake Road Neighborhood District Associations (NDAs); Clackamas Fire District #1 (CFD#1); Clackamas County Department of Transportation and Development (DTD); Metro; TriMet; NW Natural; and properties within 300 ft of the subject site.

The following is a summary of the comments received by the City. See Attachment 4 for further details.

- **Jodi Wright, East Metro Resource Engineer, NW Natural:** NW Natural has a 1-in steel main 13 ft south of the north property line on Willard St, as well as a service feeding the primary building site. Both need to be protected during construction activities.
- **Alex Roller, Engineering Tech II, City of Milwaukie Engineering Department:** Comments related to the proposal’s compliance with MMC Title 12 Streets, Sidewalks, and Public Places; and MMC Chapter 19.700 Public Facility Improvements.

*Staff Response: The Engineering Department’s comments have been integrated into the Recommended Findings and Conditions as appropriate.*

## ATTACHMENTS

Attachments are provided as indicated by the checked boxes. All material is available for viewing upon request.

|  | Early PC Mailing         | PC Packet                           | Public Copies                       | Packet                              |
|--|--------------------------|-------------------------------------|-------------------------------------|-------------------------------------|
| 1. Recommended Findings in Support of Approval   | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> |
| 2. Recommended Conditions of Approval  | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> |
| 3. Applicant's Narrative and Supporting Documentation (stamped received on March 13, 2019, unless otherwise noted) |                          |                                     |                                     |                                     |
| a. Narrative   | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> |
| b. Appendix A—Application Forms  | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> |
| c. Appendix B—Ownership Information  | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> |
| d. Appendix C—Notification Materials   | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> |
| e. Appendix D—Technical Reports  | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> |
| • Arborist Tree Evaluation   |                          |                                     |                                     |                                     |
| f. Appendix E—Land Use Plans   | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> |
| • Sheet C100—Existing Conditions   |                          |                                     |                                     |                                     |
| • Sheet C154—Demolition Plan   |                          |                                     |                                     |                                     |
| • Sheet C204—Site & Horizontal Control Plans (May 6, 2019)   |                          |                                     |                                     |                                     |
| • Sheet C215—Circulation Plan (May 6, 2019)  |                          |                                     |                                     |                                     |
| • Sheet C304—Grading Plan (May 6, 2019)  |                          |                                     |                                     |                                     |
| • Sheet C402—Storm Drainage Plan (May 6, 2019)   |                          |                                     |                                     |                                     |
| • Sheet C605—Onsite Details  |                          |                                     |                                     |                                     |
| • Sheet C1005—Erosion & Sediment Control Plan (May 6, 2019)  |                          |                                     |                                     |                                     |
| • Sheet L1.0—Planting Plan (March 20, 2019)  |                          |                                     |                                     |                                     |
| • Sheet EPH1—Site Photometrics--Lighting (March 20, 2019)  |                          |                                     |                                     |                                     |
| 4. Comments Received   | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> |

Key:

Early PC Mailing = paper materials provided to Planning Commission at the time of public notice 20 days prior to the hearing.

PC Packet = paper materials provided to Planning Commission 7 days prior to the hearing.

Public Copies = paper copies of the packet available for review at City facilities and at the Planning Commission meeting.

Packet = packet materials available online at <https://www.milwaukieoregon.gov/planning/csu-2019-002>.

**Recommended Findings in Support of Approval  
File #CSU-2019-002  
Milwaukie High School Parking Lot**

Sections of the Milwaukie Municipal Code (MMC) not addressed in these findings are found to be inapplicable to the decision on this application.

1. The applicant, the North Clackamas School District, has applied for approval to redevelop the existing tennis courts at the Milwaukie High School campus into an off-street parking lot. The project area is in the southeast corner of the intersection of Willard St and 25<sup>th</sup> Ave in the southeast corner of the overall campus. The larger subject property (the overall campus) is comprised of a single tax lot approximately 14.7 acres in size, addressed as 2301 SE Willard St.

Most of the campus is zoned Residential R-2, including the project area for the proposed development. (A small area in the southwest corner of the campus is zoned Downtown Mixed Use and the parking lot in the northwest corner of the site is zoned Residential-Business Office (R-1-B).) The school is currently approved as a Community Service Use (CSU), and the proposed development represents a major modification to the CSU. The land use application file number is CSU-2019-002.

2. The applicant has proposed replacement of the existing tennis courts with a 30-stall parking lot to provide more off-street parking for the campus. The project area includes 3 tennis courts and 10 off-street parking spaces, all of which would be removed and replaced with a new off-street parking lot. As proposed, traffic would circulate through the parking area in a one-way fashion, entering from Willard St and exiting onto 25<sup>th</sup> Ave. Striping, wheelstops, landscaping, lighting, pedestrian walkways, and similar features would be provided in accordance with the design standards of MMC Subsection 19.606.
3. The proposal is subject to the following provisions of the Milwaukie Municipal Code (MMC):
  - MMC Title 12 Streets, Sidewalks, and Public Places
  - MMC Section 19.302 Medium and High Density Residential Zones (including R-2)
  - MMC Section 19.504 Site Design Standards
  - MMC Chapter 19.600 Off-Street Parking and Loading
  - MMC Chapter 19.700 Public Facility Improvements
  - MMC Section 19.904 Community Service Uses
  - MMC Section 19.911 Variances
  - MMC Section 19.1006 Type III Review

The applicant's submittal is an application for major modification of a Community Service Use. The application has been processed and public notice provided in accordance with MMC Section 19.1006 Type III Review. A requirement for a preapplication conference was waived by the Planning Director, as allowed by MMC Subsection 19.1002.2.

Public notice was sent to property owners and current residents within 300 ft of the subject property on April 29, 2019. A public hearing with the Planning Commission was held on May 14, 2019, as required by law.

Note: Late in the review process, staff realized that it had provided inaccurate information to the applicant regarding the required driveway spacing from the intersection. Staff had incorrectly used the figure of 45 ft, which is the standard for single-family residential projects on local streets; in fact, 100-ft spacing is required for all other uses accessing local streets. The applicant designed the parking lot with the 45-ft standard in mind, and staff agrees that the design is acceptable in the context of the project area. However, a variance to the 100-ft spacing standard is technically required and has been addressed by staff as part of these recommended findings.

4. MMC Title 12 Streets, Sidewalks, and Public Places

a. MMC Chapter 12.16 Access Management

MMC Section 12.16.040 establishes standards for access (driveway) requirements, including access spacing, number and location of accessways, and limitations for access onto collector and arterial streets. For uses other than single-family residential development accessing local and neighborhood streets, one accessway is allowed per frontage and new driveways must be spaced at least 100 ft from the nearest intersection.

*The project area is located in the southeast corner of the intersection of Willard St and 25<sup>th</sup> Ave, two local streets. As proposed, a new one-way driveway would be established on each frontage. Staff incorrectly directed the applicant to use the 45-ft spacing standard allowed for single-family residential projects on local streets, when a 100-ft spacing should have been applied. The applicant designed the in-only driveway on Willard St to be just under 100 ft from the intersection and the out-only driveway on 25<sup>th</sup> Ave to be just over 50 ft from the intersection. Staff realized this error late in the review process and has recommended a variance to the standard for the 25<sup>th</sup> Ave driveway, as addressed in Finding 10. The Willard St driveway can be shifted the few feet necessary to meet the standard, and a condition has been established accordingly. Compliance with the conditions of approval established for CSU-2017-007 will ensure that the new driveways will meet all other applicable standards for access management.*

*As conditioned, and as per the variance for driveway spacing addressed in Finding 10, the development is consistent with the applicable standards of MMC 12.16.*

b. MMC Chapter 12.24 Clear Vision at Intersections

MMC 12.24 establishes standards for maintenance of clear vision at intersections to protect the safety and welfare of the public in their use of City streets.

*Compliance with the conditions of approval established for CSU-2017-007, including right-of-way dedication and frontage improvements, will ensure that all driveways, accessways, and intersections associated with the proposed development conform to the applicable standards of MMC 12.24.*

*The Planning Commission finds that, as conditioned and with approval of the variance for driveway spacing addressed in Finding 10, the development meets all applicable requirements of MMC Title 12. This standard is met.*

5. MMC Section 19.302 Medium and High Density Residential Zones (including R-2)

The project area on the subject property is zoned Residential R-2. MMC 19.302 establishes the allowable uses and development standards for the residential R-2 zone.

a. Permitted Uses

As per MMC Table 19.302.2, community service uses (CSUs) are allowed subject to the provisions of MMC Section 19.904.

*Milwaukie High School is an approved CSU on the subject property. The proposed development, which would significantly change a portion of the school campus, represents a major modification to the CSU and is subject to the provisions of MMC 19.904.*

b. Development Standards

MMC Table 19.302.2 establishes development standards for the R-2 zone. However, given that the project area represents such a small part of the overall campus area and that the proposed development is a surface parking lot and no new structures, few of the R-2 development standards are applicable.

*The R-2 minimum vegetation requirement of 15% is the same as the landscaping requirement for school CSUs as established in MMC Subsection 19.904.7.J. As addressed in Finding 9-b, the proposed development will result in an increase in landscaping on the overall campus, which is currently over 18% of the total lot area. No other R-2 development standards are applicable.*

*The Planning Commission finds that the applicable development standards of the R-2 zone are met.*

*As proposed, the Planning Commission finds that the proposed development meets the applicable standards of the underlying R-2 zone. This standard is met.*

6. MMC Section 19.504 Site Design Standards

MMC 19.504 provides standards for various aspects of site design. The applicable portions of this section are addressed below.

a. MMC Subsection 19.504.1 Clear Vision Areas

MMC 19.504.1 refers to clear vision area requirements in MMC Chapter 12.24.

*As addressed in Finding 4-b, all driveways, accessways, and intersections associated with the proposed development conform to the applicable standards of MMC 12.24.*

*This standard is met.*

b. MMC Subsection 19.504.7 Minimum Vegetation

MMC 19.504.7 requires that no more than 20% of the required vegetation area may be covered with bark mulch.

*As proposed, the new landscaping areas would be planted with a mix of trees, shrubs, and ground covering grasses. Very little of the newly vegetated areas would be covered with bark mulch.*

*This standard is met.*

c. MMC Subsection 19.504.9 On-Site Walkways and Circulation

MMC 19.504.9 establishes standards for on-site walkways, including requirements that on-site walkways be at least 5 ft wide, provided for every 300 ft of frontage, constructed of hard surface materials that are permeable for stormwater, and lighted to a minimum level of 0.5 footcandles.

*The proposed development includes a pervious, 5-ft-wide walkway through the parking area, with lighting sufficient to meet the minimum 0.5-footcandle standard. As proposed, one on-site walkway would run north-south through the parking area to connect with the public sidewalk on the project area's Willard St frontage. As shown on revised plan sheets provided by the applicant prior to the public hearing, another on-site walkway would connect to the public sidewalk on the project area's 25<sup>th</sup> Ave frontage.*

*This standard is met.*

*The Planning Commission finds that the applicable site design standards of MMC 19.504 are met.*

7. MMC Chapter 19.600 Off-Street Parking and Loading

MMC 19.600 regulates off-street parking and loading areas on private property outside the public right-of-way (ROW). The purpose of these requirements includes providing adequate space for off-street parking, minimizing parking impacts to adjacent properties, and minimizing environmental impacts of parking areas.

a. MMC Section 19.602 Applicability

MMC 19.602 establishes the applicability of the provisions of MMC 19.600. In particular, MMC Subsection 19.602.4.A establishes applicability for parking projects developed to serve an existing use but not associated with other development activity or change in use. Such activity shall conform to the requirements of MMC Sections 19.604 and 19.606-19.611. In addition, the total number of new spaces in the existing and new parking areas shall not exceed the maximum allowed quantity of parking as established in MMC Section 19.605.

*The proposed development is a new off-street parking area in the southeast corner of the site that would serve the existing school use.*

*The Planning Commission finds that the provisions of MMC 19.600 are applicable to the proposed development.*

b. MMC Section 19.604 General Parking Standards

MMC Subsections 19.604.1 to 19.604.4 establish general standards for off-street parking areas, including requirements related to the provision of parking in conjunction with development activity, the location of accessory parking, and use and availability of parking areas, and the prohibition of using parking areas for storage.

*The applicant proposes to increase the total number of parking spaces by constructing a new off-street parking lot on the school campus. As proposed, the existing number of off-site parking spaces available to the school will be retained and all spaces will continue to be available to support the existing school use.*

*The Planning Commission finds that the applicable standards of this section are met.*

c. MMC Section 19.605 Vehicle Parking Quantity Requirements

MMC 19.605 establishes standards to ensure that development provides adequate vehicle parking (off-street) based on estimated parking demand. The section establishes processes for determination of parking requirements, exemptions and reductions to the required ratios, and provisions for shared parking.

For high schools, MMC Table 19.605.1 establishes minimum and maximum quantity requirements of 1 space per 0.25 and 0.33 students, respectively. In addition, one space per staff person is required.

*Land use approval for a renovation of the overall school campus (including replacement of the main classroom building, reconfiguration of an existing off-street parking lot, and construction of a new off-street parking lot) was granted through master file #CSU-2017-007 and addressed off-street parking. With a 1,500-student capacity and a staff of 140, MMC Table 19.605.1 requires a minimum of 515 spaces and allows a maximum of 635 spaces. The proximity of the campus to the Orange line light-rail station in downtown Milwaukee qualifies for a 25% reduction in the minimum parking requirement, reducing it from 515 to 386. No changes to the school capacity or staffing are included with the current proposal.*

*The school has shared parking agreements with two nearby churches (St. John the Baptist Catholic Church on Washington St and St. Stephens Serbian Orthodox Church on 27<sup>th</sup> Ave) that provide a total of 90 off-street spaces. With the improvements approved by CSU-2017-007, the school site provides a total of 333 off-street parking spaces (243 spaces in on-site parking lots and 90 shared spaces). The proposed development would add a net total of 20 off-street spaces, bringing the total number of parking spaces available to the school up to 353.*

*The Planning Commission finds that the proposed development would bring the school site closer to conformance with the adjusted minimum standard of 386 spaces. This standard is met.*



d. MMC Section 19.606 Parking Area Design and Landscaping

MMC 19.606 establishes standards for parking area design and landscaping, to ensure that off-street parking areas are safe, environmentally sound, and aesthetically pleasing, and that they have efficient circulation.

(1) MMC Subsection 19.606.1 Parking Space and Aisle Dimension

MMC 19.606.1 establishes dimensional standards for required off-street parking spaces and drive aisles. For 45°-angle spaces, the minimum width is 9 ft and minimum depth is 18.5 ft, with a 12-ft minimum curb length and 13-ft one-way drive aisles. For 90°-angle spaces, the minimum width is 9 ft and minimum depth is 18 ft, with a 9-ft minimum curb length and 22-ft drive aisles.

*The proposed development includes 27 spaces set at a 45° angle, with the remaining 3 spaces set at a 90° angle near the exit onto 25<sup>th</sup> Ave. For both configurations, the proposed stall dimensions meet the minimum standards, and the 15-ft-wide drive aisle that runs through the parking area is adequate to serve the 45° spaces. However, the proposed 20-ft-wide drive aisle adjacent to the 90° spaces does not meet the minimum standard of 22 ft; a condition has been established to require widening the drive aisle accordingly where adjacent to the 90° spaces.*

*As conditioned, the applicable standards are met.*

(2) MMC Subsection 19.606.2 Landscaping

MMC 19.606.2 establishes standards for parking lot landscaping, including for perimeter and interior areas. The purpose of these landscaping standards is to provide buffering between parking areas and adjacent properties, break up large expanses of paved area, help delineate between parking spaces and drive aisles, and provide environmental benefits such as stormwater management, carbon dioxide absorption, and a reduction of the urban heat island effect.

(a) MMC Subsection 19.606.2.C Perimeter Landscaping

In all but the downtown zones, perimeter landscaping areas must be at least 6 ft wide where abutting other properties and at least 8 ft wide where abutting the public ROW. At least 1 tree must be planted for every 30 lineal ft of landscaped buffer area, with the remainder of the buffer planted with grass, shrubs, ground cover, mulch, or other landscaped treatment. Required trees must be species that will provide a minimum 20-ft diameter shade canopy within 10 years of planting, based on the expected growth of the selected trees. Parking areas adjacent to residential uses must provide a continuous visual screen from 1 to 4 ft above the ground to adequately screen vehicle lights.

*The project area is adjacent to the public ROW on its northern (Willard St) and western (25<sup>th</sup> Ave) perimeters and is adjacent to other properties on its southern and eastern perimeters. As proposed, the new parking area would provide*

*perimeter planting areas at least 8 ft wide around the entire lot, except at the very southwest corner. A condition has been established to require the applicant to revise the site plan to ensure an 8-ft landscaping buffer at the southwest corner of the site. As proposed, trees would be spaced at approximately 30-ft intervals (with the exception of the northwest corner of the project area), with shrubs and ground cover planted within the remainder of the landscaping area. A condition has been established to ensure that enough trees are provided to meet the required spacing and to confirm that the species chosen will provide at least a 20-ft diameter shade canopy within 10 years.*

*Where the parking lot is adjacent to residential areas on the south, the applicant has proposed screening vegetation to shield vehicle lights, though it is not clear that the size and species proposed will provide the required visual screening from 1 to 4 ft above the ground when initially planted. A condition has been established to ensure that plantings or fencing will be provided to sufficiently screen adjacent residential properties from vehicle lights prior to a final inspection that would allow the parking lot to be put into official use.*

*As conditioned, the applicable standards of this subsection are met.*

(b) MMC Subsection 19.606.2.D Interior Landscaping

At least 25 sq ft of interior landscaped area are required for each parking space. Planting areas must be at least 120 sq ft in area, at least 6 ft in width, and dispersed throughout the parking area. For landscape islands, at least 1 tree shall be planted per island, with the remainder of the buffer planted with grass, shrubs, ground cover, mulch, or other landscaped treatment.

*The proposed development would establish 30 new parking spaces, for which a minimum of 750 sq ft of interior landscaping is required. As proposed, the site plan provides at least 3,000 sq ft of interior landscaping, well over the minimum required. All interior landscaped areas are at least 120 sq ft in size and are disbursed throughout the various parking areas on the site.*

*This standard is met.*

(c) MMC Subsection 19.606.2.E Other Parking and Landscaping Provisions

Preservation of existing trees in off-street parking areas is encouraged and may be credited toward the total number of trees required. Parking area landscaping must be installed prior to final inspection, unless a performance bond is posted with the City. Required landscaping areas may serve as stormwater management facilities, and pedestrian walkways are allowed within landscape buffers if the buffer is at least 2 ft wider than required by MMC 19.606.2.C and 19.606.2.D.

*The plans submitted indicate that 8 of the 9 existing trees on the site would be removed for the proposed development, due either to direct conflicts with improvements or to their proximity to the work area and the likelihood of*

*irreparable root damage. Proper and complete installation of landscaping will be confirmed as part of the subsequent Development Review and final inspection. As proposed, no pedestrian walkway would interfere with the minimum dimensions of a landscaping area.*

*This standard is met.*

*As conditioned, the Planning Commission finds that the applicable standards of MMC 19.606.2 are met.*

(3) MMC Subsection 19.606.3 Additional Design Standards

MMC 19.606.3 establishes various design standards, including requirements related to paving and striping, wheel stops, pedestrian access, internal circulation, and lighting.

(a) MMC Subsection 19.606.3.A Paving and Striping

Paving and striping are required for all required maneuvering and standing areas, with a durable and dust-free hard surface and striping to delineate spaces and directional markings for driveways and accessways.

*The plans submitted indicate that all parking areas would be paved and striped.*

*This standard is met.*

(b) MMC Subsection 19.606.3.B Wheel Stops

Parking bumpers or wheel stops are required to prevent vehicles from encroaching onto public rights-of-way, adjacent landscaped areas, or pedestrian walkways. Curbing may substitute for wheel stops if vehicles will not encroach into the minimum required width for landscape or pedestrian areas.

*The plans submitted show wheel stops in every new parking stall.*

*This standard is met.*

(c) MMC Subsection 19.606.3.C Site Access and Drive Aisles

Accessways to parking areas shall be the minimum number necessary to provide access without inhibiting safe circulation on the street. Drive aisles shall meet the dimensional requirements of MMC 19.606.1 (addressed in Finding 7-d-1). Along collector and arterial streets, no parking space shall be located such that its maneuvering area is in an ingress or egress aisle within 20 ft of the back of the sidewalk. Driveways and on-site circulation shall be designed so that vehicles enter the ROW in a forward motion.

*As proposed, traffic would circulate through the parking area in one-way fashion, entering from Willard St and exiting onto 25<sup>th</sup> Ave, both of which are classified as local streets. As addressed in Finding 7-d-1, a condition has been established to ensure that all drive aisles meet the applicable width standard. The one-way*

*circulation pattern would ensure that all vehicles using the parking area will enter the ROW in a forward motion.*

*As conditioned, this standard is met.*

(d) MMC Subsection 19.606.3.D Pedestrian Access and Circulation

Pedestrian access shall be provided so that no off-street parking space is farther than 100 ft away, measured along vehicle drive aisles, from a building entrance or a walkway that is continuous, leads to a building entrance, and meets the design standards of MMC Subsection 19.504.9.E.

*As proposed, no off-street parking space is farther than 100 ft away from a building entrance or walkway that meets the standards of this subsection. As addressed in Finding 6-c, on-site walkways are proposed that meet the applicable design standards of MMC 19.504.9.E.*

*This standard is met.*

(e) MMC Subsection 19.606.3.E Internal Circulation

The Planning Director has the authority to review the pedestrian, bicycle, and vehicular circulation of the site and impose conditions to ensure safe and efficient on-site circulation. Such conditions may include, but are not limited to, on-site signage, pavement markings, addition or modification of curbs, and modification of drive aisle dimensions.

*The Planning Director has reviewed the proposed circulation plan and concluded that it provides safe and efficient on-site circulation.*

*This standard is met.*

(f) MMC Subsection 19.606.3.F Lighting

Lighting is required for parking areas with more than 10 spaces and must have a cutoff angle of 90° or greater to ensure that lighting is directed toward the parking surface. Lighting shall not cause a light trespass of more than 0.5 footcandles measured vertically at the boundaries of the site and shall provide a minimum illumination of 0.5 footcandles for pedestrian walkways in off-street parking areas.

*The applicant's submittal includes a photometric plan that confirms the parking area an on-site walkway would be illuminated to at least 0.5 footcandles. Along the eastern perimeter, there are 3 points where the illumination level on the adjacent church property would exceed 0.5 footcandles, so a condition has been established to ensure that adequate screening is provided to adjust the light trespass down to an acceptable level.*

*As conditioned, this standard is met.*

*As conditioned, the Planning Commission finds that the applicable standards of MMC 19.606.3 are met.*

*As conditioned, the Planning Commission finds that the applicable design and landscaping standards of this section are met.*

e. MMC Section 19.608 Loading

MMC 19.608 establishes standards for off-street loading areas and empowers the Planning Director to determine whether loading spaces are required. For residential development with fewer than 50 dwelling units on a site that abuts a local street, no loading space is required; otherwise, 1 space is required. Loading spaces shall be at least 35 ft long and 10 ft wide, with a height clearance of 13 ft, and located where not a hindrance to drive aisles or walkways.

*As addressed previously in the findings for CSU-2017-007, the site currently has at least 1 designated loading space on the west side of the campus. The Planning Director has determined that no additional loading spaces are required.*

*The Planning Commission finds that the applicable standards of this section are met.*

f. MMC Section 19.609 Bicycle Parking

MMC 19.609 establishes standards for bicycle parking for new development of various uses. Unless otherwise specified, the number of bicycle spaces provided shall be at least 10% of the minimum required vehicle parking for the use. When at least 10 bicycle spaces are required, a minimum of 50% of the spaces shall be covered and/or enclosed. MMC Subsection 19.609.3.A provides that each bicycle parking space shall have minimum dimensions of 2 ft by 6 ft, with 5-ft-wide aisles for maneuvering. MMC Subsection 19.609.4 requires bike racks to be located within 50 ft of a main building entrance.

*As discussed in Finding 7-c, the school has an adjusted minimum parking requirement of 386 vehicular spaces; therefore, the minimum bike parking requirement for the school is 38 spaces. A total of 48 existing covered bike parking spaces remain available on the site, so no additional bike spaces are required.*

*The Planning Commission finds that the applicable standards of this section are met.*

g. MMC Section 19.610 Carpool and Vanpool Parking

MMC 19.610 establishes carpool parking standards for new industrial, institutional, and commercial development. The number of carpool/vanpool parking spaces shall be at least 10% of the minimum amount of required parking spaces. Carpool/vanpool spaces shall be located closer to the main entrances of the building than other employee or student parking, except ADA spaces and shall be clearly designated with signs or pavement markings for use only by carpools/vanpools.

*As part of the review of the larger campus renovation approved by CSU-2017-007, the applicant was granted a variance from the requirement to provide carpool parking spaces. No carpool spaces are required for the proposed development.*

*The Planning Commission finds that the applicable standards of this section are met.*

*As conditioned, the Planning Commission finds that the proposed development meets all applicable standards of MMC 19.600.*

8. MMC Chapter 19.700 Public Facility Improvements

MMC 19.700 is intended to ensure that development, including redevelopment, provides public facilities that are safe, convenient, and adequate in rough proportion to their public facility impacts.

MMC Section 19.702 establishes the applicability of the provisions of MMC 19.700, including new construction. MMC Subsection 19.708.1 provides general standards for streets, including for access management, clear vision, street layout and connectivity, and intersection design and spacing.

*The applicant proposes to redevelop existing tennis courts into an off-street parking lot. By itself, the proposed development would trigger the requirements of MMC 19.700. However, the larger campus renovation reviewed and approved with CSU-2017-007 established that there would be an increase in traffic resulting from the larger high school (although the size of the student body would not increase). The conditions written for CSU-2017-007 that apply to the 25<sup>th</sup> Ave and Willard St frontages also apply to this land use application.*

*As addressed in Finding 4, and with approval of the variance discussed in Finding 10, the proposed development complies with all applicable standards for access management and clear vision as established in MMC 19.708.1.*

*The Planning Commission finds that the proposed development meets all applicable standards of MMC 19.700.*

9. MMC Section 19.904 Community Service Uses

MMC 19.904 provides standards and procedures for review of applications for community service uses (CSUs). These are uses that are not specifically allowed outright in most zoning districts but that address a public necessity or otherwise provide some public benefit. CSUs include public and private schools and their associated sports facilities. MMC Subsection 19.904.3 provides that the establishment of a new CSU or a major modification to a CSU shall be evaluated through a Type III review per Section 19.1006.

*The redevelopment of the existing tennis courts to construct an additional off-street parking lot on the school campus represents a major modification to the school's existing CSU and so is subject to Type III review.*

MMC Subsection 19.904.4 provides the following approval criteria for establishment of a new CSU or a major modification to a CSU:

- a. The building setback, height limitation, and off-street parking and similar requirements governing the size and location of development in the underlying zone are met. Where a specific standard is not proposed for a CSU, the standards of the underlying zone must be met.

*As discussed in Finding 5, the proposed development meets the applicable standards of the underlying R-2 zone, except where those standards are superseded by the specific applicable standards for schools as provided in MMC Subsection 19.904.7 and discussed below in Finding 9-b.*

*The Planning Commission finds that this standard is met.*

- b. Specific standards for the proposed uses as found in MMC Subsections 19.904.7-11 are met.

MMC Subsection 19.904.7 establishes specific standards for schools, which are addressed as follows:

- (1) MMC Subsection 19.904.7.A requires public elementary or secondary schools to provide a site area/pupil ratio as required by state law. However, the proposed development does not include any changes to the size of the existing school campus.

*This standard is not applicable to the proposed development.*

- (2) MMC Subsection 19.904.7.B has outdoor play area requirements for preschools, nursery schools, day-care centers, or kindergartens.

*This standard is not applicable to a high school.*

- (3) MMC Subsection 19.904.7.C requires walkways, both on and off the site for safe pedestrian access.

*As addressed in Finding 6-c, the proposed development would provide on-site walkways to connect the new parking spaces with the public sidewalks on Willard St and 25<sup>th</sup> Ave.*

*This standard is met.*

- (4) MMC Subsection 19.904.7.D requires sight-obscuring fencing of 4 to 6 ft in height to separate play areas from adjacent residential uses.

*The proposed development would replace existing tennis courts with a new off-street parking lot. This standard is not applicable.*

- (5) MMC Subsection 19.904.7.E requires adequate public facilities to serve the school.

*The approval of CSU-2017-007 for the larger renovation of the overall school campus included a requirement for street improvements on the various public streets adjacent to the school property, including Willard St and 25<sup>th</sup> Ave. Improvements to the existing stormwater system in Willard St and 25<sup>th</sup> Ave were also required, including new manholes, catch basins, and bioswales. No additional updates to public facilities are required for the proposed development.*

*This standard is met.*

- (6) MMC Subsection 19.904.7.F safe loading and ingress and egress on and to the site.

*As proposed, traffic would circulate through the parking area in one-way fashion, entering from Willard St and exiting onto 25<sup>th</sup> Ave, both of which are classified as local streets. The one-way circulation pattern would ensure that all vehicles using the parking area will enter the ROW safely in a forward motion.*

*This standard is met.*

- (7) MMC Subsection 19.904.7.G requires compliance with the parking standards in MMC 19.600.

*These standards are addressed in Finding 7, with conditions established to ensure compliance with applicable off-street parking requirements.*

*As conditioned, this standard is met.*

- (8) MMC Subsection 19.904.7.H requires minimum yard setbacks of 20 ft.

*The proposed development does not include construction of any new structures that would be subject to setback requirements. This standard is not applicable.*

- (9) MMC Subsection 19.904.7.I requires bicycle facilities which “adequately serve the site.”

*As discussed in Finding 7-f, the school site currently provides more than the minimum required number of bike parking spaces.*

*This standard is met.*

- (10) MMC Subsection 19.904.7.J requires a minimum landscaped area of 15%.

*As discussed in the approval of CSU-2017-007, the larger campus renovation resulted in a total of over 18% landscaping for the overall site. The proposed development would replace an existing tennis court area (non-vegetated) with a parking lot that includes new landscaping areas. The minimum vegetation is already provided and would be increased somewhat by the proposed development.*

*This standard is met.*

*As conditioned, the Planning Commission finds that the proposed development meets the applicable standards of MMC 19.904.7.*

- c. The hours and levels of operation of the proposed use are reasonably compatible with surrounding uses.

*The hours of operation of the proposed new parking lot would be similar to those of the school and other campus facilities, which have been found under previous land use reviews to be compatible with surrounding uses.*

*The Planning Commission finds that this standard is met.*



- d. The public benefits of the proposed use are greater than the negative impacts, if any, on the neighborhood.

*The proposed development would result in a net gain of 20 off-street parking spaces, bringing the school campus closer to conformance with the minimum number of spaces required based on the current capacity of students and staff. The project includes frontage improvements on Willard St and 25<sup>th</sup> Ave, which would result in improved sidewalks, safer pedestrian crossings, and improved access spacing from intersections. Within the 25<sup>th</sup> Ave ROW, the replacement of 90°-parking spaces with parallel spaces would make more efficient use of the street surface and would make 25<sup>th</sup> Ave seem more like a roadway and less like a parking lot itself.*

*The nature of use of the project area would change, with the occasional tennis court use being replaced by a parking lot that would likely see daily use during the school year. However, the parking use is a relatively passive one for long periods during the average day and presents minor impacts when compared with the recreational use of the tennis courts. The code limits the amount of light spill onto other properties. The required tree planting and additional landscaping around the perimeter of the parking lot, including a continuous vegetated screen to prevent vehicle headlights from shining onto adjacent residential properties to the south and east, would provide a buffer between those properties and the main school use on the site.*

*The Planning Commission finds that this standard is met.*

- e. The location is appropriate for the type of use proposed.

*The project area is an appropriate location for the proposed parking lot, given its proximity on the school campus to the main classroom building. The majority of surface area within the project area is an existing impervious surface that would be replaced with another impervious surface for parking. Although several existing trees would be removed, twice that number of trees would be planted within several thousand square feet of new landscaped areas. The project area's location at the corner of two local streets allows for a one-way circulation design that would reduce vehicle conflicts and provide safe ingress and egress.*

*The Planning Commission finds that this standard is met.*

*As conditioned, the Planning Commission finds that the proposed development meets the approval criteria of MMC 19.904.4 as a major modification to the school as a CSU.*

## 10. MMC Section 19.911 Variances

MMC Section 19.911 establishes the variance process for seeking relief from specific code sections that have the unintended effect of preventing reasonable development or imposing undue hardship.

- a. MMC Subsection 19.911.2 Applicability

MMC 19.911.2 establishes applicability standards for variance requests.

Variances may be requested to any standard of MMC Title 19, provided the request is not specifically listed as ineligible in MMC Subsection 19.911.2.B. Ineligible variances

include requests that result in any of the following: change of a review type, change or omission of a procedural step, change to a definition, increase in density, allowance of a building code violation, allowance of a use that is not allowed in the base zone, or the elimination of restrictions on uses or development that contain the word “prohibited.”

*Staff has recognized that it had inadvertently had provided inaccurate information to the applicant regarding the required driveway spacing from the intersection. Staff had incorrectly used the figure of 45 ft, which is the standard for single-family residential projects on local streets; in fact, 100-ft spacing is required for all other uses accessing local streets. The applicant designed the parking lot to meet the 45-ft standard, with the in-only driveway on Willard St spaced just under 100 ft from the intersection and the out-only driveway on 25<sup>th</sup> Ave spaced just over 50 ft from the intersection. A condition has been established to ensure that the Willard St driveway meets the 100-ft standard, but a variance is required to approve the 25<sup>th</sup> Ave driveway at the spacing proposed.*

*The requested variance meets the eligibility requirements established in MMC 19.911.2.*

b. MMC Subsection 19.911.4 Approval Criteria

MMC 19.911.4 establishes approval criteria for variance requests. Specifically, MMC Subsection 19.911.4.B.1 provides the following Discretionary Relief Criteria:

- (1) The applicant’s alternatives analysis provides, at a minimum, an analysis of the impacts and benefits of the variance proposal as compared to the baseline code requirements.

*One of the primary purposes of the minimum spacing requirement is to limit queuing impacts on intersections. Both driveways are designed to allow travel in only one direction, which by itself reduces potential conflicts on the street. The driveway on 25<sup>th</sup> Ave is designed as an exit-only access onto the street, so most queuing related to that accessway will occur within the parking lot rather than on the street. As proposed, the new driveway would be located directly opposite the driveway serving the residential property on the southwest corner of 25<sup>th</sup> Ave and Willard St (2460 SE Willard St), which would align turning movements and prevent vehicle conflicts.*

*In addition, 25<sup>th</sup> Ave dead-ends at the southern boundary of the project area. The block-long street does provide access to 11 garages for the adjacent condominiums to the south as well as to the 49-space parking lot for the newly redeveloped Northwest Housing Alternatives (NHA) property to the west. However, the NHA lot has another driveway on Willard St, so its trips are distributed between the two streets. Although the proposed school parking lot driveway on 25<sup>th</sup> Ave would be closer to the intersection with Willard St than the required 100 ft, the anticipated volume of traffic on the street is not high enough to cause queuing problems. And the school schedule is different enough from that of the adjacent residential uses, especially with the 25<sup>th</sup> Ave driveway being exit-only, that no significant queuing impacts are anticipated.*

*The Planning Commission finds that an adequate analysis of the impacts and benefits of the requested variance compared to the baseline requirements has been provided. This criterion is met.*

- (2) The proposed variance is determined to be both reasonable and appropriate, and it meets one or more of the following criteria:
  - (a) The proposed variance avoids or minimizes impacts to surrounding properties.
  - (b) The proposed variance has desirable public benefits.
  - (c) The proposed variance responds to the existing built or natural environment in a creative and sensitive manner.

*As noted above, the one-way nature of both driveways minimizes possible conflicts on the street. In the initial stages of project planning, the applicant met with neighbors in the adjacent condominiums to the south and found significant opposition to the idea of locating a parking lot driveway farther to the south on 25<sup>th</sup> Ave, due to concerns about potential conflicts with the condominium driveway. As a result, the applicant has located the driveway as far to the north as practical to minimize potential impacts.*

*Additionally, the proposed driveway would be located directly across 25<sup>th</sup> Ave from the existing driveway serving 2460 SE Willard St. That alignment would provide a more uniform arrangement of access points and clearer vision for vehicles entering the roadway on that part of 25<sup>th</sup> Ave, considering that vehicles will only be exiting the school parking lot and always in a forward motion.*

*The Planning Commission finds that the requested variance is reasonable and appropriate and that it meets one or more of the criteria provided in MMC Subsection 19.911.B.1.b.*

- (3) Impacts from the proposed variance will be mitigated to the extent practicable.

*The reduced distance between the proposed 25<sup>th</sup> Ave driveway and the intersection with Willard St would result in a shorter space for queuing without blocking the driveway. However, the one-way nature of circulation proposed through the parking area and the proposal to make the driveway an exit-only access would effectively make the parking lot drive aisle a off-street queuing space. And the location of the two other significant driveways (for the condominiums and the NHA parking lot) at the southern end of 25<sup>th</sup> Ave (at least 100 ft from the proposed school parking lot driveway) allow ample space for queuing in that section of 25<sup>th</sup> Ave if needed.*

*The Planning Commission finds that the requested variance will not result in any impacts that require mitigation.*

*As proposed, the Planning Commission finds that the requested variance meets the approval criteria established in MMC 19.911.4.B.1 for Type III variances seeking discretionary relief.*

*The Planning Commission finds that the requested variance is allowable as per the applicable standards of MMC 19.911.*

11. The application was referred to the following departments and agencies on March 28, 2019:

- Milwaukie Building Department
- Milwaukie Engineering Department
- Milwaukie Public Works Department
- Milwaukie Police Department
- City Attorney
- Historic Milwaukie Neighborhood District Association (NDA), Chairperson and Land Use Committee (LUC)
- Lake Road NDA, Chairperson and LUC
- Clackamas Fire District #1 (CFD#1)
- Clackamas County Department of Transportation and Development (DTD)
- Metro
- TriMet
- NW Natural

In addition, notice of the public hearing was mailed to owners and residents of properties within 300 ft of the subject property on April 24, 2019.

The comments received are summarized as follows:

- **Jodi Wright, East Metro Resource Engineer, NW Natural:** NW Natural has a 1-in steel main 13 ft south of the north property line on Willard St, as well as a service feeding the primary building site. Both need to be protected during construction activities.
- **Alex Roller, Engineering Tech II, City of Milwaukie Engineering Department:** Comments related to the proposal's compliance with MMC Title 12 Streets, Sidewalks, and Public Places; and MMC Chapter 19.700 Public Facility Improvements.

**Recommended Conditions of Approval  
File #CSU-2019-002  
Milwaukie High School Parking Lot**

**Conditions**

1. In conjunction with the required submittal for development review, the applicant shall make the following revisions to all relevant plan sheets:
  - a. As per Finding 4-a, shift the Willard St driveway to the east as needed to meet the minimum 100-ft spacing requirement from the intersection with 25<sup>th</sup> Ave.
  - b. As per Finding 7-d-1, widen the drive aisle adjacent to the 90° spaces as needed to provide a minimum 22-ft width.
  - c. As per Finding 7-d-2(a), ensure that the perimeter landscaping buffer at the southwest corner of the site is at least 8 ft wide.
  - d. As per Finding 7-d-2(a), revise the landscaping plan as needed to ensure that perimeter landscaping areas will be planted with trees spaced at 30-ft intervals and that the required trees will provide a minimum 20-ft diameter shade canopy within 10 years of planting, based on expected growth of the selected trees.
2. Prior to final inspection to allow official use of the new parking lot, the following shall be resolved:
  - a. As per Finding 7-d-2(a), demonstrate that adjacent residential properties will be adequately screened from vehicle lights as required by MMC Subsection 19.606.2.C.3, whether with the plantings proposed or by some other means.
  - b. As per Finding 7-d-3(f), provide shielding as needed to reduce light trespass onto adjacent properties to no more than 0.5 footcandles along the eastern perimeter of the project area.
  - c. Confirm that all applicable conditions of approval from CSU-2017-007 related to frontage improvements, bonding, stormwater management, driveway widths, and clear vision have been met.

**Additional Requirements**

The following items are not conditions of approval necessary to meet applicable land use review criteria. They relate to other development standards and permitting requirements contained in the Milwaukie Municipal Code (MMC) and Public Works Standards that are required at various points in the development and permitting process.

1. At the time of submittal of the associated development permit application(s), the following shall be resolved:
  - a. The applicant shall submit an application for Development Review in accordance with the standards established in MMC Section 19.906.

- b. Except as otherwise modified by these conditions of approval, final plans submitted for development permit review shall be in substantial conformance with the plans approved by this action, which are the plans stamped received by the City on March 13, 2019; with modified Sheets L1.0 (Planting Plan) and EPH1 (Site Photometrics—Lighting), stamped received on March 20, 2019; and modified Sheets C204 (Site & Horizontal Control Plans), C215 (Circulation Plan), C304 (Grading Plan), C402 (Storm Drainage Plan), and C1005 (Erosion & Sediment Control Plan), stamped received on May 6, 2019. Note that Sheets L1.0 and EPH1 should be revised to show the on-site walkway connection to 25<sup>th</sup> Ave as shown on the various plan sheets received by the City on May 6, 2019.
        - c. Provide a narrative describing all actions taken to comply with these conditions of approval. In addition, describe any changes made after the issuance of this land use decision that are not related to these conditions of approval.
2. In conjunction with the final inspection(s) necessary to allow official use of the new parking lot, the following shall be resolved:
  - a. Submit a letter from the project landscape designer attesting that all required site plantings have been completed in conformance with the approved site plans and with City standards.
  - b. Construct and receive Engineering Department inspection for all required public improvements.
3. Limitations on Development Activity  
Development activity on the site shall be limited to 7:00 a.m. to 10:00 p.m. Monday through Friday and 8:00 a.m. to 5:00 p.m. Saturday and Sunday, as per MMC Subsection 8.08.070(I).
4. Landscaping Maintenance  
As per MMC Subsection 19.606.2.E, installation of parking area landscaping shall be required before a certificate of occupancy is issued, unless a performance bond is posted with the City. In the latter case, the landscaping shall be installed within 6 months of the bond's posting, or else the bond will be foreclosed and plant materials installed by the City. Parking area landscaping shall be maintained in good and healthy condition.
5. Expiration of Approval  
As per MMC Subsection 19.1001.7.E, the land use approval granted with this decision shall expire and become void unless the following criteria are satisfied:
  - a. Obtain and pay for all necessary development permits and start construction within 2 years of land use approval.
  - b. Pass final inspection and/or obtain a certificate of occupancy within 4 years of land use approval.



# MHS PARKING LOT ADDITION

11300 SE 23RD AVENUE, MILWAUKIE, OR 97222

**APPLICANT:**

NORTH CLACKAMAS SCHOOL DISTRICT  
1245 SE FULLER ROAD  
MILWAUKIE, OR 97222  
CONTACT: DAVID HOBBS

**PLANNING CONSULTANT**

3J CONSULTING, INC.  
5075 SW GRIFFITH DRIVE, SUITE 150  
BEAVERTON, OR 97005  
CONTACT: ANDREW TULL  
PHONE: (503) 946-9365

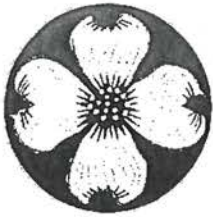
**APPLICATION TYPE**

TYPE III COMMUNITY SERVICE USE MODIFICATION

RECEIVED  
MAR 13 2019  
CITY OF MILWAUKIE  
PLANNING DEPARTMENT

**SUBMITTAL DATE**

MARCH 13, 2019



**PLANNING DEPARTMENT**  
 6101 SE Johnson Creek Blvd  
 Milwaukie OR 97206  
 503-786-7630  
 planning@milwaukieoregon.gov

# Application for Land Use Action

Master File #: CSU-2019-002

Review type\*:  I  II  III  IV  V

**CHOOSE APPLICATION TYPE(S):**

Community Service Use

...

...

...

...

RECEIVED  
 MAR 13 2019  
 CITY OF MILWAUKIE  
 PLANNING DEPARTMENT

- Use separate application forms for:**
- Annexation and/or Boundary Change
  - Compensation for Reduction in Property Value (Measure 37)
  - Daily Display Sign
  - Appeal

**RESPONSIBLE PARTIES:**

**APPLICANT** (owner or other eligible applicant—see reverse): North Clackamas School District

Mailing address: 4444 SE Lake Road Milwaukie, OR Zip: 97222

Phone(s): 503-353-6058 Email: hobbsd@nclack.k12.or.us

**APPLICANT'S REPRESENTATIVE** (if different than above): Andrew Tull

Mailing address: 3J Consulting, Inc. 5075 SW Griffith Drive, Beaverton OR 97005 Zip: 97005

Phone(s): 503-545-1907 Email: andrew.tull@3j-consulting.com

**SITE INFORMATION:**

Address: (No Site Address - Willard and 25th) 2301 SE Willard St Map & Tax Lot(s): 11e36bc 5800


Comprehensive Plan Designation: C/HD Zoning: R-2 Size of property: 0.54 Acres

**PROPOSAL (describe briefly):**

Applicant requests a Type III CSU Modification to the approved MHS CSU permit to allow for the construction of a new parking lot.

**SIGNATURE:**

**ATTEST:** I am the property owner or I am eligible to initiate this application per Milwaukie Municipal Code (MMC) Subsection 19.1001.6.A. If required, I have attached written authorization to submit this application. To the best of my knowledge, the information provided within this application package is complete and accurate.

Submitted by:  Date: 02/27/2019

**IMPORTANT INFORMATION ON REVERSE SIDE**



**WHO IS ELIGIBLE TO SUBMIT A LAND USE APPLICATION** (excerpted from MMC Subsection 19.1001.6.A):

**Type I, II, III, and IV** applications may be initiated by the property owner or contract purchaser of the subject property, any person authorized in writing to represent the property owner or contract purchaser, and any agency that has statutory rights of eminent domain for projects they have the authority to construct.

**Type V** applications may be initiated by any individual.

**PREAPPLICATION CONFERENCE:**

A preapplication conference may be required or desirable prior to submitting this application. Please discuss with Planning staff.

**REVIEW TYPES:**

This application will be processed per the assigned review type, as described in the following sections of the Milwaukie Municipal Code:

- Type I: Section 19.1004
- Type II: Section 19.1005
- Type III: Section 19.1006
- Type IV: Section 19.1007
- Type V: Section 19.1008

**THIS SECTION FOR OFFICE USE ONLY:**

| FILE TYPE   | FILE NUMBER  | FEE AMOUNT* | PERCENT DISCOUNT  | DISCOUNT TYPE | DEPOSIT AMOUNT | DATE STAMP  |
|---|--------------|-------------|-------------------|---------------|----------------|---|
| Master file   | CSU-2019-002 | \$ 2,000    | -                 |               | \$             | RECEIVED<br>MAR 13 2019<br>CITY OF MILWAUKIE<br>PLANNING DEPARTMENT |
| Concurrent application files  |              | \$          |                   |               | \$             |   |
|   |              | \$          |                   |               | \$             |   |
|   |              | \$          |                   |               | \$             |   |
|   |              | \$          |                   |               | \$             |   |
| <b>SUBTOTALS</b>  |              | \$ 2,000    |                   |               | \$             |   |
| <b>TOTAL AMOUNT RECEIVED: \$</b>  |              |             | <b>RECEIPT #:</b> |               | <b>RCD BY:</b> |   |
| <b>Associated application file #s</b> (appeals, modifications, previous approvals, etc.): |              |             |                   |               |                |   |
| <b>Neighborhood District Association(s):</b> Historic Milwaukie                           |              |             |                   |               |                |   |
| <b>Notes:</b>   |              |             |                   |               |                |   |

\*After discount (if any)

**Table of Contents**

GENERAL INFORMATION..... 3

SITE INFORMATION ..... 3

INTRODUCTION..... 4

    APPLICANT'S REQUEST..... 4

    SITE DESCRIPTION/SURROUNDING LAND USE ..... 4

    PROPOSAL..... 4

    NEIGHBORHOOD MEETING ..... 4

APPLICABLE CRITERIA ..... 5

    CHAPTER 19.300 BASE ZONES..... 5

        19.302 MEDIUM AND HIGH DENSITY RESIDENTIAL ZONES ..... 5

    CHAPTER 19.500 SUPPLEMENTARY DEVELOPMENT REGULATIONS ..... 7

        19.501 GENERAL EXCEPTIONS ..... 7

        19.504 SITE DESIGN STANDARDS..... 8

        19.505 BUILDING DESIGN STANDARDS..... 11

    CHAPTER 19.600 OFF-STREET PARKING AND LOADING ..... 12

        19.601 PURPOSE ..... 12

        19.602 APPLICABILITY ..... 12

        19.603 REVIEW PROCESS AND SUBMITTAL REQUIREMENTS ..... 13

        19.604 GENERAL PARKING STANDARDS..... 14

        19.605 VEHICLE PARKING QUANTITY REQUIREMENTS..... 15

        19.606 PARKING AREA DESIGN AND LANDSCAPING ..... 18

        19.608 LOADING ..... 25

        19.609 BICYCLE PARKING ..... 25

        19.610 CARPOOL AND VANPOOL PARKING ..... 25

    CHAPTER 19.700 PUBLIC FACILITY IMPROVEMENTS ..... 26

        19.702 APPLICABILITY ..... 26

        19.703 REVIEW PROCESS ..... 27

        19.704 TRANSPORTATION IMPACT EVALUATION ..... 31

        19.705 ROUGH PROPORTIONALITY ..... 31

        19.708 TRANSPORTATION FACILITY REQUIREMENTS..... 33

        19.709 PUBLIC UTILITY REQUIREMENTS..... 40

CHAPTER 19.900 LAND USE APPLICATIONS..... 42  
19.904 COMMUNITY SERVICE USES..... 42  
SUMMARY AND CONCLUSION ..... 46

**Attachments**

- Attachment A – Application Form
- Attachment B – Property Ownership Information
- Attachment C – Notification Materials
- Attachment D – Technical Reports
- Attachment E – Land Use Plans

**GENERAL INFORMATION**

Applicant: **North Clackamas School District**  
12451 SE Fuller Road  
Milwaukie, OR 97222  
Contact: David Hobbs  
Director of Capital Projects  
Phone: 503-353-6058  
Email: hobbsd@nclack.k12.or.us

Program Manager: **CBRE | HEERY**  
Two Centerpointe Drive, Suite 250  
Lake Oswego, OR 97035  
Contact: Steve Nicholas  
Phone: 503-431-6180  
Email: nicholasst@nclack.k12.or.us

Architect: **BRIC Architecture Inc.**  
1233 NW Northrup Street, Suite 100  
Portland, OR 97209  
Contact: Matt Jacoby  
Phone: 503-595-4900  
Email: matt.jacoby@bric-arch.com

Planning Consultant: **3J Consulting, Inc.**  
5075 SW Griffith Drive, Suite 150  
Beaverton, OR 97005  
Contact: Andrew Tull  
Phone: 503-545-1907  
Email: andrew.tull@3j-consulting.com

**SITE INFORMATION**

Parcel Number: 11E36BC 5600  
Address: 11200 SE 23<sup>rd</sup> Avenue  
Size: 14.7 acres  
Zoning Designation: R-2 and R-1-B  
Existing Use: Milwaukie High School  
Street Functional Classifications: SE Willard Street and SE 25<sup>th</sup> Avenue are classified as local roads.

Surrounding Zoning: The properties to the south and east are zoned R-2. The properties to the west are zoned R-1-B and DMU. The properties to the north are zoned R-1-B and R-2.

## **INTRODUCTION**

### **APPLICANT'S REQUEST**

The North Clackamas School District is proposing to construct a new parking lot on the existing Milwaukie High School campus and seeks approval of an application for a Type III Community Service Use Modification. This narrative has been prepared to describe the proposed development and to document compliance with the relevant sections of Milwaukie's Development Code.

### **SITE DESCRIPTION/SURROUNDING LAND USE**

Milwaukie High School is located at 11300 SE 23<sup>rd</sup> Avenue within the City of Milwaukie. The site consists of one tax lot, 1S1E36BC 5600, which is traversed by SE Willard Road. The site is approximately 14.7 acres and is primarily zoned R-2, with a small portion of the site west of 23<sup>rd</sup> avenue zoned R-1-B. The portion of the site proposed for development is the site of the existing tennis courts, south of Willard Road.

The existing building was established as a school facility in 1907. The school was built long before the CSU section of the City's zoning code was developed so no Community Service Use was originally issued for the facility. The City eventually issued a Community Service Permit for the site and the City has approved major modifications to the School's campus in 1986, 1988, 1993, 1999, 2007 and 2017. The most recent application to the City was a Community Service Use Modification (CSU-2017-007) for a major demolition and renovation and expansion of the school and facilities.

### **PROPOSAL**

The North Clackamas School District is proposing to construct a new parking lot on the existing Milwaukie High School campus. The proposed changes to the school will include the demolition of the tennis courts located southwest of the main school building, south of Willard Street. The proposed parking lot will provide 30 parking stalls, with landscaping and pedestrian pathways connecting to nearby streets and the main campus. Circulation in the proposed parking lot will be one way with an entrance only access on SE Willard Street and an exit only access on SE 25<sup>th</sup> Avenue. The proposed development will include full right-of-way improvements to SE 25<sup>th</sup> Avenue, including paving, sidewalks on both sides, a planter strip on one side and a modified turn-around. New driveways will be provided for adjacent properties which have access to SE 25<sup>th</sup> Avenue.

### **NEIGHBORHOOD MEETING**

The Applicant held a neighborhood meeting to discuss the proposed development on March 4, 2019. Invitations were mailed to property owners within 500 feet of the site. The materials from the neighborhood meeting are included with this application.

## APPLICABLE CRITERIA

The following sections of the City of Milwaukie’s Zoning and Development Ordinance have been extracted as they have been deemed to be applicable to the proposal. Following each bold applicable criteria or design standard, the Applicant has provided a series of draft findings. The intent of providing code and detailed responses and findings is to document, with absolute certainty, that the proposed development has satisfied the approval criteria for a Type III Community Service Use Modification.

### CHAPTER 19.300 BASE ZONES

#### 19.302 MEDIUM AND HIGH DENSITY RESIDENTIAL ZONES

The medium and high density residential zones are Residential Zone R-3, Residential Zone R-2.5, Residential Zone R-2, Residential Zone R-1, and Residential-Business Office Zone R-1-B. These zones implement the Medium Density and High Density residential land use designations in the Milwaukie Comprehensive Plan.

##### 19.302.1 Purpose

The medium and high density residential zones are intended to create and maintain higher density residential neighborhoods that blend a range of housing types with a limited mix of neighborhood-scale commercial, office, and institutional uses.

##### 19.302.2 Allowed Uses in Medium and High Density Residential Zones

Uses allowed, either outright or conditionally, in the medium and high density residential zones are listed in Table 19.302.2 below. Similar uses not listed in the table may be allowed through a Director’s Determination pursuant to Section 19.903. Notes and/or cross references to other applicable code sections are listed in the “Standards/Additional Provisions” column. See Section 19.201 Definitions for specific descriptions of the uses listed in the table.

| Table 19.302.2<br>Medium and High Density Residential Uses Allowed |            |  |
|--|------------|--|
| Use  | R-2        | Standards/Additional Provisions              |
| <b>Accessory and Other Uses</b>                                    |            |  |
| <b>Community Service Use</b>                                       | <b>CSU</b> | <b>Section 19.904 Community Service Uses</b> |

**CSU = Permitted with Community Service Use approval subject to provisions of Section 19.904. Type III review required to establish a new CSU or for major modification of an existing CSU. Type I review required for a minor modification of an existing CSU.**

**Applicant’s Facts and Findings:** All proposed improvements are located within the R-2 zoned portions of the property. School facilities are permitted within the City’s R-2 zoning district when approved through a Community Service Use Application. The Applicant has submitted a Modification to a Community Service Use Application and has

addressed the applicable approval criteria for the base zone and the Community Service Use.

**19.302.4 Development Standards**

In the medium and high density residential zones, the development standards in Table 19.302.4 apply. Notes and/or cross references to other applicable code sections are listed in the “Standards/Additional Provisions” column. Additional standards are provided in Section 19.302.5.

The standards in Subsection 19.302.4 are not applicable to cottage cluster development except where specifically referenced by Subsection 19.505.4.

See Sections 19.201 Definitions and 19.202 Measurements for specific descriptions of standards and measurements listed in the table.

| <b>Table 19.302.4<br/>Medium and High Density Residential Development Standards</b> |  |  |
|---|--|--|
| <b>Standard</b>   | <b>R-2</b>                                   | <b>Standards/ Additional Provisions</b>  |
| <b>A. Lot Standards</b>   |  |  |
| <b>1. Minimum lot size (sq ft)</b>  | <b>2,500</b>                                 | <b>Subsection 19.501.1 Lot Size Exceptions<br/>Subsection 19.505.4 Cottage Cluster Housing<br/>Subsection 19.505.5 Rowhouses</b>   |
| <b>a. Rowhouse</b>  | <b>7,000</b>                                 |  |
| <b>b. Duplex</b>  | <b>5,000</b>                                 |  |
| <b>c. All other lots</b>  |  |  |
| <b>2. Minimum lot width (ft)</b>  | <b>25</b>                                    |  |
| <b>a. Rowhouse</b>  | <b>50</b>                                    |  |
| <b>b. All other lots</b>  |  |  |
| <b>3. Minimum lot depth (ft)</b>  | <b>80</b>                                    |  |
| <b>a. Rowhouse</b>  | <b>80</b>                                    |  |
| <b>b. All other lots</b>  |  |  |
| <b>4. Minimum street frontage requirements (ft)</b>                                 | <b>25</b>                                    |  |
| <b>a. Rowhouse</b>  | <b>35</b>                                    |  |
| <b>b. Standard lot</b>  | <b>25</b>                                    |  |
| <b>c. Flag lot</b>  | <b>35</b>                                    |  |
| <b>d. Double flag lot</b>   |  |  |
| <b>B. Development Standards</b>   |  |  |
| <b>1. Minimum yard requirements for primary structures (ft)</b>                     | <b>15</b>                                    | <b>Subsection 19.302.5.A Side Yards<br/>Subsection 19.501.2 Yard Exceptions<br/>Subsection 19.504.8 Flag Lot Design and Development Standards</b>                          |
| <b>a. Front yard</b>  | <b>See Subsection 19.302.5.A</b>             |  |
| <b>b. Side yard</b>   | <b>15</b>                                    |  |
| <b>c. Street side yard</b>  | <b>15</b>                                    |  |
| <b>d. Rear yard</b>   | <b>15</b>                                    |  |
| <b>2. Maximum building height for primary structures</b>                            | <b>3 stories or 45 ft, whichever is less</b> | <b>Subsection 19.302.5.E Height Exceptions<br/>Subsection 19.501.3 Building Height and Side Yard Height Plane Exceptions<br/>Subsection 19.302.5.I Transition Measures</b> |

|   |              |   |
|---|--------------|---|
| 3. Side yard height plane limit<br>a. Height above ground at minimum required side yard depth (ft)<br>b. Slope of plane (degrees) | 25<br><br>45 | Subsection 19.501.3 Building Height and Side Yard Height Plane Exceptions   |
| 4. Maximum lot coverage (percent of total lot area)   | 45%          | Section 19.201 "Lot coverage" definition  |
| 5. Minimum vegetation (percent of total lot area)   | 15%          | Subsection 19.504.7 Minimum Vegetation<br>Subsection 19.302.5.D Front Yard Minimum Vegetation<br>Subsection 19.302.5.C Minimum Vegetation |
| <b>C. Other Standards</b>   |              |   |
| 1. Density requirements (dwelling units per acre)<br>a. Minimum<br>b. Maximum   | 11.6<br>17.4 | Subsection 19.202.4 Density Calculations<br>Subsection 19.302.5.F Residential Densities<br>Subsection 19.501.4 Density Exceptions         |

**Applicant's Facts and Findings:** The Applicant has not proposed any residential development therefore the standards of the underlying zone associated with residential developments do not apply to this proposal.

**CHAPTER 19.500 SUPPLEMENTARY DEVELOPMENT REGULATIONS**

**19.501 GENERAL EXCEPTIONS**

The exceptions listed in Subsections 19.501.1-4 below are "by right" exceptions. "By right" exceptions require no special review or approval by the City to implement.

**19.501.3 Building Height and Side Yard Height Plane Exceptions**

- A. Projections such as chimneys, spires, domes, elevator shaft housings, flagpoles, and other similar objects not used for human occupancy are not subject to the building height and side yard height plane limitations of the Zoning Ordinance, except as provided in an L-F Zone.**
- B. The following encroachments into a side yard height plane are allowed:**
  - 1. Roof overhangs or eaves, provided that they do not extend more than 30 in horizontally beyond the side yard height plane.**
  - 2. The gable end of a roof, provided that the encroachment is not more than 8 ft high above the side yard height plane or more than 40 ft wide.**
  - 3. Dormers, with the following limitations:**
    - a. The highest point of any dormer is at or below the height of the primary roof ridge.**
    - b. The encroachment is not more than 6 ft high above the side yard height plane or more than 8 ft wide.**



- c. **The combined width of all dormers does not exceed 50% of the length of the roof on which they are located.**

**Applicant's Facts and Findings:** No exceptions to the building height and side yard planes are proposed. The requirements of this section do not apply.

## 19.504 SITE DESIGN STANDARDS

### 19.504.9 On-Site Walkways and Circulation

#### A. Requirement

**All development subject to Chapter 19.700 (excluding single-family and multifamily residential development) shall provide a system of walkways that encourages safe and convenient pedestrian movement within and through the development site.**

**Redevelopment projects that involve remodeling or changes in use shall be brought closer into conformance with this requirement to the greatest extent practicable. On-site walkways shall link the site with the public street sidewalk system. Walkways are required between parts of a site where the public is invited to walk. Walkways are not required between buildings or portions of a site that are not intended or likely to be used by pedestrians, such as truck loading docks and warehouses.**

**Applicant's Facts and Findings:** The proposed parking lot renovation will include new sidewalks along the frontage of SE Willard Street and SE 25<sup>th</sup> Street. An onsite pedestrian pathway will connect to the sidewalk along the frontage. A new crossing will be provided at SE 25<sup>th</sup> Street, connecting to a crossing at SE Willard Street to the main campus. The proposed on-site walkways and circulation will provide safe and convenient pedestrian movements within and through the site.

The requirements of this section have been met.

#### B. Location

**A walkway into the site shall be provided for every 300 ft of street frontage.**

**Applicant's Facts and Findings:** A walkway into the site has been provided along the frontage SE Willard Street. The requirements of this section have been met.

#### C. Connections

**Walkways shall connect building entrances to one another and building entrances to adjacent public streets and existing or planned transit stops. On-site walkways shall connect with walkways, sidewalks, bicycle facilities, alleys, and other bicycle or pedestrian connections on adjacent properties used or planned for commercial, multifamily, institutional, or park use. The City may require connections to be constructed and extended to the property line at the time of development.**

**Applicant's Facts and Findings:** The onsite pedestrian pathway will connect to the sidewalk along the frontage. A new crossing will be provided at SE 25<sup>th</sup> Street, connecting to a crossing at SE Willard Street to the main campus pedestrian network. The

proposed on-site walkways and circulation will provide safe and convenient pedestrian movements within and through the site.

The requirements of this section have been met.

#### **D. Routing**

**Walkways shall be reasonably direct. Driveway crossings shall be minimized. Internal parking lot circulation and design shall provide reasonably direct access for pedestrians from streets and transit stops to primary buildings on the site.**

**Applicant's Facts and Findings:** The proposed walkway will provide access from the parking area to the main campus with minimal driveway crossing conflicts. The proposed pedestrian connection system meets the requirements of this code.

#### **E. Design Standards**

**Walkways shall be constructed with a hard surface material, shall be permeable for stormwater, and shall be no less than 5 ft in width. If adjacent to a parking area where vehicles will overhang the walkway, a 7-ft-wide walkway shall be provided. The walkways shall be separated from parking areas and internal driveways using curbing, landscaping, or distinctive paving materials. On-site walkways shall be lighted to an average 5/10-footcandle level. Stairs or ramps shall be provided where necessary to provide a direct route.**

**Applicant's Facts and Findings:** The proposed pedestrian walkway will be constructed of concrete. Plans C204 illustrates the location and material proposed for the site's walkway. The proposed walkway will have a width of five (5) feet. Wheel stops have been provided to prevent vehicle overhang in the parking lot.

As shown on the attached lighting and photometric plans, the walkway on site will be illuminated at levels between 1.42- and 15.7-foot candles. The placement of light fixtures has been deliberately considered in order to provide the highest levels of lighting along the primary pedestrian routes. The proposed pedestrian connection system meets the requirements of this code.

#### **19.504.10 Setbacks Adjacent to Transit**

**The following requirement applies to all new commercial, office, and institutional development within 500 ft of an existing or planned transit route measured along the public sidewalk that provides direct access to the transit route:**

**When adjacent to a street served by transit, new commercial, office, or institutional development, including uses authorized under Section 19.904 Community Service Uses, shall be set back no more than 30 ft from the right-of-way that is providing transit service.**

**A. An individual building may be set back more than 30 ft, provided the building is part of an approved phased development that will result in a future building(s) that complies with the 30-ft setback standard.**

- B. For sites with multiple buildings, the maximum distance from a street with transit to a public entrance of the primary building shall be no more than 100 ft.
- C. If the proposed building is part of an institutional campus, the Planning Director may allow flexibility in the setback and orientation of the building. As a trade-off for this flexibility, enhanced sidewalk connections shall be provided between the institutional building(s) and nearby transit stops.
- D. If the site abuts more than 1 street served by transit, then the maximum setback requirement need only apply to 1 street.

**Applicant's Facts and Findings:** The required building setbacks are not applicable to parking lot development.

#### 19.504.11 Preliminary Circulation Plan

A preliminary circulation plan is intended to guide site development by establishing a plan for multimodal access, connectivity, and circulation. A preliminary circulation plan is a conceptual plan, in that it does not establish a precise alignment for street, pedestrian, or bicycle facilities.

##### A. Applicability

A preliminary circulation plan is required for nonresidential development on sites 3 acres and larger that are subject to development review per Section 19.906 and where any of the following is true:

1. The site is vacant.
2. The proposed new development or redevelopment will result in reconfiguration of the transportation and development pattern for > 50% of the site.
3. The development is in the Flex Space Overlay Zone.

##### B. Plan Contents

1. The preliminary circulation plan shall include a site plan, showing land uses; building envelopes and other structures; the pedestrian, bicycle, and vehicle circulation system; vehicle and bicycle parking areas; open areas; existing trees to be preserved; and utility connections. The site plan must also include the following:
  - a. All existing improvements that will remain after development of the proposed use.
  - b. All improvements planned in conjunction with the proposed use.
  - c. Conceptual plans for possible future uses.
  - d. Pedestrian and bicycle facilities, including safe pedestrian and safe bicycle circulation between the following:
    - (1) Major buildings, activity areas, and transit stops within the site plan boundaries and adjacent streets, pathways, and transit stops.
    - (2) Adjacent developments and the proposed development.

2. The preliminary circulation plan shall include a public right-of-way/easement plan depicting the following, if applicable:

Reservation, dedication, or use of the proposed site for public purposes, including, but not limited to the following: rights-of-way, showing the name and location of all existing and proposed public and private access drives within or on the boundary of the proposed site; the right-of-way and paving dimensions; the ownership and maintenance status, if applicable; the location, width, and construction material of all existing and proposed sidewalks; pedestrian accessways and trails; and bicycle accessways and trails.

**Applicant's Facts and Findings:** The Applicant has provided a Circulation Plan under Appendix E which describes the walkable and navigable areas of the site for pedestrians and vehicles.

### C. Approval Criteria

**In reviewing a proposed preliminary circulation plan, the Planning Director shall find compliance with the relevant portions of the Comprehensive Plan, Transportation System Plan, and Section 19.708 Transportation Facility Requirements.**

**Applicant's Facts and Findings:** The Applicant has provided a Circulation Plan under Appendix E which describes the walkable and navigable areas of the site for pedestrians and vehicles. The circulation plan proposed by the applicant is consistent with the City's Comprehensive Plan and Transportation System Plan. Consistency with the Transportation System plan and Comprehensive plan has been achieved through a series of dedications and proposed roadway improvements which are aligned with the requirements for each of the roads which front the property based upon their functional classifications. The improvements proposed are described in detail in response to section 19.708.

## 19.505 BUILDING DESIGN STANDARDS

### 19.505.8 Building Orientation to Transit

The following requirement applies to all new commercial, office, mixed-use, and institutional development within 500 ft of an existing or planned transit route measured along the public sidewalk that provides direct access to the transit route:

New buildings shall have their primary orientation toward a transit street or, if not adjacent to a transit street, a public right-of-way which leads to a transit street. The primary building entrance shall be visible from the street and shall be directly accessible from a sidewalk connected to the public right-of-way. A building may have more than 1 entrance. If the development has frontage on more than 1 transit street, the primary building entrance may be oriented to either street or to the corner.

**Applicant's Facts and Findings:** The proposed development does not include a building. The requirements of this section are not applicable.

## **CHAPTER 19.600 OFF-STREET PARKING AND LOADING**

### **19.601 PURPOSE**

**Chapter 19.600 regulates off-street parking and loading areas on private property outside the public right-of-way. The purpose of Chapter 19.600 is to: provide adequate, but not excessive, space for off-street parking; avoid parking-related congestion on the streets; avoid unnecessary conflicts between vehicles, bicycles, and pedestrians; encourage bicycling, transit, and carpooling; minimize parking impacts to adjacent properties; improve the appearance of parking areas; and minimize environmental impacts of parking areas.**

**Regulations governing the provision of on-street parking within the right-of-way are contained in Chapter 19.700. The management of on-street parking is governed by Chapter 10.20. Chapter 19.600 does not enforce compliance with the Americans with Disabilities Act (ADA). ADA compliance on private property is reviewed and enforced by the Building Official.**

### **19.602 APPLICABILITY**

#### **19.602.1 General Applicability**

**The regulations of Chapter 19.600 apply to all off-street parking areas and off-street loading areas, whether required by the City as part of development or a change in use, per Subsection 19.602.3, or voluntarily installed for the convenience of users, per Subsection 19.602.4. Activity that is not described by Subsections 19.602.3 or 4 is exempt from compliance with the provisions of Chapter 19.600. Changes to nonconforming off-street parking and loading are addressed through Chapter 19.600 and not through the provisions of Chapter 19.800.**

#### **19.602.2 Maintenance Applicability**

**Property owners shall comply with the regulations of Chapter 19.600 by ensuring conformance with the standards of Chapter 19.600 related to ongoing maintenance, operations, and use of off-street parking and loading areas. Changes to existing off-street parking or loading areas that bring the area out of conformance with Chapter 19.600, or further out of conformance if already nonconforming, are prohibited.**

#### **19.602.3 Applicability for Development and Change in Use Activity**

**The provisions of Chapter 19.600 apply to development and changes of use as described in Subsection 19.602.3.**

**A. Development of a vacant site shall have off-street parking and off-street loading areas that conform to the requirements of Chapter 19.600. Development of a site that results in an increase of 100% or more of the existing floor area and/or structure footprint on a site shall also conform to the requirements of Chapter 19.600. The floor area and/or footprint of structures demolished prior to development or redevelopment on the site shall not be considered when calculating the increase in floor area and/or structural footprints.**

**B. Existing off-street parking and loading areas shall be brought closer into conformance with the standards of Chapter 19.600, per Subsection 19.602.5, when the following types of development or change in use occur:**

- 1. Development that results in an increase of less than 100% of the existing floor area and/or structure footprint.**

## 2. Changes of use, as defined in Section 19.201.

### 19.602.4 Applicability not Associated With Development or Change in Use

A. Any parking or loading area developed to serve an existing use(s) that is not associated with development activity or a change in use described in Subsection 19.602.3 shall conform to the requirements of Sections 19.604 and 19.606-19.611. The total number of spaces in the existing parking area and new parking area shall not exceed the maximum allowed quantity of parking as established in Section 19.605.

B. Any parking or loading area that is not developed to serve an existing use and is not associated with development activity or a change in use as described in Subsection 19.602.3 shall conform to the requirements of Sections 19.604 and 19.606-19.611. The requirements of Section 19.605 do not apply to parking areas described under Subsection 19.602.4.B.

**Applicant's Facts and Findings:** The proposed development will provide a new parking lot to serve the existing Milwaukie High School. Sections 19.604 and 19.606-19.611 have been addressed within this narrative.

### 19.602.5 Improvements to Existing Off-Street Parking and Loading Areas

#### A. Purpose

The purpose of Subsection 19.602.5 is to improve nonconforming off-street parking and loading areas as redevelopment occurs. These improvements should occur in conjunction with a development or change in use.

**Applicant's Facts and Findings:** The proposed development will provide a new parking lot in the location of the existing tennis courts. The requirements of this section are not applicable.

## 19.603 REVIEW PROCESS AND SUBMITTAL REQUIREMENTS

### 19.603.1 Review Process

The Planning Director shall apply the provisions of Chapter 19.600 in reviewing all land use and development permit applications, except when an application is subject to a quasi-judicial land use review or appeal, in which case the body reviewing the application or appeal has the authority to implement and interpret the provisions of Chapter 19.600.

### 19.603.2 Submittal Requirements

Except for single-family dwellings, a development or change in use subject to Chapter 19.600 as per Section 19.602 shall submit a parking plan, drawn to scale. The parking plan shall show that all applicable standards are met, and shall include but not be limited to the items listed below, unless waived by the Planning Director.

A. Delineation of individual spaces and wheel stops.

B. Drive aisles necessary to serve spaces.

C. Accessways, including driveways and driveway approaches, to streets, alleys, and properties to be served.

D. Pedestrian pathways and circulation.

- E. Bicycle parking areas and rack specifications.
- F. Fencing.
- G. Abutting land uses.
- H. Grading, drainage, surfacing, and subgrading details.
- I. Location and design of lighting fixtures and levels of illumination.
- J. Delineation of existing and proposed structures.
- K. Parking and loading area signage.
- L. Landscaping, including the following information.
  1. The location and area of existing and proposed trees, vegetation, and plant materials, including details about the number, size, and species of such items.
  2. Notation of the trees, plants, and vegetation to be removed, and protection measures for existing trees and plants to be preserved.

**Applicant's Facts and Findings:** The Applicant has provided detailed site and landscape plans which contain the information required within this section.

#### 19.604 GENERAL PARKING STANDARDS

##### 19.604.1 Parking Provided with Development Activity

All required off-street parking areas shall be provided at the time the structure is built; at the time a structure or site is enlarged; or when there is change in use or an increase in density or intensity. All required off-street parking areas shall be provided in conformance with the standards of Chapter 19.600 prior to issuance of a certificate of occupancy, or final development permit approval, or as otherwise specified in any applicable land use decision.

**Applicant's Facts and Findings:** The proposed parking lot has been designed in accordance with the requirements of Chapter 19.600. This standard has been met.

##### 19.604.2 Parking Area Location

Accessory parking shall be located in one or more of the following areas:

- A. On the same site as the primary use for which the parking is accessory.
- B. On a site owned by the same entity as the site containing the primary use that meets the standards of Subsection 19.605.4.B.2. Accessory parking that is located in this manner shall not be considered a parking facility for purposes of the base zones in Chapter 19.300.
- C. Where shared parking is approved in conformance with Subsection 19.605.4.

**Applicant's Facts and Findings:** The District is proposing to create a new parking plan for the new Milwaukie High School Building. The redesigned parking lots are located on the same site as the primary use for which the parking is accessory and on property owned by the District. This standard has been met.

##### 19.604.3 Use of Parking Area

All required off-street parking areas shall continually be available for the parking of operable vehicles of intended users of the site. Required parking shall not be rented, leased, sold, or otherwise used for parking that is unrelated to the primary or accessory use of the site, except where a shared parking agreement per Subsection 19.605.4 has been recorded. Subsection

**19.604.3 does not prohibit charging fees for parking when the parking serves the primary or accessory uses on site.**

**Applicant's Facts and Findings:** The District intends to make all proposed parking areas continually available for the parking of operable vehicles related to the operation of the site as a High School. The District does not intend to rent, lease, or sell parking stalls. This standard has been met.

#### **19.604.4 Storage Prohibited**

**No required off-street parking area shall be used for storage of equipment or materials, except as specifically authorized by Subsection 19.607.2 Commercial Vehicle, Pleasure Craft, and Recreational Vehicle Parking**

**Applicant's Facts and Findings:** The District does not intend to utilize any portion of the proposed parking areas on site for storage of equipment or materials following the completion of construction activities. Parking areas may be utilized for temporary staging or storage during various phases of construction. This standard has been met.

#### **19.605 VEHICLE PARKING QUANTITY REQUIREMENTS**

**The purpose of Section 19.605 is to ensure that development provides adequate, but not excessive, vehicle parking based on their estimated parking demand. Subsection 19.605.1 establishes parking ratios for common land uses, and Subsection 19.605.3 allows certain exemptions and reductions to these ratios based on location or on-site amenities. Modifications to the established parking ratios and determinations of parking requirements for unique land uses are allowed with discretionary review per Subsection 19.605.2.**

**Nonresidential development in the Downtown Mixed Use (DMU) and Open Space (OS) Zones is exempt from the requirements of Section 19.605.**

##### **19.605.1 Minimum and Maximum Requirements**

**A. Development shall provide at least the minimum and not more than the maximum number of parking spaces as listed in Table 19.605.1. Modifications to the standards in Table 19.605.1 may be made as per Section 19.605. Where multiple ratios are listed, the Planning Director shall determine which ratio to apply to the proposed development or use.**

**B. When a specific use has not been proposed or identified at the time of permit review, the Planning Director may elect to assign a use category from Table 19.605.1 to determine the minimum required and maximum allowed parking. Future tenants or property owners are responsible for compliance with Chapter 19.600 per the applicability provisions of Section 19.602.**

**C. If a proposed use is not listed in Table 19.605.1, the Planning Director has the discretion to apply the quantity requirements of a similar use listed in the table upon finding that the listed use and unlisted use have similar parking demands. If a similar use is not listed, the quantity requirements will be determined per Subsection 19.605.2.**



D. Where the calculation of minimum parking spaces does not result in a whole number, the result shall be rounded down to the next whole number. Where the calculation of maximum parking spaces does not result in a whole number, the result shall be rounded to the nearest whole number.

E. Parking spaces for disabled persons, and other improvements related to parking, loading, and maneuvering for disabled persons, shall conform to the Americans with Disabilities Act and shall be subject to review and approval by the Building Official. Spaces reserved for disabled persons are included in the minimum required and maximum allowed number of off-street parking spaces.

F. Uses that have legally established parking areas that exceed the maximum number of spaces allowed by Section 19.605 prior to June 17, 2010, the effective date of Ordinance #2015, shall be considered nonconforming with respect to the quantity requirements. Such uses shall not be considered parking facilities as defined in Section 19.201.

| Table 19.605.1<br>Minimum to Maximum Off-Street Parking Requirements |  |  |
|--|--|--|
| Use  | Minimum Required                                 | Maximum Allowed                                  |
| <b>B. Community Service and Other Public Uses</b>                    |  |  |
| 4. School—senior high.   | 0.25 spaces per student, plus 1 space per staff. | 0.33 spaces per student, plus 1 space per staff. |

**Applicant’s Facts and Findings:** The school district currently provides service to approximately 1,500 students and employs approximately 140 staff members. The minimum and maximum range of parking spaces required for the High School is as follows:

- Minimum Required Off-street Parking Requirement – 515 Stalls
- Maximum Permitted Off-street Parking Requirement – 635 Stalls

The previously approved Community Service Use Modification for the major renovation of the school provided a total of 333 parking stalls available for use by the students and staff on site and through the use of shared parking agreements. The proposed development will add 30 parking stalls for a total of 363 stalls. While the proposed development will not meet the minimum requirements of the code, it will bring the school closer to conformance with the requirements of this code.

**19.605.3 Exemptions and By-Right Reductions to Quantity Requirements**

The following exemptions and by-right reductions cannot be used to further modify any parking modification or determination granted under Subsection 19.605.2.

**A. Exemptions to Maximum Quantity Allowance**

The following types of parking do not count toward the maximum amount of parking allowed on a site. This exemption applies only to the quantity requirements of Section 19.605 and not

to the other requirements of Chapter 19.600. The City may impose conditions to ensure that parking spaces associated with these parking types are appropriately identified and used for the intended purpose.

1. Spaces for a parking facility.
2. Spaces for a transit facility or park and ride facility.
3. Storage or display areas for vehicle sales.
4. Employee carpool parking, when spaces are dedicated or reserved for that use.
5. Fleet parking
6. Truck loading areas.

#### **B. Reductions to Minimum Parking Requirements**

Applicants are allowed to utilize multiple reductions from Subsections 19.605.3.B.2-7, provided that the total reduction in required parking does not exceed 25% of the minimum quantity requirement listed in Table 19.605.1. The total reduction in required parking is increased to 30% in the Downtown Mixed Use Zone DMU. Applicants may not utilize the reduction in Subsection 19.605.3.B.1 in conjunction with any other reduction in Subsection 19.605.3.B.

##### **1. Reductions for Neighborhood Commercial Areas**

The minimum parking requirements of Table 19.605.1 shall be reduced by 50% for the properties described below:

- a. Properties zoned Commercial Limited (C-L).
- b. Properties zoned Commercial Neighborhood (C-N).
- c. Properties in the Neighborhood Mixed-Use (NMU) Zone in the area bounded by 40th Ave, King Rd, 44th Ave, and Jackson St.

##### **2. Proximity to Public Transit**

- a. Parking for commercial and industrial uses may be reduced by up to 10% if the development is within 500-ft walking distance, as defined in Subsection 19.605.3.B.2.d, of a transit stop with a peak hour service frequency of 30 minutes or less.
- b. Parking for multifamily uses may be reduced by up to 20% if the development is within 500-ft walking distance, as defined in Subsection 19.605.3.B.2.d, of a transit stop with a peak hour service frequency of 30 minutes or less.
- c. Parking for all uses except single-family attached and detached dwellings may be reduced by 25% if the development is within 1,000-ft walking distance, as defined in Subsection 19.605.3.B.2.d, of a light rail transit stop, or if it is located in the Downtown Mixed Use Zone DMU.

**Applicant's Facts and Findings:** The Milwaukie High School Campus qualifies for a 25% reduction in the minimum required parking standards due to the site's distance from Public Transit. The site is located within 1000 feet of the Orange MAX Line Station. The total required minimum parking is 515 stalls based upon current employment and student enrollment. The permitted reduction through the provision of qualifying reductive improvements is 129 parking stalls. The minimum number of stalls required based upon the site's proximity to public Transit is 386 parking stalls.

- d. In determining walking distance, the applicant shall measure the shortest route along sidewalks, improved pedestrian ways, or streets if sidewalks or improved pedestrian ways are not present. Walking distance shall be measured along the shortest course from the point on the development site that is nearest to the transit stop.

**Applicant’s Facts and Findings:** The walking distance from the campus to the Orange MAX Line Station is less than 500 feet.

**19.606 PARKING AREA DESIGN AND LANDSCAPING**

The purpose of Section 19.606 is to ensure that off-street parking areas are safe, environmentally sound, aesthetically pleasing, and that they have efficient circulation. These standards apply to all types of development except for cottage clusters, rowhouses, duplexes, single-family detached dwellings, and residential homes.

**19.606.1 Parking Space and Aisle Dimensions**

- A. The dimensions for required off-street parking spaces and abutting drive aisles, where required, shall be no less than in Table 19.606.1. The minimum dimensions listed in Table 19.606.1 are illustrated in Figure 19.606.1.

| Table 19.606.1<br>Minimum Parking Space and Aisle Dimensions |           |                 |                       |                       |           |
|--|-----------|-----------------|-----------------------|-----------------------|-----------|
| Angle (A)  | Width (B) | Curb Length (C) | 1-Way Aisle Width (D) | 2-Way Aisle Width (D) | Depth (E) |
| 0° (Parallel)  | 8.5'      | 22'             | 12'                   | 19'                   | 8.5'      |
| 30°  | 9'        | 17'             | 12'                   | 19'                   | 16.5'     |
| 45°  | 9'        | 12'             | 13'                   | 19'                   | 18.5'     |
| 60°  | 9'        | 10'             | 17'                   | 19'                   | 19'       |
| 90°  | 9'        | 9'              | 22'                   | 22'                   | 18'       |

**Applicant’s Facts and Findings:** The proposed parking lot will have 45-degree and 90-degree parking. All proposed parking stalls meet the width and depth requirements of this section. The proposed 1-way drive aisle has a width of 15 feet. The requirements of this section have been met.

- B. The dimension of vehicle parking spaces provided for disabled persons shall be according to federal and State requirements.

**Applicant’s Facts and Findings:** Parking for disabled or limited mobility individuals has been provided on the main campus, near the building entrances. ADA parking was provided in accordance with the requirements of the Americans with Disabilities Act (ADA) in accordance with the requirements of the Oregon Structural Specialty Codes (OSSC). The requirements of this section have been met.

- C. **Parking spaces shall be provided with adequate aisles or turnaround areas so that all vehicles may enter the street in a forward manner.**

**Applicant's Facts and Findings:** All proposed parking areas contain sufficient maneuvering areas to allow vehicles to enter the street in a forward manner. The requirements of this section have been met.

- D. **Drive aisles shall be required in parking areas greater than 5 spaces. Drive aisles shall meet the minimum width standards of Subsection 19.606.1. Where a drive aisle or portion thereof does not abut a parking space(s), the minimum allowed width for a one-way drive aisle shall be 8 ft and the minimum allowed width for a two-way drive aisle shall be 16 ft.**

**Applicant's Facts and Findings:** All proposed drive aisles have been designed to meet the minimum width requirements for two-way traffic. The requirements of this section have been met.

### 19.606.2 Landscaping

#### A. Purpose

The purpose of the off-street parking lot landscaping standards is to provide vertical and horizontal buffering between parking areas and adjacent properties, break up large expanses of paved area, help delineate parking spaces and drive aisles, and provide environmental benefits such as stormwater management, carbon dioxide absorption, and a reduction of the urban heat island effect.

#### B. General Provisions

1. **Parking area landscaping shall be required for the surface parking areas of all uses, except for cottage clusters, rowhouses, duplexes, and single-family detached dwellings. Landscaping shall be based on the standards in Subsections 19.606.2.C-E.**
2. **Landscaped areas required by Subsection 19.606.2 shall count toward the minimum amount of landscaped area required in other portions of Title 19.**
3. **Parking areas with 10 or fewer spaces in the Downtown Mixed Use Zone are exempt from the requirements of Subsection 19.606.2.**

**Applicant's Facts and Findings:** The Applicant has prepared a conceptual landscape plan for the property at this time. The Applicant is willing to accept conditions of approval requiring the final construction plans to conform to the requirements of the City's landscape standards for parking areas and interior landscaping prior to the issuance of building permits. The requirements of this section can be met through a condition of approval.

#### C. Perimeter Landscaping

The perimeter landscaping of parking areas shall meet the following standards which are illustrated in Figure 19.606.2.C.

1. **Dimensions**

The minimum width of perimeter landscape areas are shown in Table 19.606.2.C.1. Where a curb provides the border for a perimeter landscape area, the dimension shall be measured from the inside of the curb(s). The Planning Director may reduce the required minimum width of a perimeter landscaping area where existing development or site constraints make it infeasible to provide drive aisles, parking spaces, and the perimeter landscaping buffer width listed in Table 19.606.2.C.1.

| Table 19.606.2.C.1<br>Minimum Perimeter Landscape Strip Dimensions                           |                |                 |
|--|----------------|-----------------|
| Location   | Downtown Zones | All Other Zones |
| Lot line abutting a right-of-way   | 4'             | 8'              |
| Lot line abutting another property, except for abutting properties that share a parking area | 0'             | 6'              |

**Applicant's Facts and Findings:** The new parking lot has been separated from the SE Willard Street right-of-way with an eight (8) foot planted area. An eight (8) foot wide planting area has been provided between SE 25<sup>th</sup> Street and the proposed parking lot. The parking lot has been separated from neighboring properties with a planted area of six feet. The requirements of this section have been met.

**2. Planting Requirements**

**Landscaping requirements for perimeter buffer areas shall include 1 tree planted per 40 lineal ft of landscaped buffer area. Where the calculation of the number of trees does not result in a whole number, the result shall be rounded up to the next whole number. Trees shall be planted at evenly spaced intervals along the perimeter buffer to the greatest extent practicable. The remainder of the buffer area shall be grass, ground cover, mulch, shrubs, trees, or other landscape treatment other than concrete and pavement.**

**Applicant's Facts and Findings:** The Applicant has prepared a landscape plan for the property at this time. The Applicant is willing to accept conditions of approval requiring the final construction plans to conform to the requirements of the City's landscape standards for parking areas and interior landscaping prior to the issuance of building permits. The requirements of this section can be met through a condition of approval.

**3. Additional Planting Requirements Adjacent to Residential Uses**

**In addition to the planting requirements of Subsection 19.606.2.D.2, all parking areas adjacent to a residential use shall have a continuous visual screen in the landscape perimeter area that abuts the residential use. The area of required screening is illustrated in Figure 19.606.2.C.3. The screen must be opaque throughout the year from 1 to 4 ft above ground to adequately screen vehicle lights.**

**These standards must be met at the time of planting. Examples of acceptable visual screens are a fence or wall, an earth berm with plantings, and other plantings of trees and shrubs.**

**Applicant's Facts and Findings:** The parking lot abuts residential properties to the south and east. The Applicant has prepared only a conceptual landscape plan for the property at this time. The Applicant is willing to accept conditions of approval requiring the final construction plans to conform to the requirements of the City's landscape standards for parking areas and interior landscaping prior to the issuance of building permits. The requirements of this section can be met through a condition of approval.

**D. Interior Landscaping**

**The interior landscaping of parking areas shall meet the following standards which are illustrated in Figure 19.606.2.D.**

**1. General Requirements**

**Interior landscaping of parking areas shall be provided for sites where there are more than 10 parking spaces on the entire site. Landscaping that is contiguous to a perimeter landscaping area and exceeds the minimum width required by Subsection 19.606.2.C.1 will be counted as interior landscaping if it meets all other requirements of Subsection 19.606.2.D.**

**2. Required Amount of Interior Landscaped Area**

**At least 25 sq ft of interior landscaped area must be provided for each parking space. Planting areas must be at least 120 sq ft in area and dispersed throughout the parking area.**

**Applicant's Facts and Findings:** The proposed parking lot will provide 30 new parking spaces. A total of 750 square feet of new landscape area is required. Approximately 3,428 square feet has been provided with planting areas which all exceed the minimum 120 square foot sizing requirements.

The Applicant has prepared only a conceptual landscape plan for the property at this time. The Applicant is willing to accept conditions of approval requiring the final construction plans to conform to the requirements of the City's landscape standards for parking areas and interior landscaping prior to the issuance of building permits. The requirements of this section can be met through a condition of approval.

**3. Location and Dimensions of Interior Landscaped Areas**

- a. Interior landscaped area shall be either a divider median between opposing rows of parking, or a landscape island in the middle or at the end of a parking row.**
- b. Interior landscaped areas must be a minimum of 6 ft in width. Where a curb provides the border for an interior landscape area, the dimension shall be measured from the inside of the curb(s).**

**4. Planting Requirements for Interior Landscaped Areas**

- a. For divider medians, at least 1 shade or canopy tree must be planted for every 40 linear ft. Where the calculation of the number of trees does not result in a whole number, the result shall be rounded up to the next whole number. Trees shall be planted at evenly spaced intervals to the greatest extent practicable.
- b. For landscape islands, at least 1 tree shall be planted per island. If 2 interior islands are located contiguously, they may be combined and counted as 2 islands with 2 trees planted.
- c. The remainder of any divider median or landscape island shall be grass, ground cover, mulch, shrubs, trees, or other landscape treatment other than concrete and pavement.

**Applicant's Facts and Findings:** A landscaped divider median meeting the requirements of this section has been provided along the interior of the parking lot. The requirements of this section have been met.

**5. Additional Landscaping for Large Parking Areas**

Parking areas with more than 100 spaces on a site shall not have more than 15 spaces in a row without providing an interior landscaped island. See Figure 19.606.2.D.5.

**Applicant's Facts and Findings:** The proposed parking lot has 30 spaces. The requirements of this section are not applicable.

**E. Other Parking Area Landscaping Provisions**

- 1. Preservation of existing trees is encouraged in the off-street parking area and may be credited toward the total number of trees required, based on staff's review.
- 2. Installation of parking area landscaping shall be required before a certificate of occupancy is issued, unless a performance bond is posted with the City. Then landscaping shall be installed within 6 months thereafter or else the bond will be foreclosed and plant materials installed by the City.
- 3. Parking area landscaping shall be maintained in good and healthy condition.
- 4. Required parking landscaping areas may serve as stormwater management facilities for the site. The Engineering Director has the authority to review and approve the design of such areas for conformance with the Public Works Standards. This allowance does not exempt the off-street parking landscape area from meeting the design or planting standards of Subsection 19.606.2.
- 5. Pedestrian walkways are allowed within perimeter and interior landscape buffer if the landscape buffer is at least 2 ft wider than required in Subsections 19.606.2.C.1 and 19.606.2.D.3.b.

**Applicant's Facts and Findings:** The Applicant has prepared only a conceptual landscape plan for the property at this time. The Applicant is willing to accept conditions of approval requiring the final construction plans to conform to the requirements of the City's

landscape standards for parking areas and interior landscaping prior to the issuance of building permits. The requirements of this section can be met through a condition of approval.

### 19.606.3 Additional Design Standards

#### A. Paving and Striping

**Paving and striping are required for all required maneuvering and standing areas. Off-street parking areas shall have a durable and dust-free hard surface, shall be maintained for all-weather use, and shall be striped to show delineation of parking spaces and directional markings for driveways and accessways. Permeable paving surfaces may be used to reduce surface water runoff and protect water quality.**

**Applicant's Facts and Findings:** The proposed parking areas will be striped with asphalt surfaces and are striped to show all parking spaces and directional marking. No permeable asphalt has been proposed. The requirements of this section have been met.

#### B. Wheel Stops

**Parking bumpers or wheel stops, of a minimum 4-in height, shall be provided at parking spaces to prevent vehicles from encroaching on the street right-of-way, adjacent landscaped areas, or pedestrian walkways. Curbing may substitute for wheel stops if vehicles will not encroach into the minimum required width for landscape or pedestrian areas.**

**Applicant's Facts and Findings:** Curb stops will be provided on all proposed parking stalls. The requirements of this section have been met.

#### C. Site Access and Drive Aisles

- 1. Accessways to parking areas shall be the minimum number necessary to provide access while not inhibiting the safe circulation and carrying capacity of the street. Driveway approaches shall comply with the access spacing standards of Chapter 12.16.**
- 2. Drive aisles shall meet the dimensional requirements in Subsection 19.606.1.**
- 3. Parking drive aisles shall align with the approved driveway access and shall not be wider than the approved driveway access within 10 ft of the right-of-way boundary.**
- 4. Along collector and arterial streets, no parking space shall be located such that its maneuvering area is in an ingress or egress aisle within 20 ft of the back of the sidewalk, or from the right-of-way boundary where no sidewalk exists.**
- 5. Driveways and on-site circulation shall be designed so that vehicles enter the right-of-way in a forward motion.**

**Applicant's Facts and Findings:** All proposed drive aisles have been designed for safe and efficient movement throughout the parking areas. All proposed drive aisles meet the minimum width requirements as specified within 19.606.1 and have been designed to allow for access onto the public street network with only forward motions. The requirements of this section have been met.

#### D. Pedestrian Access and Circulation



Subsection 19.504.9 establishes standards that are applicable to an entire property for on-site walkways and circulation. The purpose of Subsection 19.606.3.D is to provide safe and convenient pedestrian access routes specifically through off-street parking areas. Walkways required by Subsection 19.606.3.D are considered part of the on-site walkway and circulation system required by Subsection 19.504.9.

1. Pedestrian access shall be provided for off-street parking areas so that no parking space is further than 100 ft away, measured along vehicle drive aisles, from a building entrance, or a walkway that meets the standards of Subsection 19.606.3.D.2.
2. Walkways through off-street parking areas must be continuous, must lead to a building entrance, and meet the design standards of Subsection 19.504.9.E.

**Applicant's Facts and Findings:** All proposed drive aisles have been designed for safe and efficient movement throughout the parking areas. All proposed drive aisles meet the minimum width requirements as specified within 19.606.1 and have been designed to allow for access onto the public street network with only forward motions. The requirements of this section have been met.

#### E. Internal Circulation

##### 1. General Circulation

The Planning Director has the authority to review the pedestrian, bicycle, and vehicular circulation of the site and impose conditions to ensure safe and efficient on-site circulation. Such conditions may include, but are not limited to, on-site signage, pavement markings, addition or modification of curbs, and modifying drive aisle dimensions.

**Applicant's Facts and Findings:** The Applicant acknowledges that the planning director may review the proposed circulation systems on site and may impose conditions of approval to ensure that safe and efficient circulation is provided.

#### F. Lighting

Lighting is required for parking areas with more than 10 spaces. The Planning Director may require lighting for parking areas of less than 10 spaces if the parking area would not be safe due to the lack of lighting. Lighting shall be designed to enhance safe access for vehicles and pedestrians on the site, and shall meet the following standards:

1. Lighting luminaires shall have a cutoff angle of 90 degrees or greater to ensure that lighting is directed toward the parking surface.
2. Parking area lighting shall not cause a light trespass of more than 0.5 footcandles measured vertically at the boundaries of the site.
3. Pedestrian walkways and bicycle parking areas in off-street parking areas shall have a minimum illumination level of 0.5 footcandles, measured horizontally at the ground level.

4. **Where practicable, lights shall be placed so they do not shine directly into any WQR and/or HCA location. The type, size, and intensity of lighting shall be selected so that impacts to habitat functions are minimized.**

**Applicant's Facts and Findings:** The proposed development will provide lighting within parking lots and along the internal pedestrian walkway. The attached lighting plan shows the proposed lighting scheme for the parking lot will meet the requirements of this section by providing at least 0.5 foot candles of illumination.

#### **19.608 LOADING**

##### **19.608.2. Number of Loading Spaces**

**The Planning Director shall determine whether to require off-street loading for commercial, industrial, public, and semipublic uses. The ratios listed below should be the minimum required unless the Planning Director finds that a different number of loading spaces are needed upon reviewing the loading needs of a proposed use.**

**Applicant's Facts and Findings:** The proposed development will not impact the existing loading zones on site. There are two loading zones located adjacent to the high school. The requirements of this section have been met.

#### **19.609 BICYCLE PARKING**

##### **19.609.1 Applicability**

**Bicycle parking shall be provided for all new commercial, industrial, community service use, and multifamily residential development. Temporary and seasonal uses (e.g., fireworks and Christmas tree stands) and storage units are exempt from Section 19.609. Bicycle parking shall be provided in the Downtown Mixed Use Zone and at transit centers.**

##### **19.609.2 Quantity of Spaces**

**A. The quantity of required bicycle parking spaces shall be as described in this subsection. In no case shall less than 2 spaces be provided.**

1. **Unless otherwise specified, the number of bicycle parking spaces shall be at least 10% of the minimum required vehicle parking for the use.**
2. **The number of bicycle parking spaces at transit centers shall be provided at the ratio of at least 1 space per 100 daily boardings.**
3. **Multifamily residential development with 4 or more units shall provide 1 space per unit.**

**Applicant's Facts and Findings:** In 2009, the District installed a total of 54 covered bicycle parking stalls on site. The minimum number of bicycles stalls required is 41 spaces, based upon a minimum required parking requirement of 410 spaces. The site already meets this standard.

#### **19.610 CARPOOL AND VANPOOL PARKING**

##### **19.610.1 Applicability**

**New industrial, institutional, and commercial development with 20 or more required parking spaces shall provide carpool/vanpool parking.**

**Applicant's Facts and Findings:** The District has proposed to construct more than 20 parking spaces therefore the provisions of this section apply.

#### 19.610.2 Number of Spaces

**The number of carpool/vanpool parking spaces shall be at least 10% of the minimum amount of required parking spaces. The minimum amount of required parking spaces shall take into account the reduction allowed by Subsection 19.605.3.B.4.**

**Applicant's Facts and Findings:** The District has proposed to construct 30 parking spaces therefore the provisions of this section apply. The proposed parking lot will include 3 carpool spaces. The requirements of this section have been met.

#### 19.610.3 Location

**Parking for carpools/vanpools shall be located closer to the main entrances of the building than other employee or student parking, except ADA spaces.**

#### 19.610.4 Standards

**Carpool/vanpool spaces shall be clearly designated with signs or pavement markings for use only by carpools/vanpools.**

**Applicant's Facts and Findings:** The proposed parking lot is located across SE Willard Street from the main school building. The carpool parking has been provided along a main pedestrian pathway which will connect to the main school site.

The requirements of this section have been met.

### CHAPTER 19.700 PUBLIC FACILITY IMPROVEMENTS

#### 19.701.2 For Public Facilities

- A. Ensure that public facility improvements are safe, convenient, and adequate.**
- B. Ensure that public facility improvements are designed and constructed to City standards in a timely manner.**
- C. Ensure that the expenditure of public monies for public facility improvements is minimized when improvements are needed for private development.**
- D. Ensure that public facility improvements meet the City of Milwaukie Comprehensive Plan goals and policies.**

**Applicant's Facts and Findings:** The City's Public Facilities approval criteria apply to this application as new construction is proposed.

#### 19.702 APPLICABILITY

##### 19.702.1 General

**Chapter 19.700 applies to the following types of development in all zones:**

- A. Partitions.**
- B. Subdivisions.**
- C. Replats.**
- D. New construction.**

**E. Modification or expansion of an existing structure or a change or intensification in use that results in any one of the following. See Subsections 19.702.2-3 for specific applicability provisions for single-family residential development and development in downtown zones.**

- 1. A new dwelling unit.**
- 2. Any increase in gross floor area.**
- 3. Any projected increase in vehicle trips, as determined by the Engineering Director.**

**Applicant's Facts and Findings:** The City's Public Facilities approval criteria apply to this application as new construction is proposed.

### **19.703 REVIEW PROCESS**

#### **19.703.1 Preapplication Conference**

**For all proposed development that requires a land use application and is subject to Chapter 19.700 per Section 19.702, the applicant shall schedule a preapplication conference with the City prior to submittal of the land use application. The Engineering Director may waive this requirement for proposals that are not complex.**

#### **19.703.2 Application Submittal**

**For all proposed development that is subject to Chapter 19.700 per Section 19.702, one of the following types of applications is required.**

**A. Development Permit Application**

**If the proposed development does not require a land use application, compliance with Chapter 19.700 will be reviewed as part of the development permit application submittal.**

**B. Transportation Facilities Review (TFR) Land Use Application**

**If the proposed development triggers a transportation impact study (TIS) per Section 19.704, a TFR land use application shall be required. Compliance with Chapter 19.700 will be reviewed as part of the TFR application submittal and will be subject to a Type II review process as set forth in Section 19.1005. The TFR application shall be consolidated with, and processed concurrently with, any other required land use applications.**

**If the proposed development does not trigger a TIS per Section 19.704, but does require the submittal of other land use applications, compliance with Chapter 19.700 will be reviewed during the review of the other land use applications.**

**Applicant's Facts and Findings:** The Applicant has not been required to provide a transportation Impact Statement however other land use applications have been proposed. The City will therefore review the proposed improvements to the public facility improvements as part of this application package.

#### **19.703.3 Approval Criteria**

For all proposed development that is subject to Chapter 19.700 per Section 19.702, the required development permit and/or land use application shall demonstrate compliance with the following approval criteria at the time of submittal.

**A. Procedures, Requirements, and Standards**

**Development and related public facility improvements shall comply with procedures, requirements, and standards of Chapter 19.700 and the Public Works Standards.**

**Applicant's Facts and Findings:** All proposed improvements to the City's public facilities included within this application have been designed to meet the requirements of section 19.700 and the City's public works standards. All improvements will be installed in accordance with the City's procedures and requirements. The requirements of this section for preliminary design are met and the installation standards can be met through the imposition of a condition of approval.

**B. Transportation Facility Improvements**

**Development shall provide transportation improvements and mitigation at the time of development in rough proportion to the potential impacts of the development per Section 19.705 Rough Proportionality, except as allowed by Chapter 13.32 Fee in Lieu of Construction.**

**Development in downtown zones that is exempt per Subsection 19.702.3.B shall only be required to provide transportation improvements that are identified by a Transportation Impact Study as necessary to mitigate the development's transportation impacts. Such development is not required to provide on-site frontage improvements.**

**Applicant's Facts and Findings:** The proposed development has not triggered the need for a transportation impact statement because the proposed use of the campus will not change as a result of the proposed development. As such, no transportation improvements beyond the frontage improvements proposed along the site's frontages have been proposed.

**C. Safety and Functionality Standards**

**The City will not issue any development permits unless the proposed development complies with the City's basic safety and functionality standards, the purpose of which is to ensure that development does not occur in areas where the surrounding public facilities are inadequate. Upon submittal of a development permit application, an applicant shall demonstrate that the development property has or will have all of the following:**

- 1. Adequate street drainage, as determined by the Engineering Director.**
- 2. Safe access and clear vision at intersections, as determined by the Engineering Director.**
- 3. Adequate public utilities, as determined by the Engineering Director.**
- 4. Access onto a public street with the minimum paved widths as stated in Subsection 19.703.3.C.5 below.**
- 5. Adequate frontage improvements as follows:**

- a. For local streets, a minimum paved width of 16 ft along the site's frontage.
  - b. For nonlocal streets, a minimum paved width of 20 ft along the site's frontage.
  - c. For all streets, a minimum horizontal right-of-way clearance of 20 ft along the site's frontage.
6. Compliance with Level of Service D for all intersections impacted by the development, except those on Oregon Highway 99E that shall be subject to the following:
- a. Level of Service F for the first hour of the morning or evening 2-hour peak period.
  - b. Level of Service E for the second hour of the morning or evening 2-hour peak period.

**Applicant's Facts and Findings:** The district's proposed improvements are consistent with the City's requirements for safety and functionality. The site has been designed with consideration for safe access and clear vision at intersections. As described in the responses to 19.703.4, below, all minimum requirements for the site's surrounding street network have been satisfied through the proposed improvement plans. All required utilities will be installed as required by the City's Engineering Director.

#### 19.703.4 Determinations

There are four key determinations related to transportation facility improvements that occur during the processing of a development permit or land use application. These determinations are described below in the order in which they occur in the review process. They are also shown in Figure 19.703.4. In making these determinations, the Engineering Director will take the goals and policies of the TSP into consideration and use the criteria and guidelines in this chapter.

##### A. Impact Evaluation

For development that is subject to Chapter 19.700 per Subsection 19.702.1, the Engineering Director will determine whether the proposed development has impacts to the transportation system pursuant to Section 19.704. Pursuant to Subsection 19.704.1, the Engineering Director will also determine whether a transportation impact study (TIS) is required. If a TIS is required, a transportation facilities review land use application shall be submitted pursuant to Subsection 19.703.2.B.

For development that is subject to Chapter 19.700 per Subsection 19.702.2, the City has determined that there are impacts to the transportation system if the proposed single-family residential expansion/conversion is greater than 200 sq ft.

**Applicant's Facts and Findings:** The Director indicated that because of the low potential for any unusual impacts associated with this development, no Traffic Impact Statement would be required in support of the proposed improvements.

##### B. Street Design

**Given the City's existing development pattern, it is expected that most transportation facility improvements will involve existing streets and/or will serve infill development. To ensure that required improvements are safe and relate to existing street and development conditions, the Engineering Director will determine the most appropriate street design cross section using the standards and guidelines contained in Section 19.708. On-site frontage improvements are not required for downtown development that is exempt per Subsection 19.702.3.B.**

**Applicant's Facts and Findings:** The site has frontage along SE Willard and SE 25<sup>th</sup> Avenue. Frontage improvements to SE Willard were completed under the previous CSU Modification (CSU-2017-007). SE 25<sup>th</sup> Avenue is classified as a local street within the City's TSP. Local Streets are intended to be between 20 and 68 feet wide with shared travel zones, on street parking, green space and sidewalks. The existing right-of-way width is 50 feet. The applicant is proposing to construct full right-of-way improvements, including a 20-foot-wide travel zone, on street parking on one side, a curb-tight sidewalk on one side, and a sidewalk and green space on one side. New driveways will be provided for neighboring properties.

The requirements of this section have been met.

### **C. Proportional Improvements**

**When transportation facility improvements are required pursuant to this chapter, the Engineering Director will conduct a proportionality analysis pursuant to Section 19.705 to determine the level of improvements that are roughly proportional to the level of potential impacts from the proposed development. Guidelines for conducting a proportionality analysis are contained in Subsection 19.705.2.**

**Applicant's Facts and Findings:** All required improvements to the streets surrounding the property are being completed as part of the proposed development. No proportionate share or additional improvements are required in order to mitigate any potential impacts from the project.

### **D. Fee in Lieu of Construction (FILOC)**

**If transportation facility improvements are required and determined to be proportional, the City will require construction of the improvements at the time of development. However, the applicant may request to pay a fee in lieu of constructing the required transportation facility improvements. The Engineering Director will approve or deny such requests using the criteria for making FILOC determinations found in Chapter 13.32 Fee in Lieu of Construction.**

**Applicant's Facts and Findings:** The applicant has not proposed to provide any fees in lieu of site construction.

**19.704 TRANSPORTATION IMPACT EVALUATION**

The Engineering Director will determine whether a proposed development has impacts on the transportation system by using existing transportation data. If the Engineering Director cannot properly evaluate a proposed development’s impacts without a more detailed study, a transportation impact study (TIS) will be required to evaluate the adequacy of the transportation system to serve the proposed development and determine proportionate mitigation of impacts. The TIS determination process and requirements are detailed below.

**19.704.1 TIS Determination**

**A. Based on information provided by the applicant about the proposed development, the Engineering Director will determine when a TIS is required and will consider the following when making that determination.**

- 1. Changes in land use designation, zoning designation, or development standard.**
- 2. Changes in use or intensity of use.**
- 3. Projected increase in trip generation.**
- 4. Potential impacts to residential areas and local streets.**
- 5. Potential impacts to priority pedestrian and bicycle routes, including, but not limited to, school routes and multimodal street improvements identified in the TSP.**
- 6. Potential impacts to intersection level of service (LOS).**

**B. It is the responsibility of the applicant to provide enough detailed information for the Engineering Director to make a TIS determination.**

**C. A TIS determination is not a land use action and may not be appealed.**

**Applicant’s Facts and Findings:** The Director indicated that because of the low potential for any unusual impacts associated with this development, no Traffic Impact Statement would be required in support of the proposed improvements.

**19.705 ROUGH PROPORTIONALITY**

The purpose of this section is to ensure that required transportation facility improvements are roughly proportional to the potential impacts of the proposed development. The rough proportionality requirements of this section apply to both frontage and off-site, or nonfrontage, improvements. A rough proportionality determination may be appealed pursuant to Subsection 19.703.5.

The Engineering Director will conduct a proportionality analysis for any proposed development that triggers transportation facility improvements per this chapter, with the exception of development subject to Subsection 19.702.2. The Engineering Director may conduct a proportionality analysis for development that triggers transportation facility improvements per Subsection 19.702.2.

When conducting a proportionality analysis for frontage improvements, the Engineering Director will not consider prior use for the portion of the proposed development that involves new construction. The Engineering Director will, however, consider any benefits that are estimated to accrue to the development property as a result of any required transportation facility improvements.



The following general provisions apply whenever a proportionality analysis is conducted.

#### 19.705.1 Impact Mitigation

Mitigation of impacts, due to increased demand for transportation facilities associated with the proposed development, shall be provided in rough proportion to the transportation impacts of the proposed development. When a TIS is required, potential impacts will be determined in accordance with Section 19.704. When no TIS is required, potential impacts will be determined by the Engineering Director.

**Applicant's Facts and Findings:** The Director indicated that because of the low potential for any unusual impacts associated with this development, no Traffic Impact Statement would be required in support of the proposed improvements.

#### 19.705.2 Rough Proportionality Guidelines

The following shall be considered when determining proportional improvements:

- A. Condition and capacity of existing facilities within the impact area in relation to City standards. The impact area is generally defined as the area within a 1/2-mile radius of the proposed development. If a TIS is required pursuant to Section 19.704, the impact area is the TIS study area.
- B. Existing vehicle, bicycle, pedestrian, and transit use within the impact area.
- C. The effect of increased demand associated with the proposed development on transportation facilities and on other approved, but not yet constructed, development projects within the impact area.
- D. The most recent use when a change in use is proposed that does not involve new construction.
- E. Applicable TSP goals, policies, and plans.
- F. Whether any route affected by increased transportation demand within the impact area is listed in any City program including, but not limited to, school trip safety, neighborhood traffic management, capital improvement, and system development improvement.
- G. Accident history within the impact area.
- H. Potential increased safety risks to transportation facility users, including pedestrians and cyclists.
- I. Potential benefit the development property will receive as a result of the construction of any required transportation facility improvements.
- J. Other considerations as may be identified in the review process.

**Applicant's Facts and Findings:** The Director indicated that because of the low potential for any unusual impacts associated with this development, no Traffic Impact Statement would be required in support of the proposed improvements.

## **19.708 TRANSPORTATION FACILITY REQUIREMENTS**

This section contains the City's requirements and standards for improvements to public streets, including pedestrian, bicycle, and transit facilities. For ease of reading, the more common term "street" is used more frequently than the more technical terms "public right-of-way" or "right-of-way." As used in this section, however, all three terms have the same meaning.

The City recognizes the importance of balancing the need for improved transportation facilities with the need to ensure that required improvements are fair and proportional. The City also acknowledges the value in providing street design standards that are both objective and flexible. Objective standards allow for consistency of design and provide some measure of certainty for developers and property owners. Flexibility, on the other hand, gives the City the ability to design streets that are safe and that respond to existing street and development conditions in a way that preserves neighborhood character.

The City's street design standards are based on the street classification system described in the TSP. Figure 8-1 of the TSP identifies the functional street classification for every street in the City and Figure 10-1 identifies the type and size of street elements that may be appropriate for any given street based on its classification.

### **19.708.1 General Street Requirements and Standards**

#### **A. Access Management**

All development subject to Chapter 19.700 shall comply with access management standards contained in Chapter 12.16.

#### **B. Clear Vision**

All development subject to Chapter 19.700 shall comply with clear vision standards contained in Chapter 12.24.

#### **C. Development in Downtown Zones**

Street design standards and right-of-way dedication for the downtown zones are subject to the requirements of the Milwaukie Public Works Standards, which implement the streetscape design of the Milwaukie Downtown and Riverfront Plan: Public Area Requirements (PAR). Unless specifically stated otherwise, the standards in Section 19.708 do not apply to development located in the downtown zones or on street sections shown in the PAR per Subsection 19.304.6.

#### **D. Development in Non-Downtown Zones**

Development in a non-downtown zone that has frontage on a street section shown in the PAR is subject to the requirements of the Milwaukie Public Works Standards, which implements the street design standards and right-of-way dedication requirements contained in the PAR for that street frontage. The following general provisions apply only to street frontages that are

not shown in the PAR and for development that is not in any of the downtown zones listed in Subsection 19.708.1.C above:

1. Streets shall be designed and improved in accordance with the standards of this chapter and the Public Works Standards. ODOT facilities shall be designed consistent with State and federal standards. County facilities shall be designed consistent with County standards.
2. Streets shall be designed according to their functional classification per Figure 8-3b of the TSP.
3. Street right-of-way shall be dedicated to the public for street purposes in accordance with Subsection 19.708.2. Right-of-way shall be dedicated at the corners of street intersections to accommodate the required turning radii and transportation facilities in accordance with Section 19.708 and the Public Works Standards. Additional dedication may be required at intersections for improvements identified by the TSP or a required transportation impact study.
4. The City shall not approve any development permits for a proposed development unless it has frontage or approved access to a public street.
5. Off-site street improvements shall only be required to ensure adequate access to the proposed development and to mitigate for off-site impacts of the proposed development.
6. The following provisions apply to all new public streets and extensions to existing public streets.
  - a. All new streets shall be dedicated and improved in accordance with this chapter.
  - b. Dedication and construction of a half-street is generally not acceptable. However, a half-street may be approved where it is essential to allow reasonable development of a property and when the review authority finds that it will be possible for the property adjoining the half-street to dedicate and improve the remainder of the street when it develops. The minimum paved roadway width for a half-street shall be the minimum width necessary to accommodate 2 travel lanes pursuant to Subsection 19.708.2.
7. Traffic calming may be required for existing or new streets. Traffic calming devices shall be designed in accordance with the Public Works Standards or with the approval of the Engineering Director.

#### 8. Railroad Crossings

Where anticipated development impacts trigger a need to install or improve a railroad crossing, the cost for such improvements may be a condition of development approval.

#### 9. Street Signs

The City shall install all street signs, relative to traffic control and street names, as specified by the Engineering Director. The applicant shall reimburse the City for the cost of all such signs installed by the City.

#### 10. Streetlights

The location of streetlights shall be noted on approved development plans. Streetlights shall be installed in accordance with the Public Works Standards or with the approval of the Engineering Director.

**Applicant's Facts and Findings:** The Applicant has provided a detailed description of the roadway improvements proposed as part of this project within the findings created in response to section 19.703.4. The Applicant has also provided detailed preliminary development plans showing that all proposed improvements have been designed in accordance with the City's Transportation System Plan and public works standards.

The proposed parking lot will have an entrance on SE Willard Avenue, with an exit only access on SE 25<sup>th</sup> Avenue. Access spacing for the site's driveways on the southern side of the campus has been reviewed and refined through discussions with our transportation consultants and the City Engineer.

#### **E. Street Layout and Connectivity**

- 1. The length, width, and shape of blocks shall take lot size standards, access and circulation needs, traffic safety, and topographic limitations into consideration.**
- 2. The street network shall be generally rectilinear but may vary due to topography or other natural conditions.**
- 3. Streets shall be extended to the boundary lines of the developing property where necessary to give access to or allow for future development of adjoining properties.**
  - a. Temporary turnarounds shall be constructed for street stubs in excess of 150 ft in length. Drainage facilities shall be constructed to properly manage stormwater runoff from temporary turnarounds.**
  - b. Street stubs to adjoining properties shall not be considered turnarounds, unless required and designed as turnarounds, since they are intended to continue as through streets when adjoining properties develop.**
  - c. Reserve strips may be required in order to ensure the eventual continuation or completion of a street.**
- 4. Permanent turnarounds shall only be provided when no opportunity exists for creating a through street connection. The lack of present ownership or control over abutting property shall not be grounds for construction of a turnaround. For proposed land division sites that are 3 acres or larger, a street ending in a turnaround shall have a maximum length of 200 ft, as measured from the cross street right-of-way to the farthest point of right-of-way containing the turnaround. For proposed land division sites that are less than 3 acres, a street ending in a turnaround shall have a maximum length of 400 ft, measured from the cross street right-of-way to the farthest point of right-of-way containing the turnaround. Turnarounds shall be designed in accordance with the requirements of the Public Works Standards. The requirements of this subsection may be adjusted by the Engineering Director to avoid alignments that encourage nonlocal through traffic.**
- 5. Closed-end street systems may serve no more than 20 dwellings.**

**Applicant's Facts and Findings:** The Applicant is not proposing to create any new streets as part of this development. The applicant is proposing to construct full right-of-way improvements, including a 20-foot-wide travel zone, on street parking on one side, a curb-tight sidewalk on one side, and a sidewalk and green space on one side. A modified turn-around has been provided at the end of 25<sup>th</sup> Avenue.

#### **F. Intersection Design and Spacing**

- 1. Connecting street intersections shall be located to provide for traffic flow, safety, and turning movements, as conditions warrant.**
  - 2. Street and intersection alignments for local streets shall facilitate local circulation but avoid alignments that encourage nonlocal through traffic.**
  - 3. Streets should generally be aligned to intersect at right angles (90 degrees). Angles of less than 75 degrees will not be permitted unless the Engineering Director has approved a special intersection design.**
  - 4. New streets shall intersect at existing street intersections so that centerlines are not offset. Where existing streets adjacent to a proposed development do not align properly, conditions shall be imposed on the development to provide for proper alignment.**
- 5. Minimum and maximum block perimeter standards are provided in Table 19.708.1.**
- 6. Minimum and maximum intersection spacing standards are provided in Table 19.708.1.**

#### **19.708.2 Street Design Standards**

**Applicant's Facts and Findings:** The Applicant has provided a detailed description of the roadway improvements proposed as part of this project within the findings created in response to section 19.703.4. No new intersections between existing roadways are proposed.

**Table 19.708.2 contains the street design elements and dimensional standards for street cross sections by functional classification. Dimensions are shown as ranges to allow for flexibility in developing the most appropriate cross section for a given street or portion of street based on existing conditions and the surrounding development pattern. The additional street design standards in Subsection 19.708.2.A augment the dimensional standards contained in Table 19.708.2. The Engineering Director will rely on Table 19.708.2 and Subsection 19.708.2.A to determine the full-width cross section for a specific street segment based on functional classification. The full-width cross section is the sum total of the widest dimension of all individual street elements. If the Engineering Director determines that a full-width cross section is appropriate and feasible, a full-width cross section will be required. If the Engineering Director determines that a full-width cross section is not appropriate or feasible, the Engineering Director will modify the full-width cross section requirement using the guidelines provided in Subsection 19.708.2.B. Standards for design speed, horizontal/vertical curves, grades, and curb return radii are specified in the Public Works Standards.**

#### **A. Additional Street Design Standards**

These standards augment the dimensional standards contained in Table 19.708.2 and may increase the width of an individual street element and/or the full-width right-of-way dimension.

1. Minimum 10-ft travel lane width shall be provided on local streets with no on-street parking.
2. Where travel lanes are next to a curb line, an additional 1 ft of travel lane width shall be provided. Where a travel lane is located between curbs, an additional 2 ft of travel lane width shall be provided.
3. Where shared lanes or bicycle boulevards are planned, up to an additional 6 ft of travel lane width shall be provided.
4. Bike lane widths may be reduced to a minimum of 4 ft where unusual circumstances exist, as determined by the Engineering Director, and where such a reduction would not result in a safety hazard.
5. Where a curb is required by the Engineering Director, it shall be designed in accordance with the Public Works Standards.
6. Center turn lanes are not required for truck and bus routes on street classifications other than arterial roads.
7. On-street parking in industrial zones shall have a minimum width of 8 ft.
8. On-street parking in commercial zones shall have a minimum width of 7 ft.
9. On-street parking in residential zones shall have a minimum width of 6 ft.
10. Sidewalk widths may be reduced to a minimum of 4 ft for short distances for the purpose of avoiding obstacles within the public right-of-way including, but not limited to, trees and power poles.
11. Landscape strip widths shall be measured from back of curb to front of sidewalk.
12. Where landscape strips are required, street trees shall be provided a minimum of every 40 ft in accordance with the Public Works Standards and the Milwaukie Street Tree List and Street Tree Planting Guidelines.
13. Where water quality treatment is provided within the public right-of-way, the landscape strip width may be increased to accommodate the required treatment area.
14. A minimum of 6 in shall be required between a property line and the street element that abuts it; e.g., sidewalk or landscape strip.

**B. Street Design Determination Guidelines**

The Engineering Director shall make the final determination regarding right-of-way and street element widths using the ranges provided in Table 19.708.2 and the additional street design standards in Subsection 19.708.2.A. The Engineering Director shall also determine whether any individual street element may be eliminated on one or both sides of the street in accordance with Figure 10-1 of the TSP. When making a street design determination that varies from the full-width cross section, the Engineering Director shall consider the following:

1. Options and/or needs for environmentally beneficial and/or green street designs.
2. Multimodal street improvements identified in the TSP.

3. **Street design alternative preferences identified in Chapter 10 of the TSP, specifically regarding sidewalk and landscape strip improvements.**
4. **Existing development pattern and proximity of existing structures to the right-of-way.**
5. **Existing right-of-way dimensions and topography.**

**Applicant's Facts and Findings:** The Applicant has provided a detailed description of the roadway improvements proposed as part of this project within the findings created in response to section 19.703.4. The Applicant has also provided detailed preliminary development plans showing that all proposed improvements have been designed in accordance with the City's Transportation System Plan and public works standards.

### **19.708.3 Sidewalk Requirements and Standards**

#### **A. General Provisions**

1. **Goals, objectives, and policies relating to walking are included in Chapter 5 of the TSP and provide the context for needed pedestrian improvements. Figure 5-1 of the TSP illustrates the Pedestrian Master Plan and Table 5-3 contains the Pedestrian Action Plan.**
2. **Americans with Disabilities Act (ADA) requirements for public sidewalks shall apply where there is a conflict with City standards.**

**Applicant's Facts and Findings:** The Applicant has provided a detailed description of the roadway improvements proposed as part of this project within the findings created in response to section 19.703.4. The Applicant has also provided detailed preliminary development plans showing that all proposed improvements have been designed in accordance with the City's Transportation System Plan and public works standards.

#### **B. Sidewalk Requirements**

##### **1. Requirements**

**Sidewalks shall be provided on the public street frontage of all development per the requirements of this chapter. Sidewalks shall generally be constructed within the dedicated public right-of-way, but may be located outside of the right-of-way within a public easement with the approval of the Engineering Director.**

##### **2. Design Standards**

**Sidewalks shall be designed and improved in accordance with the requirements of this chapter and the Public Works Standards.**

##### **3. Maintenance**

**Abutting property owners shall be responsible for maintaining sidewalks and landscape strips in accordance with Chapter 12.04.**

**Applicant's Facts and Findings:** The Applicant has provided a detailed description of the roadway improvements proposed as part of this project within the findings created in response to section 19.703.4. The Applicant has also provided detailed preliminary development plans showing that all proposed improvements have been designed in accordance with the City's Transportation System Plan and public works standards.

#### **19.708.4 Bicycle Facility Requirements and Standards**

##### **A. General Provisions**

- 1. Bicycle facilities include bicycle parking and on-street and off-street bike lanes, shared lanes, bike boulevards, and bike paths.**
- 2. Goals, objectives, and policies relating to bicycling are included in Chapter 6 of the TSP and provide the context for needed bicycle improvements. Figure 6-2 of the TSP illustrates the Bicycle Master Plan, and Table 6-3 contains the Bicycle Action Plan.**

##### **B. Bicycle Facility Requirements**

###### **1. Requirements**

**Bicycle facilities shall be provided in accordance with this chapter, Chapter 19.600, the TSP, and the Milwaukie Downtown and Riverfront Plan: Public Area Requirements. Requirements include, but are not limited to, parking, signage, pavement markings, intersection treatments, traffic calming, and traffic diversion.**

###### **2. Timing of Construction**

**To assure continuity and safety, required bicycle facilities shall generally be constructed at the time of development. If not practical to sign, stripe, or construct bicycle facilities at the time of development due to the absence of adjacent facilities, the development shall provide the paved street width necessary to accommodate the required bicycle facilities**

###### **3. Design Standards**

**Bicycle facilities shall be designed and improved in accordance with the requirements of this chapter and the Public Works Standards. Bicycle parking shall be designed and improved in accordance with Chapter 19.600 and the Milwaukie Downtown and Riverfront Plan: Public Area Requirements.**

**Applicant's Facts and Findings:** The Applicant has provided a detailed description of the roadway improvements proposed as part of this project within the findings created in response to section 19.703.4. The Applicant has also provided detailed preliminary development plans showing that all proposed improvements have been designed in accordance with the City's Transportation System Plan and public works standards.

#### **19.708.5 Pedestrian/Bicycle Path Requirements and Standards**

**Applicant's Facts and Findings:** No new bicycle pathways have been required or proposed within the proposed development. The requirements of this section do not apply.

#### **19.708.6 Transit Requirements and Standards**

##### **A. General Provisions**

- 1. Transit facilities include bus stops, shelters, and related facilities. Required transit facility improvements may include the dedication of land or the provision of a public easement.**



**2. Goals, objectives, and policies relating to transit are included in Chapter 7 of the TSP. Figure 7-3 of the TSP illustrates the Transit Master Plan, and Table 7-2 contains the Transit Action Plan.**

**Applicant's Facts and Findings:** No new public transit facilities have been required by the City as part of this project. The requirements of this section do not apply as no new public transit facilities are proposed.

## **19.709 PUBLIC UTILITY REQUIREMENTS**

### **19.709.1 Review Process**

The Engineering Director shall review all proposed development subject to Chapter 19.700 per Section 19.702 in order to: (1) evaluate the adequacy of existing public utilities to serve the proposed development, and (2) determine whether new public utilities or an expansion of existing public utilities is warranted to ensure compliance with the City's public utility requirements and standards.

#### **A. Permit Review**

The Engineering Director shall make every effort to review all development permit applications for compliance with the City's public utility requirements and standards within 10 working days of application submittal. Upon completion of this review, the Engineering Director shall either approve the application, request additional information, or impose conditions on the application to ensure compliance with this chapter.

#### **B. Review Standards**

Review standards for public utilities shall be those standards currently in effect, or as modified, and identified in such public documents as Milwaukie's Comprehensive Plan, Wastewater Master Plan, Water Master Plan, Stormwater Master Plan, Transportation System Plan, and Public Works Standards.

**Applicant's Facts and Findings:** The Applicant has provided a Storm Drainage Plan (Sheet C605) under Appendix E. Stormwater will be treated onsite with Contech Stormfilter water quality manholes prior to entering a subsurface infiltration system. Sewer and water utility improvements are not proposed as a part of this development.

### **19.709.2 Public Utility Improvements**

Public utility improvements shall be required for proposed development that would have a detrimental effect on existing public utilities, cause capacity problems for existing public utilities, or fail to meet standards in the Public Works Standards. Development shall be required to complete or otherwise provide for the completion of the required improvements.

**A.** The Engineering Director shall determine which, if any, utility improvements are required. The Engineering Director's determination requiring utility improvements shall be based upon an analysis that shows the proposed development will result in one or more of the following situations:

- 1. Exceeds the design capacity of the utility.**
- 2. Exceeds Public Works Standards or other generally accepted standards.**
- 3. Creates a potential safety hazard.**
- 4. Creates an ongoing maintenance problem**

**B. The Engineering Director may approve one of the following to ensure completion of required utility improvements.**

**1. Formation of a reimbursement district in accordance with Chapter 13.30 for off-site public facility improvements fronting other properties.**

**2. Formation of a local improvement district in accordance with Chapter 3.08 for off-site public facility improvements fronting other properties.**

**Applicant's Facts and Findings:** The Applicant has provided a Storm Drainage Plan (Sheet C605) under Appendix E. Stormwater will be treated onsite with Contech Stormfilter water quality manholes prior to entering a subsurface infiltration system. Sewer and water utility improvements are not proposed as a part of this development.

### **19.709.3 Design Standards**

**Public utility improvements shall be designed and improved in accordance with the requirements of this chapter, the Public Works Standards, and improvement standards and specifications identified by the City during the development review process. The applicant shall provide engineered utility plans to the Engineering Director for review and approval prior to construction to demonstrate compliance with all City standards and requirements.**

**Applicant's Facts and Findings:** The proposed stormwater utility improvements have been designed in accordance with the requirements of this chapter. The attached plans have been submitted to the City's Engineering Director for review. No construction on site will be scheduled without the required approval and permits.

### **19.709.4 Oversizing**

**The Engineering Director may require utility oversizing in anticipation of additional system demand. If oversizing is required, the Engineering Director may authorize a reimbursement district or a system development charge (SDC) credit in accordance with Chapter 13.28.**

**Applicant's Facts and Findings:** The Applicant has proposed improvements to the site's utility network to satisfy the demands associated with the new parking lot. The City's Engineering Director has not indicated that any specific oversizing of the system will be required within the site's vicinity.

### **19.709.5 Monitoring**

**The Engineering Director shall monitor the progress of all public utility improvements by the applicant to ensure project completion and compliance with all City permitting requirements and standards. Utility improvements are subject to the requirements of Chapter 12.08. Follow-up action, such as facility inspection, bond release, and enforcement, shall be considered a part of the monitoring process.**

**Applicant's Facts and Findings:** The Applicant will work with the City's Engineering Director throughout the construction process to ensure that all proposed improvements are completed to the satisfaction of the Director.

## CHAPTER 19.900 LAND USE APPLICATIONS

### 19.904 COMMUNITY SERVICE USES

#### 19.904.2 Applicability

Any community service use shall be subject to the provisions of this section. Application must be submitted to establish or modify a community service use. Community service uses include certain private and public utilities, institutions, and recreational facilities as listed below:

##### A. Institutions—Public/Private and Other Public Facilities

1. Schools, public or private, and their accompanying sports facilities, day-care centers, private kindergartens;
2. Government office buildings for local, state, or federal government such as a City hall, courthouse, police station, or other similar buildings;
3. Hospital;
4. Cemetery;
5. Nursing or convalescent home;
6. Religious institutions;
7. Community meeting building;
8. Temporary or transitional facility;
9. Other similar uses as determined by the Planning Commission.

**Applicant's Facts and Findings:** Schools are an institutional use within the City of Milwaukie's Zoning Code. The provisions of this section apply to the project because the Applicant has proposed an amendment to the School's existing Community Service Use Permit.

#### 19.904.3 Review Process

Except as provided in Subsections 19.904.5.C for minor modifications and 19.904.11 for wireless communication facilities, community service uses shall be evaluated through a Type III review per Section 19.1006.

**Applicant's Facts and Findings:** The Applicant acknowledges the required Type III review process.

#### 19.904.4 Approval Criteria

An application for a community service use may be allowed if the following criteria are met:

- A. The building setback, height limitation, and off-street parking and similar requirements governing the size and location of development in the underlying zone are met. Where a specific standard is not proposed in the CSU, the standards of the underlying zone are met;

**Applicant's Facts and Findings:** The Applicant had analyzed the allowable building setbacks, height limitations, off-street parking requirements and other applicable

development standards. The Applicant's responses to sections 19.300 (Base Zones), 19.400 (Overlay Zones and Special Areas), 19.500 (Supplementary Development Regulations), 19.600 (Off-street Parking), and 19.700 (Public Facilities) of the City's code confirm that the District's proposal is in compliance with all applicable underlying development standards and limitations.

**B. Specific standards for the proposed uses as found in Subsections 19.904.7-11 are met;**

**Applicant's Facts and Findings:** The standards of 19.904.7 apply to Schools. The Applicant has addressed these standards within this narrative.

**C. The hours and levels of operation of the proposed use are reasonably compatible with surrounding uses;**

**Applicant's Facts and Findings:** The use of the site as a High School is not proposed to change. The hours and levels of operation are anticipated to be very similar to those in place today, which are reasonable compatible with the surrounding uses.

**D. The public benefits of the proposed use are greater than the negative impacts, if any, on the neighborhood; and**

**Applicant's Facts and Findings:** The Main Milwaukie High School building has served the residents of the City of Milwaukie and the North Clackamas School District for several generations. The relocation of the tennis courts to Rowe Middle School and replacement with the proposed parking lot will allow for more needed parking on the main high school campus. This will reduce the need for students and staff to park off-site, reducing congestion and pedestrian movements within the surrounding neighborhood.

**E. The location is appropriate for the type of use proposed.**

**Applicant's Facts and Findings:** The District's proposed placement of the parking lot adjacent to the school will provide needed parking in close proximity to the school, reducing the need for students and staff to park off-site.

**19.904.5 Procedures for Reviewing a Community Service Use**

**A. The Planning Commission will hold a public hearing on the establishment of, or major modification of, the proposed community service use. If the Commission finds that the approval criteria in Subsection 19.904.4 are met, the Commission shall approve the designation of the site for community service use. If the Commission finds otherwise, the application shall be denied. An approval allows the use on the specific property for which the application was submitted, subject to any conditions the Planning Commission may attach.**

**B. In permitting a community service use or the modification of an existing one, the City may impose suitable conditions which assure compatibility of the use with other uses in the vicinity. These conditions may include but are not limited to:**

- 1. Limiting the manner in which the use is conducted by restricting the time an activity may take place and by minimizing such environmental effects as noise and glare;**
- 2. Establishing a special yard, setback, lot area, or other lot dimension;**
- 3. Limiting the height, size, or location of a building or other structure;**
- 4. Designating the size, number, location, and design of vehicle access points;**
- 5. Increasing roadway widths, requiring street dedication, and/or requiring improvements within the street right-of-way including full street improvements;**
- 6. Designating the size, location, screening, drainage, surfacing, or other improvement of a parking area or truck loading area; and/or**
- 7. Limiting or otherwise designating the number, size, location, height, and lighting of signs.**

**C. The Planning Director may approve minor modifications to an approved community service per Section 19.1004 Type I Review, provided that such modification:**

- 1. Does not increase the intensity of any use;**
- 2. Meets all requirements of the underlying zone relating to building size and location and off-street parking and the standards of Title 19;**
- 3. Does not result in deterioration or loss of any protected natural feature or open space, and does not negatively affect nearby properties;**
- 4. Does not alter or contravene any conditions specifically placed on the development by the Planning Commission or City Council; and**
- 5. Does not cause any public facility, including transportation, water, sewer and storm drainage, to fail to meet any applicable standards relating to adequacy of the public facility.**

**Applicant's Facts and Findings:**

The Applicant acknowledges the process for review of Type III Applications. The Applicant acknowledges that the City may place conditions of approval upon the application in order to assure compatibility with the uses which are present within the neighborhood. Though it is possible for the City to assign conditions of approval related to suitability, the site is already in use as an existing High School Campus. The District's proposal to construct a parking lot at the existing location of the high school site warrants very few conditions to ensure suitability as the school has been in operation for longer than many of the homes within the surrounding neighborhood have been there.

The City's Planning Commission and City Council can find that no special conditions of approval require implementation prior to permitting the development of the proposed building and site improvements.

#### **19.904.6 Application Requirements**

**An application for approval of a community service use shall include the following:**

- A. Name, address and telephone number of applicant and/or property owner;**
- B. Map number and/or subdivision block and lot;**

- C. Narrative concerning the proposed request;
- D. Copy of deed, or other document showing ownership or interest in property. If applicant is not the owner, the written authorization from the owner for the application shall be submitted;
- E. Vicinity map;
- F. Comprehensive plan and zoning designations;
- G. A map showing existing uses, structures, easements, and public utilities and showing proposed development, placement of lot lines, etc.;
- H. Detailed plans for the specific project;
- I. Any information required by other applicable provisions of local, state or federal law;
- J. Proof of payment of the applicable fees;
- K. Additional drawings, surveys or other material necessary to understand the proposed use may be required.

**Applicant's Facts and Findings:** The Applicant has provided each of the required submission materials to allow the City to consider the proposed improvements and the application.

**19.904.7 Specific Standards for Schools**

Public, private or parochial, elementary, secondary, preschool, nursery schools, kindergartens, and day-care centers are included.

- A. Public elementary or secondary schools shall provide the site area/pupil ratio required by state law. Other schools shall provide 1 acre of site area for each 75 pupils of capacity or for each 2½ classrooms, whichever is greater, except as provided in Subsection 19.904.7.B below.
- B. Preschools, nursery schools, day-care centers, or kindergartens shall provide a fenced, outdoor play area of at least 75 sq ft for each child of total capacity, or a greater amount if so required by state law. In facilities where groups of children are scheduled at different times for outdoor play, the total play area may be reduced proportionally based on the number of children playing out-of-doors at one time. However, the total play area may not be reduced by more than half. These uses must comply with the State Children's Services Division requirements as well as the City provisions.
- C. Walkways, both on and off the site, shall be provided as necessary for safe pedestrian access to schools subject to the requirements and standards of Chapter 19.700.
- D. Where Subsection 19.904.7.B is applicable, a sight-obscuring fence of 4 to 6 ft in height shall be provided to separate the play area from adjacent residential uses.
- E. Public facilities must be adequate to serve the facility.
- F. Safe loading and ingress and egress will be provided on and to the site.
- G. Off-street parking (including buses) shall be provided as per Chapter 19.600.
- H. Minimum setback requirements:  
 Front yard: 20 ft  
 Rear yard: 20 ft  
 Side yard: 20 ft

**Setbacks may be increased depending on the type and size of school in order to ensure adequate buffering between uses and safety for students.**

**I. Bicycle facilities are required which adequately serve the facility.**

**J. 15% of the total site is to be landscaped.**

**Applicant's Facts and Findings:** The proposed parking lot will provide walkway on and off site to provide necessary and safe pedestrian access to the school. The requirements of Chapter 19.700 have been addressed within this narrative.

The loading areas for the new main school building have been located on the main campus to the east of the new building. The proposed parking area will be consistent with the City's code and placed in order to minimize conflicts with pedestrians and other vehicular traffic. The proposed parking area addresses several existing safety concerns and should create a much safer environment for students, parents, and staff.

Off-street parking requirements for vehicles, busses, and bicycles has been addressed within the Applicant's responses to Chapter 19.600.

Approximately 77% of the school site is currently in landscape area and open space. The requirements of this section have been met.

## **SUMMARY AND CONCLUSION**

Based upon the materials submitted herein, the Applicant respectfully requests approval from the County's Planning Department of this application for a Type III Community Service Use Modification.



Ron Stewart  
*Assistant Superintendent  
Finance and Operations*

stewartro@nclack.k12.or.us  
12400 SE Freeman Way, Milwaukie, Oregon 97222  
503-353-6071

November 17, 2017

Mr. Brett Kelter, Associate Planner  
City of Milwaukie  
Community Development  
6101 SE Johnson Creek Boulevard  
Milwaukie, OR 97206

**RE: Authorization to Sign Land Use  
Applications on behalf of the District**

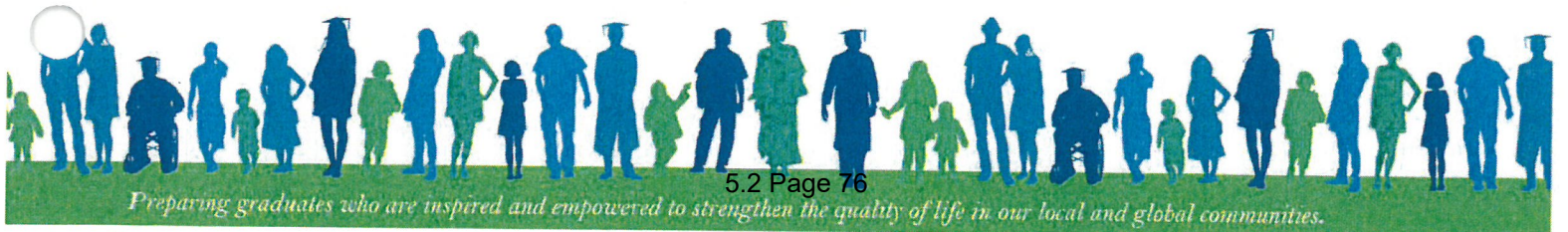
Dear Mr. Kelter,

This letter has been prepared in order to confirm that the North Clackamas School District has provided authorization for David Hobbs, the District's Capital Projects Director to provide all necessary signatures for the land use applications related to the District's capital improvement projects. Should you have any questions, please feel free to contact our offices at 503-353-6072.

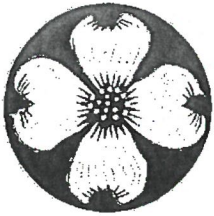
Sincerely,

A handwritten signature in blue ink that reads "Ron Stewart". The signature is stylized with a long, sweeping horizontal line extending to the right.

Ron Stewart  
*Assistant Superintendent  
Finance and Operations*







**PLANNING DEPARTMENT**  
 6101 SE Johnson Creek Blvd  
 Milwaukie OR 97206  
 503-786-7630  
 planning@milwaukieoregon.gov

# Application for Land Use Action

Master File #: CSU-2019-002

Review type\*:  I  II  III  IV  V

**CHOOSE APPLICATION TYPE(S):**

Community Service Use

...

...

...

...

RECEIVED  
 MAR 13 2019  
 CITY OF MILWAUKIE  
 PLANNING DEPARTMENT

- Use separate application forms for:
- Annexation and/or Boundary Change
  - Compensation for Reduction in Property Value (Measure 37)
  - Daily Display Sign
  - Appeal

**RESPONSIBLE PARTIES:**

**APPLICANT** (owner or other eligible applicant—see reverse): North Clackamas School District

Mailing address: 4444 SE Lake Road Milwaukie, OR Zip: 97222

Phone(s): 503-353-6058 Email: hobbsd@nclack.k12.or.us

**APPLICANT'S REPRESENTATIVE** (if different than above): Andrew Tull

Mailing address: 3J Consulting, Inc. 5075 SW Griffith Drive, Beaverton OR 97005 Zip: 97005

Phone(s): 503-545-1907 Email: andrew.tull@3j-consulting.com

**SITE INFORMATION:**

Address: (No Site Address - Willard and 25th) 2301 SE Willard St Map & Tax Lot(s): 11e36bc 5800


Comprehensive Plan Designation: C/HD Zoning: R-2 Size of property: 0.54 Acres

**PROPOSAL (describe briefly):**

Applicant requests a Type III CSU Modification to the approved MHS CSU permit to allow for the construction of a new parking lot.

**SIGNATURE:**

**ATTEST:** I am the property owner or I am eligible to initiate this application per Milwaukie Municipal Code (MMC) Subsection 19.1001.6.A. If required, I have attached written authorization to submit this application. To the best of my knowledge, the information provided within this application package is complete and accurate.

Submitted by:  Date: 02/27/2019

**IMPORTANT INFORMATION ON REVERSE SIDE**

**WHO IS ELIGIBLE TO SUBMIT A LAND USE APPLICATION** (excerpted from MMC Subsection 19.1001.6.A):

**Type I, II, III, and IV** applications may be initiated by the property owner or contract purchaser of the subject property, any person authorized in writing to represent the property owner or contract purchaser, and any agency that has statutory rights of eminent domain for projects they have the authority to construct.

**Type V** applications may be initiated by any individual.

**PREAPPLICATION CONFERENCE:**

A preapplication conference may be required or desirable prior to submitting this application. Please discuss with Planning staff.

**REVIEW TYPES:**

This application will be processed per the assigned review type, as described in the following sections of the Milwaukie Municipal Code:

- Type I: Section 19.1004
- Type II: Section 19.1005
- Type III: Section 19.1006
- Type IV: Section 19.1007
- Type V: Section 19.1008

**THIS SECTION FOR OFFICE USE ONLY:**

| FILE TYPE                    | FILE NUMBER  | FEE AMOUNT* | PERCENT DISCOUNT | DISCOUNT TYPE | DEPOSIT AMOUNT | DATE STAMP   |
|------------------------------|--------------|-------------|------------------|---------------|----------------|--|
| Master file                  | CSU-2019-002 | \$ 2,000    | -                |               | \$             | RECEIVED<br><b>MAR 13 2019</b><br>CITY OF MILWAUKIE<br>PLANNING DEPARTMENT |
| Concurrent application files |              | \$          |                  |               | \$             |  |
|                              |              | \$          |                  |               | \$             |  |
|                              |              | \$          |                  |               | \$             |  |
|                              |              | \$          |                  |               | \$             |  |
| <b>SUBTOTALS</b>             |              | \$ 2,000    |                  |               | \$             |  |

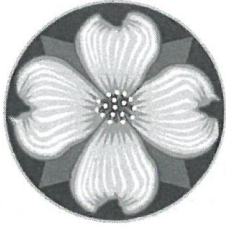
**TOTAL AMOUNT RECEIVED: \$** \_\_\_\_\_ **RECEIPT #:** \_\_\_\_\_ **RCD BY:** \_\_\_\_\_

**Associated application file #s** (appeals, modifications, previous approvals, etc.):

**Neighborhood District Association(s):** *Historic Milwaukie*

**Notes:**

\*After discount (if any)



**MILWAUKIE PLANNING**  
6101 SE Johnson Creek Blvd  
Milwaukie OR 97206  
503-786-7630  
planning@milwaukieoregon.gov

# Submittal Requirements

**For all Land Use Applications  
(except Annexations and Development Review)**

All land use applications must be accompanied by a signed copy of this form (see reverse for signature block) and the information listed below. The information submitted must be sufficiently detailed and specific to the proposal to allow for adequate review. Failure to submit this information may result in the application being deemed incomplete per the Milwaukie Municipal Code (MMC) and Oregon Revised Statutes.

Contact Milwaukie Planning staff at 503-786-7630 or [planning@milwaukieoregon.gov](mailto:planning@milwaukieoregon.gov) for assistance with Milwaukie's land use application requirements.

1. **All required land use application forms and fees**, including any deposits.

*Applications without the required application forms and fees will not be accepted.*

2. **Proof of ownership or eligibility to initiate application** per MMC Subsection 19.1001.6.A.

*Where written authorization is required, applications without written authorization will not be accepted.*

3. **Detailed and comprehensive description** of all existing and proposed uses and structures, including a summary of all information contained in any site plans.

*Depending upon the development being proposed, the description may need to include both a written and graphic component such as elevation drawings, 3-D models, photo simulations, etc. Where subjective aspects of the height and mass of the proposed development will be evaluated at a public hearing, temporary onsite "story pole" installations, and photographic representations thereof, may be required at the time of application submittal or prior to the public hearing.*

4. **Detailed statement** that demonstrates how the proposal meets the following:

A. All applicable development standards (listed below):

1. **Base zone standards** in Chapter 19.300.
2. **Overlay zone standards** in Chapter 19.400.
3. **Supplementary development regulations** in Chapter 19.500.
4. **Off-street parking and loading standards and requirements** in Chapter 19.600.
5. **Public facility standards and requirements**, including any required street improvements, in Chapter 19.700.

B. All applicable application-specific approval criteria (check with staff).

*These standards can be found in the MMC, here: [www.qcode.us/codes/milwaukie/](http://www.qcode.us/codes/milwaukie/)*

5. **Site plan(s), preliminary plat, or final plat** as appropriate.

*See Site Plan, Preliminary Plat, and Final Plat Requirements for guidance.*

6. **Copy of valid preapplication conference report**, when a conference was required.

**APPLICATION PREPARATION REQUIREMENTS:**

- Five hard copies of all application materials are required at the time of submittal. Staff will determine how many additional hard copies are required, if any, once the application has been reviewed for completeness.
- All hard copy application materials larger than 8½ x 11 in. must be folded and be able to fit into a 10- x 13-in. or 12- x 16-in. mailing envelope.
- All hard copy application materials must be collated, including large format plans or graphics.

**ADDITIONAL INFORMATION:**

- Neighborhood District Associations (NDAs) and their associated Land Use Committees (LUCs) are important parts of Milwaukie's land use process. The City will provide a review copy of your application to the LUC for the subject property. They may contact you or you may wish to contact them. Applicants are strongly encouraged to present their proposal to all applicable NDAs prior to the submittal of a land use application and, where presented, to submit minutes from all such meetings. NDA information: [www.milwaukieoregon.gov/citymanager/what-neighborhood-district-association](http://www.milwaukieoregon.gov/citymanager/what-neighborhood-district-association).
- Submittal of a full or partial electronic copy of all application materials is strongly encouraged.

As the authorized applicant I, (print name) \_\_\_\_\_, attest that all required application materials have been submitted in accordance with City of Milwaukie requirements. I understand that any omission of required items or lack of sufficient detail may constitute grounds for a determination that the application is incomplete per MMC Subsection 19.1003.3 and Oregon Revised Statutes 227.178. I understand that review of the application may be delayed if it is deemed incomplete.

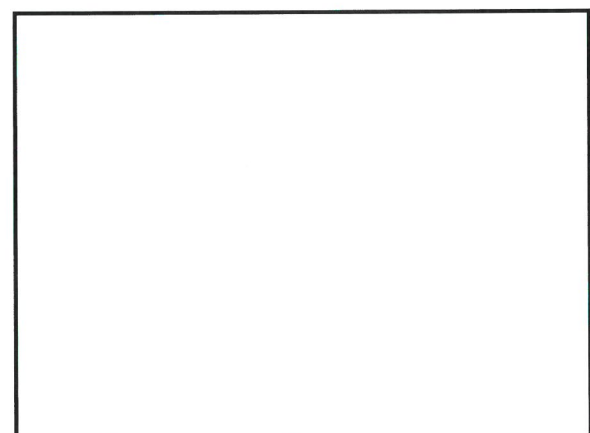
Furthermore, I understand that, if the application triggers the City's sign-posting requirements, I will be required to post signs on the site for a specified period of time. I also understand that I will be required to provide the City with an affidavit of posting prior to issuance of any decision on this application.

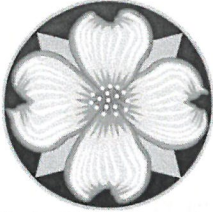
Applicant Signature: \_\_\_\_\_  


Date: March 12, 2019

**Official Use Only**

Date Received (date stamp below):





**MILWAUKIE PLANNING**  
 6101 SE Johnson Creek Blvd  
 Milwaukie OR 97206  
 503.786.7600  
 planning@milwaukieoregon.gov

# PREAPPLICATION CONFERENCE WAIVER

I/We, Andrew Tull, 3J Consulting, Inc. (print), as applicant(s)/property owner(s) of 11200 SE 23rd Avenue (address of property), request to waive the requirement for a preapplication conference for the submission of a **Type II / III / IV / V** (circle one) land use application per MMC Subsection 19.1002.2 Applicability.

**Please provide an explanation for the waiver request:**

MMC Section 19.1002 Preapplication Conference is provided on the reverse

This application is for the creation of a new parking lot at Milwaukie High School. The District and the applicant are prepared to make a submission for the parking lot without a pre-application conference.

Signed:   
 Applicant/Property Owner

Approved: \_\_\_\_\_  
 Planning Director

## **19.1002 PREAPPLICATION CONFERENCE**

### **19.1002.1 Purpose**

The purpose of the preapplication conference is to acquaint the applicant or applicant's representative with the requirements of the municipal code in preparation for submission of a land use application, including relevant approval criteria, development standards, and procedures. The preapplication conference is not an exhaustive review of all potential issues or requirements. Furthermore, the information provided by the City is not binding, and it does not preclude the City from raising new issues or identifying additional requirements during the land use review process.

### **19.1002.2 Applicability**

- A. For Type I applications, a preapplication conference is optional.
- B. For Type II, III, IV, and V applications, and expedited annexations per Section 19.1104, a preapplication conference is required, with the following exceptions:
  - 1. The Planning Director may waive the preapplication conference requirement for proposals that are not complex or, for some other reason, would not benefit from a formal conference.
  - 2. A preapplication conference is not required for City-initiated Type IV or V applications.

### **19.1002.3 Preapplication Conference Procedures**

The Planning Director shall adopt administrative rules for how the City processes preapplication conferences. The rules shall ensure that preapplication conferences are held in a timely fashion and provide a thorough explanation of all required City permits, fees, and approvals for any given development proposal. They shall include standards for scheduling, conducting, and communicating the outcomes of preapplication conferences.

### **19.1002.4 Preapplication Conference Expiration**

- A. A preapplication conference is valid for 2 years. If a land use application or development permit has not been submitted within 2 years of the conference date, the applicant is required to schedule a new preapplication conference prior to submittal. This requirement may be waived per Subsection 19.1002.2.B.1.
- B. An applicant may request additional preapplication conferences at any time. There is no limit to the number of preapplication conferences that may be requested.
- C. If a development proposal is significantly modified after a preapplication conference occurs, the Planning Director may require a new preapplication conference. The City may refuse to accept a land use application or development permit for a significantly altered development proposal until a new preapplication conference is held.



# Attachment B -- Ownership Information

*First American Title*

Customer Service Department  
 121 SW Morrison St., Suite 300  
 Portland, OR 97204  
 Phone: 503.219.TRIO (8746)  
 Fax: 503.790.7872  
 Email: cs.portland@firstam.com  
 Date: 10/4/2017

## OWNERSHIP INFORMATION

Owner: Clackamas Sd #12 North  
 Coowner:  
 Site: 11200 SE 23rd Ave Milwaukie 97222-7754  
 Mail: 12400 SE Freeman Way Milwaukie OR 97222

Parcel #: 00027544  
 Ref Parcel #: 11E36BC05600  
 TRS: T: 01S R: 01E S: 36 Q: NW  
 County: Clackamas

### PROPERTY DESCRIPTION

Map Grid: 656-J3  
 Census Tract: 020800 Block: 3006  
 Neighborhood: HISTORIC MILWAUKIE  
 School Dist: 12 NORTH CLACKAMAS  
 Subdiv/Plat: Duersts Add  
 Land Use: AMSC AGRICULTURAL MISC  
 Zoning: Milwaukie-R-2 High Density Residential District  
 Watershed: Johnson Creek-Willamette River  
 263 DUERSTS ADD LTS 3-6 PT LTS 1 & 2  
 BLK 1 LTS 1 2 9 & 10 BLK 2 & VAC ST 460  
 YOUNGS ADD LTS 1-18 BLK 4 LTS 1-8 BLK  
 Legal: 5 BLKS 1 & 2 LT 2 & PT LT 1 BLK 6 VAC ST  
 & PT SEC 5 MILWAUKIE PT BLK 55 &  
 VAC|Y|179608

### ASSESSMENT AND TAXATION

Market Land: \$1,600,524  
 Market Impr: \$46,511,350  
 Market Total: \$48,111,874  
 % Improved: 97  
 Assessed Total: \$27,303,677 (2016)  
 Levy Code: 012-002  
 Tax: \$0.00 (2016)  
 Millage Rate: 19.6884

### PROPERTY CHARACTERISTICS

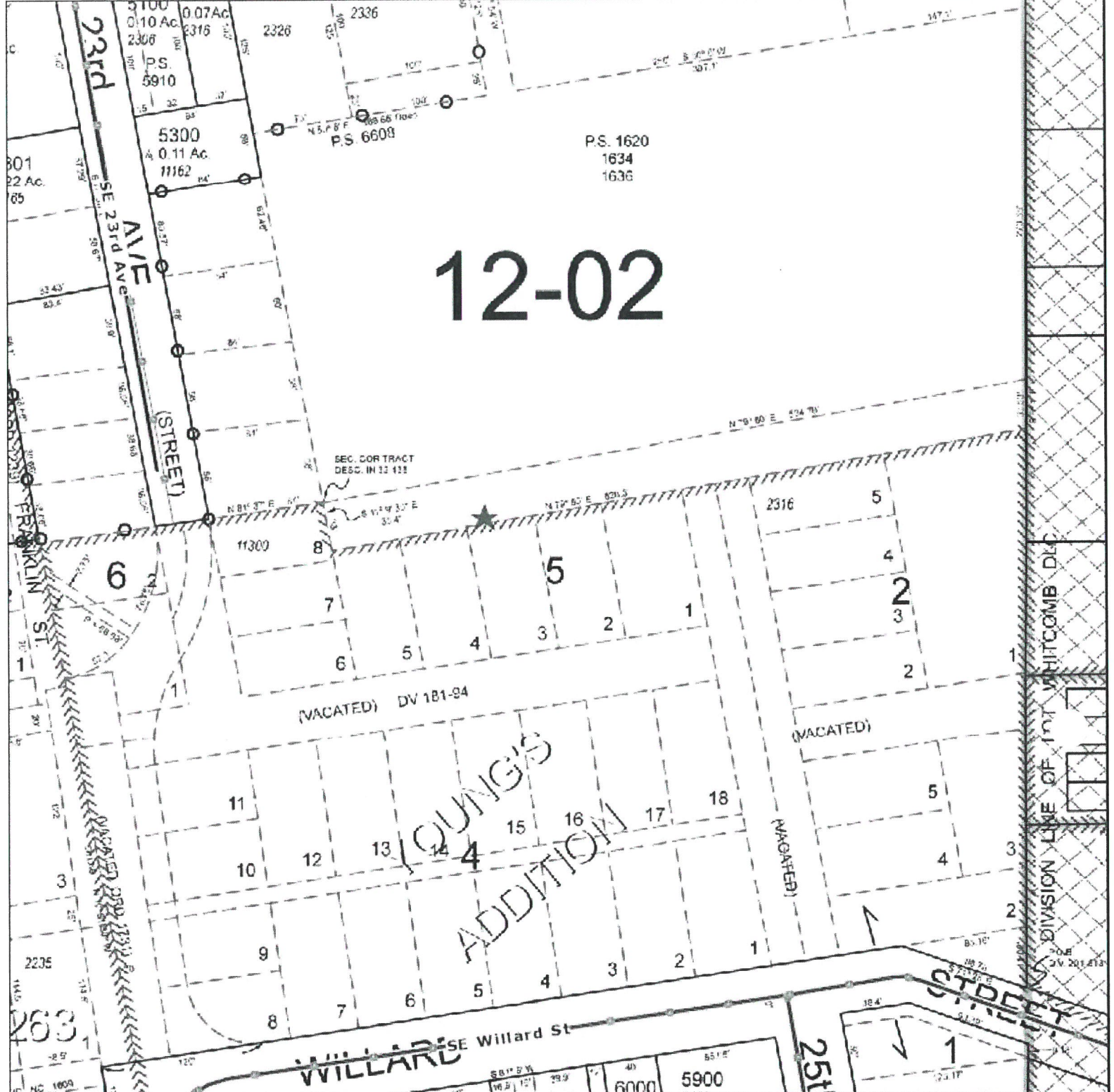
|                    |                 |                        |
|--------------------|-----------------|------------------------|
| Bedrooms:          | Building Area:  | Year Built:            |
| Baths, Total: 0.00 | First Floor:    | Eff Year Built:        |
| Baths, Full:       | Second Floor:   | Lot Size: 14.77 Acres  |
| Baths, Half:       | Basement Fin:   | Lot Size: 643,381 SqFt |
| Total Units:       | Basement Unfin: | Lot Width:             |
| # Stories: 0.00    | Basement Total: | Lot Depth:             |
| # Fireplaces:      | Attic Fin:      | Roof Material:         |
| Cooling: No        | Attic Unfin:    | Roof Shape:            |
| Heating:           | Attic Total:    |                        |
| Ext Walls:         | Garage:         |                        |
| Building Style:    |                 | Const Type: 0.0        |

### SALES AND LOAN INFORMATION

| Owner | Date | Doc #   | Sale Price | Deed Type | Loan Amt | Loan Type |
|-------|------|---------|------------|-----------|----------|-----------|
|       |      | 550-287 |            |           |          |           |

This title information has been furnished without charge, in conformance with the guidelines approved by the State of Oregon Insurance Commissioner. The Insurance Division cautions intermediaries that this service is designed to benefit the ultimate insureds. Indiscriminate use only benefiting intermediaries will not be permitted. Said services may be discontinued. No liability is assumed for any errors in this report.

# 12-02



**First American Title**

This map/plat is being furnished as an aid in locating the herein described land in relation to adjoining streets, natural boundaries and other land, and is not a survey of the land depicted. Except to the extent a policy of title insurance is expressly modified by endorsement, if any, the company does not insure dimensions, distances, location of easements, acreage or other matters shown thereon.

geoAdvantage





**First American Title**

This map/plat is being furnished as an aid in locating the herein described land in relation to adjoining streets, natural boundaries and other land, and is not a survey of the land depicted. Except to the extent a policy of title insurance is expressly modified by endorsement, if any, the company does not insure dimensions, distances, location of easements, acreage or other matters shown thereon.

**geoAdvantage**

1008  
Chicago Titu c 212429

V.G. & BETTY JEAN VAN BERGEN  
 12366 SE Guilford Dr.  
 Milwaukie OR 97222  
Grantor's Name and Address  
 NORTH CLACKAMAS SCHOOL DISTRICT #12  
 4444 SE Lake Road  
 Milwaukie OR 97222  
Grantee's Name and Address  
After recording, return to (Name, Address, Zip):  
 NORTH CLACKAMAS SCHOOL DISTRICT #12  
 4444 SE Lake Road  
 Milwaukie OR 97222  
Until requested otherwise, send all tax statements to (Name, Address, Zip):  
 NORTH CLACKAMAS SCHOOL DISTRICT #12  
 4444 SE Lake Road  
 Milwaukie OR 97222

Agenda # R99/00-72

RECORDED IN CLACKAMAS COUNTY  
JOHN KAUFFMAN, COUNTY CLERK

2000-013963



\$31.00

03/03/2000 01:34:57 PM

SPACE DD - 1 - 3 AMIEE  
\$10.00 \$11.00 \$10.00

Witness my hand and seal of County attixed.

NAME TITLE  
By \_\_\_\_\_ Deputy.

BARGAIN AND SALE DEED - STATUTORY FORM  
(INDIVIDUAL GRANTOR)

V.G. VAN BERGEN and BETTY JEAN VAN BERGEN, Grantor,  
conveys to NORTH CLACKAMAS SCHOOL DISTRICT NO. 12, Grantee,

the following real property situated in Clackamas County, Oregon, to-wit:  
SEE ATTACHED LEGAL DESCRIPTIONS: commonly known as 2326 and 2336 SE  
Washington Street, Milwaukie, Oregon:

This sale is a real property transaction entered into by the parties  
in lieu of condemnation by the Buyer.

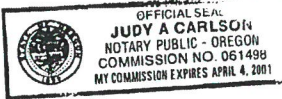
(IF SPACE INSUFFICIENT, CONTINUE DESCRIPTION ON REVERSE)  
The true consideration for this conveyance is \$ 450,000.00 (Here, comply with the requirements of ORS 93.030.)

DATED February 29, 2000

THIS INSTRUMENT WILL NOT ALLOW USE OF THE PROPERTY DESCRIBED IN  
THIS INSTRUMENT IN VIOLATION OF APPLICABLE LAND USE LAWS AND REGU-  
LATIONS. BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON  
ACQUIRING FEE TITLE TO THE PROPERTY SHOULD CHECK WITH THE APPRO-  
PRIATE CITY OR COUNTY PLANNING DEPARTMENT TO VERIFY APPROVED USES  
AND TO DETERMINE ANY LIMITS ON LAWSUITS AGAINST FARMING OR FOREST  
PRACTICES AS DEFINED IN ORS 90.930.

*V.G. Van Bergen*  
V.G. VAN BERGEN  
*Betty Jean Van Bergen*  
BETTY JEAN VAN BERGEN

STATE OF OREGON, County of Clackamas ) ss.  
This instrument was acknowledged before me February 29, 2000  
by V.G. VAN BERGEN and BETTY JEAN VAN BERGEN



*Judy A. Carlson*  
Judy A. Carlson  
Notary Public for Oregon  
My commission expires \_\_\_\_\_

Order No: 212429

LEGAL DESCRIPTION

PARCEL I:

Part of the Lot Whitcomb and wife Donation Land Claim No. 38, in Section 36, Township 1 South, Range 1 East, in the City of Milwaukie, County of Clackamas and State of Oregon, described as follows:

Beginning at a point on the South side of Washington Street in the City of Milwaukie, which point is the Northwest corner of that tract of land conveyed to Sam G. Marines, et ux, by Deed recorded March 18, 1919, in Book 152, Page 270, Deed Records; thence Southerly along the Westerly boundary line of said Marines Tract 100 feet to the Northwest corner of that tract of land conveyed to Sam George Marines, et ux, by Deed recorded November 6, 1946, in Book 379, Page 711, Clackamas County Deed Records; thence continuing Southerly along the Westerly boundary line of said second Marines Tract 25 feet to the Southwest corner thereof; thence Westerly along the Northerly line of that parcel of land conveyed to Union High School District No. 5 of Clackamas County, by Deed recorded March 10, 1938, in Book 244, Page 256, Clackamas County Deed Records, 73 feet, more or less, to a point in the Easterly boundary line of that tract of land conveyed to George C. Hildenbrand, et ux, by Deed recorded March 4, 1925, in Book 180, Page 151, Clackamas County Deed Records; thence Northerly along the East boundary line of said Hildenbrand Tract, and along the East boundary line of that parcel of land conveyed to Milwaukie Veterans Building Commission, by Deed recorded January 15, 1949, in Book 415, Page 653, Clackamas County Deed Records, 125 feet, more or less, to the South line of said Washington Street; thence Easterly along the South line of said Washington Street 73 feet to the point of beginning.

PARCEL II:

A part of the Lot Whitcomb Donation Land Claim in Township 1 South, Range 1 East of the Willamette Meridian, in the City of Milwaukie, in the County of Clackamas and State of Oregon, more particularly described as follows:

Beginning at an iron pipe on the South line of Washington Street in the Town of Milwaukie, Oregon, North 80° East 73 feet from the Northwest corner of a tract of land conveyed to A. J. Walker by deed recorded in Book 53 on Page 393, Record of Deed of Clackamas County, Oregon; thence running North 80° East 100 feet; thence at right angles South 10° East 100 feet; thence South 80° West 100 feet; thence North 10° West 100 feet to the place of beginning.

ALSO, beginning at a point which is the Northwest corner of a tract of land conveyed to Sam George Marines and Martha Eleanor Marines, husband and wife, by deed recorded in Book 152, Page 270, of the Deed Records of Clackamas County, Oregon; thence running Southerly along the Westerly line of said Marines tract a distance of 100 feet to point of beginning of the tract herein described; thence running Easterly along the Southerly line of said Marines tract, a distance of 100 feet to the Southeast corner of said Marines tract; thence Southerly along a tract of land conveyed to Union High School District No. 5 of Clackamas County, Oregon by deed recorded in Book 244, Page 256 of the Deed Records of Clackamas county, Oregon, a distance of 25 feet to a point; thence running Westerly along the Northerly line of said High School tract a distance of 100 feet to a point; thence Northerly 25 feet to the point of beginning.

February 11, 2019

**Neighborhood Meeting  
Parking Lot Improvements at Milwaukie High School**

To Our Neighbor,

3J Consulting, Inc. represents the North Clackamas School District regarding a proposal to create a new parking lot on property owned by the District located south of the new Milwaukie High School. The new parking area will create 30 new spaces within a landscaped parking lot at the corner of SE Lake and SE 25<sup>th</sup> Avenue. This site is currently in use as the High School's tennis courts. The District is in the process of constructing new tennis courts for the High School at Rowe Middle School. The subject parcel is shown on the attached vicinity map. We would like to take the opportunity to discuss the proposal with you in more detail, prior to submitting a formal land use application to the City.

The purpose of this meeting is to provide a forum for the District and surrounding property owners/residents to review and consider the proposal. The meeting gives you the opportunity to share with us any special information you know about the property. We will answer questions related to how the project meeting relevant development standards consistent with Milwaukie's land use regulations.

You are cordially invited to attend a meeting on:

**Monday, March 4, 2019  
6:00PM-7:00PM  
Milwaukie High School Commons  
2301 SE Willard Street  
Milwaukie, Oregon 97222**

Parking for this meeting is available in the parking lot located southwest of the Administration Building.

Please note that this will be an informational meeting and is not intended to take the place of public hearing before the City. Members of the public will have the opportunity to present testimony to the Planning Commission at the public hearing.

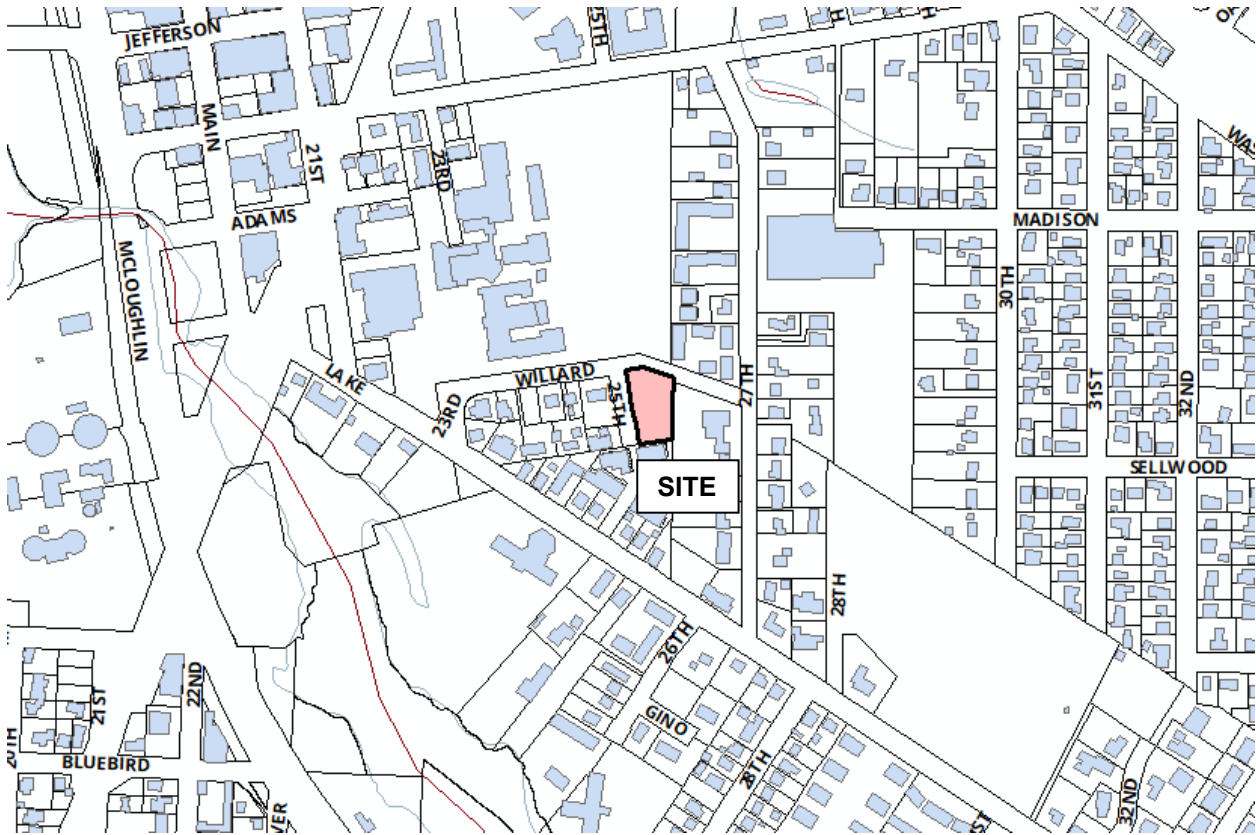
We look forward to discussing this proposal with you. Please feel free to contact us at (503) 946-9365 or [andrew.tull@3j-consulting.com](mailto:andrew.tull@3j-consulting.com) if you have questions.

Sincerely,



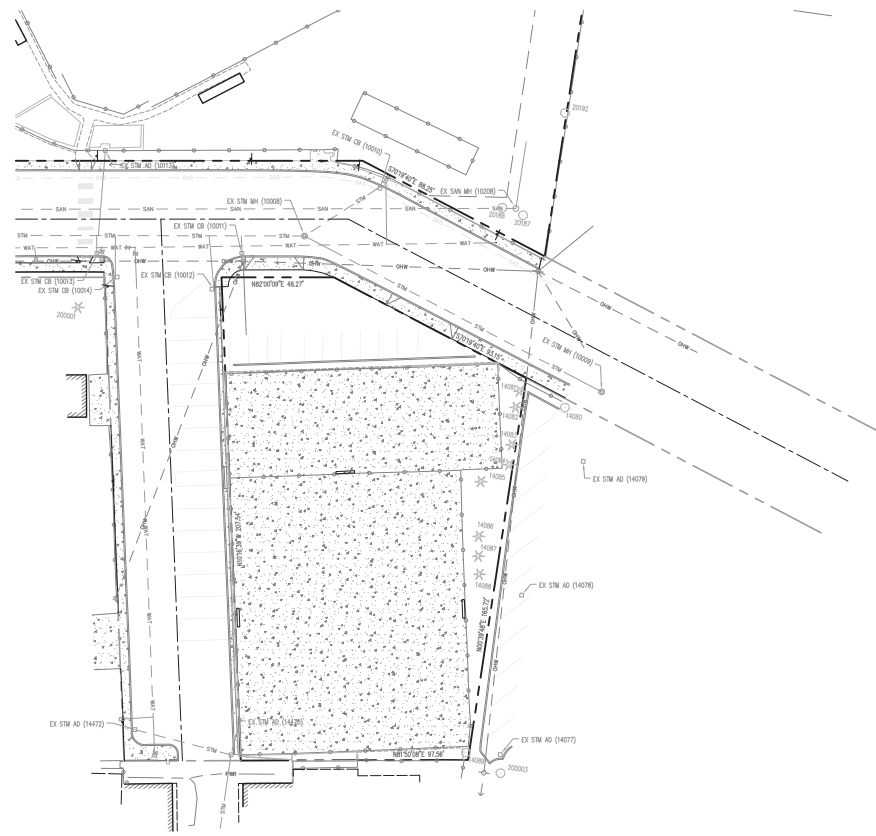
Andrew Tull  
Principal Planner  
3J Consulting, Inc.



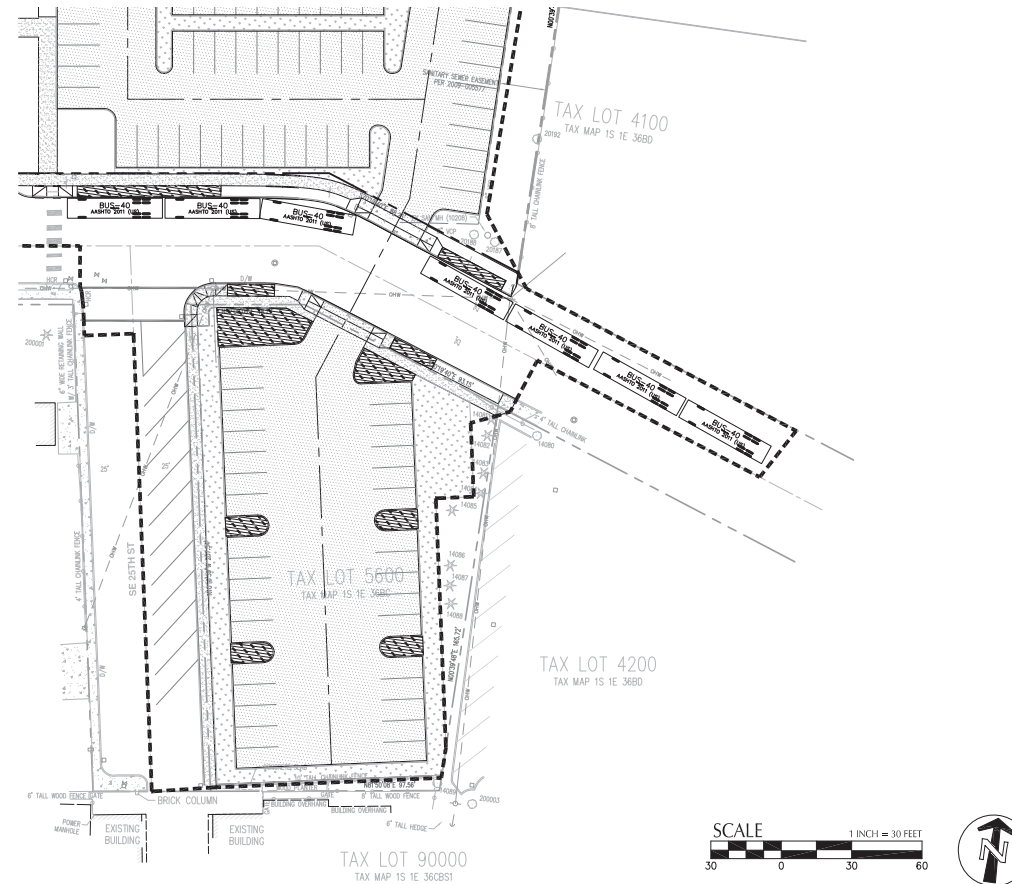


VICINITY MAP

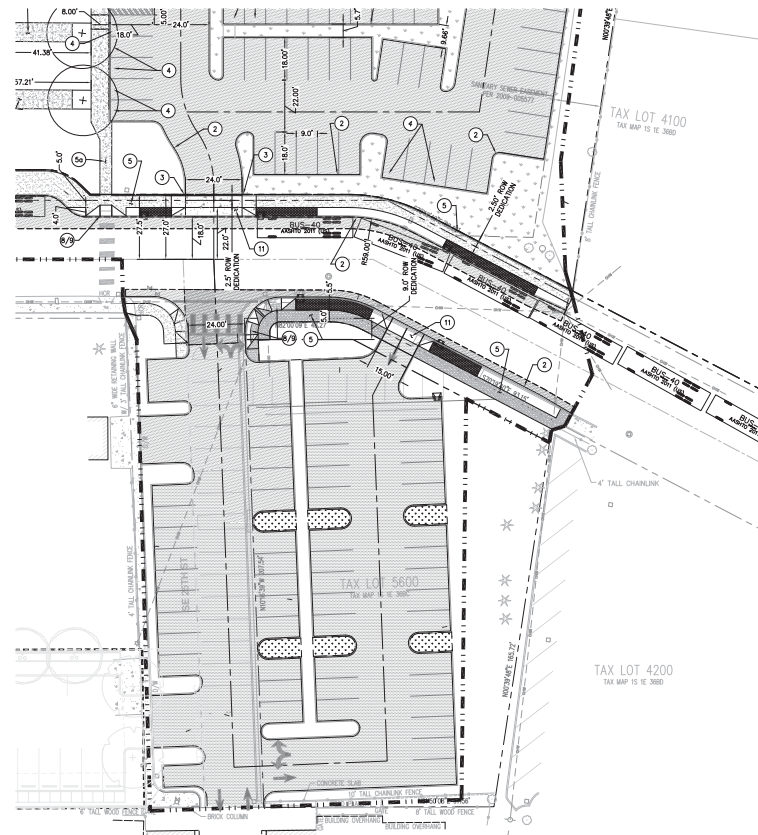




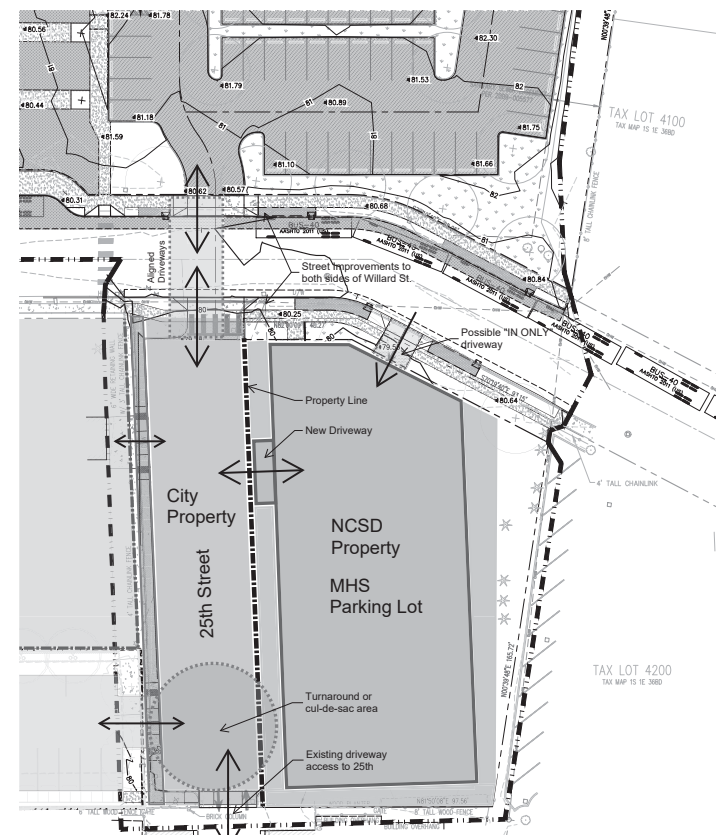
**EXISTING SITE PLAN July 2017**



**SITE PLAN - Option 1 July 2017**



**SITE PLAN - Option 2 December 2017**



**SITE PLAN - Option 3 January 2018**

**BRIC**  
ARCHITECTURE, INC.

1233 NW Northrup Street  
Suite 100  
Portland, Oregon 97209  
tel. (503) 595 4900

PRELIMINARY

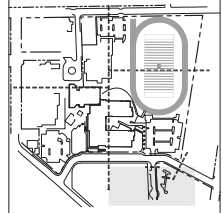
**kpff**

111 SW Fifth Ave., Suite 2400  
Portland, OR 97204  
O: 503.227.3251  
F: 503.274.4661  
www.kpff.com

**NORTH**  
Clackamas Schools

**MILWAUKIE HIGH SCHOOL**

NORTH CLACKAMAS SCHOOL DISTRICT  
2301 SE Willard Street, Milwaukie, OR 97222  
t: (503) 353-5830  
f: (503) 353-5846



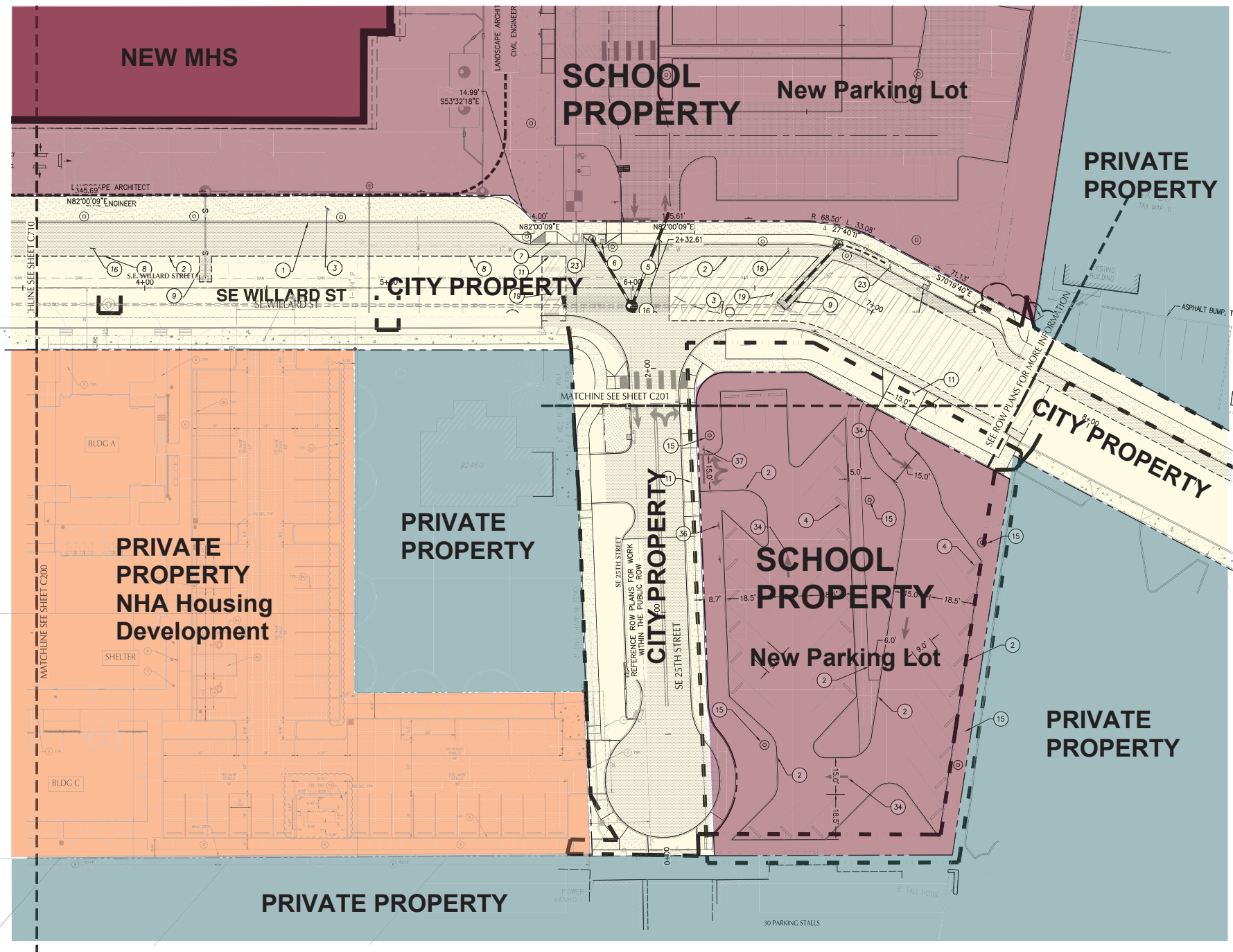
key plan

| revisions |  |
|-----------|--|
|           |  |

|         |                            |
|---------|----------------------------|
| phase   | 100% CD - IFC - PERMIT SET |
| date    | 11/26/2018                 |
| project | 17010                      |

SITE AND HORIZONTAL CONTROL PLANS

**C204**



**SITE PLAN - Current Design January 2019**

**SHEET NOTES**

1. ALL DIMENSIONS ARE TO FACE OF CURB OR FACE OF WALL (UNLESS OTHERWISE NOTED).
2. SEE LANDSCAPE PLANS FOR PLANTING AND ADDITIONAL SITE FEATURES.
3. SEE LANDSCAPE PLANS FOR ALL PEDESTRIAN HARDSCAPE ON THE LANDSCAPE ARCHITECT SIDE OF THE REFERENCE LINE, INCLUDING CONCRETE SCORING AND JOINT LOCATIONS.
4. SEE ARCHITECTURAL PLANS FOR ADDITIONAL DIMENSIONS AND LAYOUT.
5. SEE LANDSCAPE PLANS FOR ALL FENCING HANDRAILS AND GATES.
6. SEE SHEETS C700-C802 FOR PROPOSED FRONTAGE IMPROVEMENTS IN RIGHT-OF-WAY, TO BE PERMITTED UNDER SEPARATE PERMIT.
7. SEE SHEET C205 FOR LINE AND CURVE DATA TABLES.

**SHEET LEGEND**

- BUILDING OVERHANG
- PROPERTY LINE
- BUILDING
- HEAVY CONCRETE PAVING - 1/2" (1/2")
- LIGHT ASPHALT PAVEMENT SECTION (PULL IN PARKING) - 4B (1/2")
- HEAVY ASPHALT PAVEMENT SECTION (DRIVE AISLE) - 4A (1/2")
- CONCRETE SIDEWALK - SEE LANDSCAPE PLANS FOR ADDITIONAL DETAIL - 1 (1/2")
- PERVIOUS CONCRETE - 1111 (1/2")
- ARTIFICIAL TURF - SEE LANDSCAPE PLANS FOR DETAILS
- LANDSCAPE AREA - SEE LANDSCAPE PLANS FOR DETAILS
- 2" MILL & OVERLAY
- SEALCOAT AND RESTRIPE EX. ASPHALT
- SAWCUT LINE
- LIMITS OF WORK - OFFSET FOR CLARITY
- FIRE LANE
- SITE LIGHTING (SEE SHEET EPH1 FOR PHOTOMETRICS)
- PROPOSED TREE
- MATCHLINE
- RED PAINTED CURB
- POROUS ASPHALT PAVEMENT SECTION

**KEY NOTES**

| #  | DESCRIPTION   |            |
|----|---|------------|
| 1  | MOUNTABLE CURB  | 7/C600     |
| 2  | STANDARD CURB   | 6/C600     |
| 3  | CURB ENDING TAPER TO FLUSH  | 8/C600     |
| 4  | WHEEL STOP  | 5/C601     |
| 5  | CONCRETE SIDEWALK   | 1/C600     |
| 5a | PERVIOUS CONCRETE SIDEWALK  | SW110/C600 |
| 6a | ADA PARKING STALLS AND STRIPING   | 2/C601     |
| 6b | "NO PARKING" ZONE STRIPING  |            |
| 6c | 4" PARKING STRIPING, TYPICAL  |            |
| 7  | ADA PARKING SIGN & VAN  | 7/C601     |
| 8  | DETECTABLE WARNING  | 3/C601     |
| 9  | CURB RAMP   | 8/C601     |
| 10 | CAST-IN-PLACE CONCRETE WALL   |            |
| 11 | CONCRETE DRIVEWAY APRON   | 502B/C802  |
| 12 | CONCRETE STAIRWAY. SEE LANDSCAPE PLANS.   |            |
| 13 | CURB SPILLWAY WITH SPLASH BLOCK   | SW330/C800 |
| 14 | ACCESSIBLE CROSSWALK  | 4/C601     |
| 15 | SITE LIGHTING - SEE SHEET EPH1 FOR PHOTOMETRICS   |            |
| 16 | SEE LANDSCAPE PLANS FOR PLAZA DETAIL INFORMATION.   |            |
| 17 | RETAINING WALL  |            |
| 18 | SEE LANDSCAPE PLANS FOR WALL LAYOUT.  |            |
| 19 | FLUSH CURB  | 9/C600     |
| 20 | HEIGHTENED CURB (MAX 18")   |            |
| 21 | ELECTRICAL SLAB & STRUCTURE SEE ARCH. SHEETS  |            |
| 22 | BUILDING GRID COORDINATES SHOWN ARE PRELIMINARY. COORDINATE BUILDING GRID LOCATION WITH ARCHITECTURAL PLANS PRIOR TO CONSTRUCTION |            |
| 23 | FIRE LANE PAINTED CURB WITH "FIRE-LANE-NO PARKING" STENCILS AT 20' INTERVALS  |            |
| 24 | TRASH ENCLOSURE-SEE ARCHITECTURAL SHEETS  |            |
| 25 | ELECTRICAL HOUSING-SEE ELECTRICAL DRAWINGS  |            |
| 30 | PARENT DROP OFF SIGN  | 14/C601    |
| 31 | BUS LOADING SIGN  | 10/C601    |
| 32 | PROVIDE STOP BAR  |            |
| 33 | CARPPOOL PARKING SIGN   | 11/C601    |
| 34 | PROVIDE DIRECTIONAL ARROWS  | 1/C601     |
| 35 | SEE LANDSCAPE PLANS FOR PLANTING INFORMATION  |            |
| 36 | GREEN VEHICLE PARKING SIGN  | 12/C601    |
| 37 | STOP SIGN   | 6/C601     |
| 38 | DO NOT ENTER SIGN   | 13/C601    |
| 39 | ELECTRICAL VEHICLE PARKING STALL  |            |
| 40 | FLUSH CONCRETE CROSSING/DRIVEWAY APRON  | 502C/C800  |

**BRIC**  
ARCHITECTURE, INC.

1233 NW Northrup Street  
Suite 100  
Portland, Oregon 97209  
tel. (503) 595 4900

PRELIMINARY

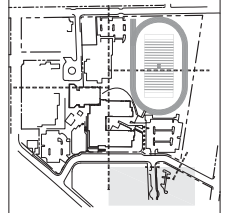
**kpff**

111 SW Fifth Ave., Suite 2400  
Portland, OR 97204  
O: 503.227.3251  
F: 503.274.4661  
www.kpff.com

**NORTH**  
Clackamas Schools

**MILWAUKIE HIGH SCHOOL**

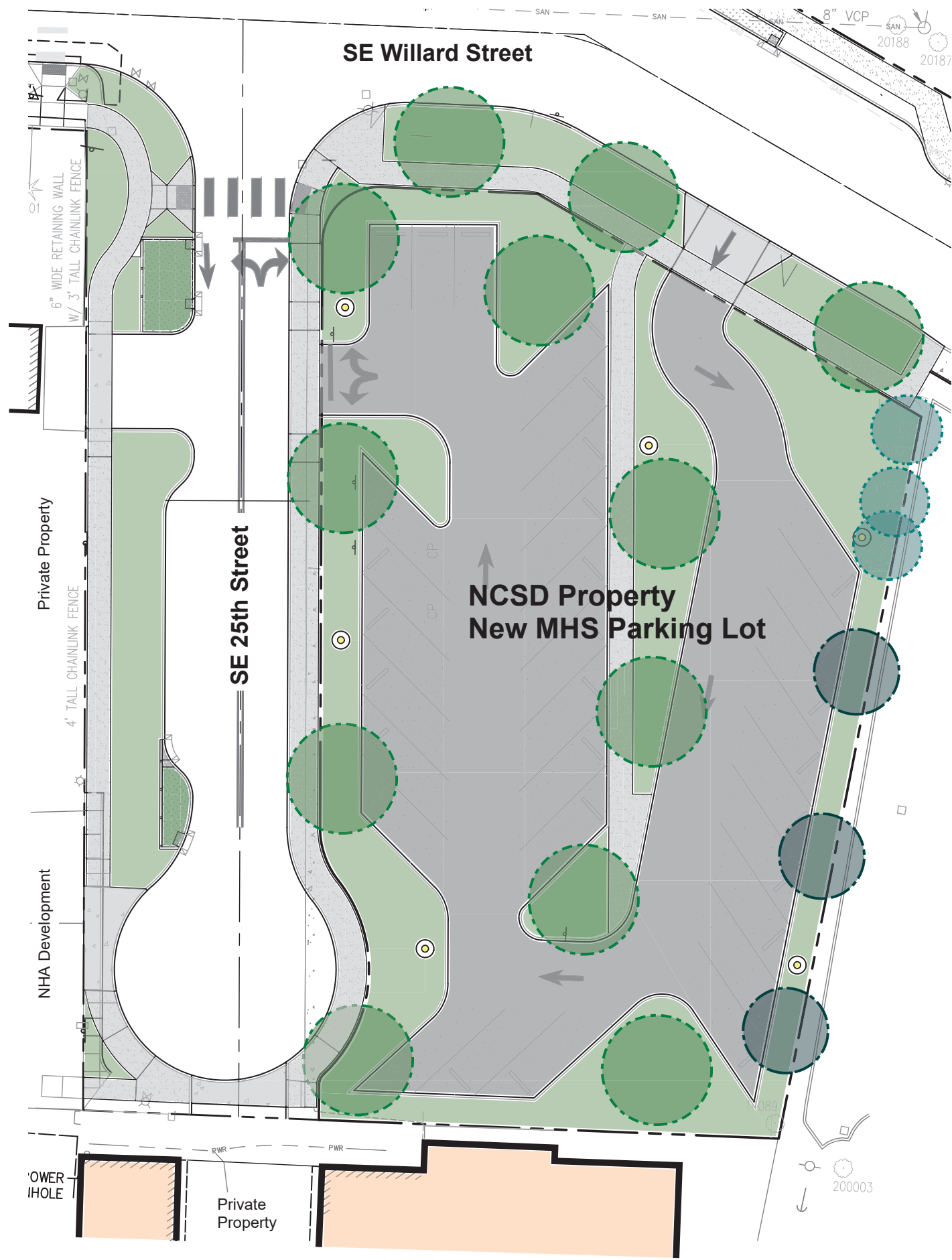
NORTH CLACKAMAS SCHOOL DISTRICT  
2301 SE Willard Street, Milwaukie, OR 97222  
t: (503) 353-5830  
f: (503) 353-5846



| revisions                                | phase   | 100% CD - IFC - PERMIT SET |
|--|---------|----------------------------|
|  | date    | 11/26/2018                 |
|  | project | 17010                      |
| <b>SITE AND HORIZONTAL CONTROL PLANS</b> |         |                            |
| <b>C204</b>                              |         |                            |

File: N:\C:\p\2017\1700019-Milwaukie-HS-CAD\PLDT\CD\1700019-C200-SITE.dwg TAB: C204  
Pictorial: 1/4/19 at 3:51pm By: Juggenheimer





**SHEET NOTES**

1. ALL DIMENSIONS ARE TO FACE OF CURB OR FACE OF WALL (UNLESS OTHERWISE NOTED).
2. SEE LANDSCAPE PLANS FOR PLANTING AND ADDITIONAL SITE FEATURES.
3. SEE LANDSCAPE PLANS FOR ALL PEDESTRIAN HARDSCAPE ON THE LANDSCAPE ARCHITECT SIDE OF THE REFERENCE LINE, INCLUDING CONCRETE SCORING AND JOINT LOCATIONS.
4. SEE ARCHITECTURAL PLANS FOR ADDITIONAL DIMENSIONS AND LAYOUT.
5. SEE LANDSCAPE PLANS FOR ALL FENCING HANDRAILS AND GATES.
6. SEE SHEETS C700-C802 FOR PROPOSED FRONTAGE IMPROVEMENTS IN RIGHT-OF-WAY, TO BE PERMITTED UNDER SEPARATE PERMIT.
7. SEE SHEET C205 FOR LINE AND CURVE DATA TABLES.

**BRIC**  
ARCHITECTURE, INC.

1233 NW Northrup Street  
Suite 100  
Portland, Oregon 97209  
tel. (503) 595 4900

PRELIMINARY

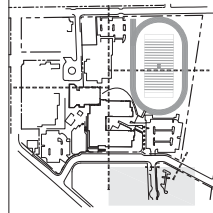
**kpff**

111 SW Fifth Ave., Suite 2400  
Portland, OR 97204  
t: 503.227.3251  
f: 503.274.4681  
www.kpff.com

**NORTH**  
Clackamas Schools

**MILWAUKIE HIGH SCHOOL**

NORTH CLACKAMAS SCHOOL DISTRICT  
2301 SE Willard Street, Milwaukie, OR 97222  
t: (503) 353-5830  
f: (503) 353-5846



key plan

| revisions |  |
|-----------|--|
|           |  |

|         |                            |
|---------|----------------------------|
| phase   | 100% CD - IFC - PERMIT SET |
| date    | 11/26/2018                 |
| project | 17010                      |

SITE AND HORIZONTAL CONTROL PLANS

**C204**



File: N:\C:\p\2017\1700019-Milwaukie-HS-CAD\PLDT\CD\700019-C200-SITE.dwg TAB: C204  
Pictorial: 1/4/19 at 3:51pm By: Juggenheimer



# 3J CONSULTING

5075 SW GRIFFITH DRIVE, SUITE 150  
 BEAVERTON, OREGON 97005  
 PH: (503) 946.9365  
 WWW.3J-CONSULTING.COM

## MILWAUKIE HIGH SCHOOL SIGN-IN SHEET MARCH 4, 2019

| NAME           | ADDRESS                                  | EMAIL & PHONE           |
|----------------|--|-------------------------|
| Susan Jenkins  | 2431 SE Lake Rd<br>Milwaukie (TLE)       | sjenkins7@evergreen.net |
| Dandy King     | 2439 SE Lake Rd<br>Milw. OR 97222        | sjk2001@hotmail.com     |
| Shirley Walker | 2445 SE Lake<br>97222                    |                         |
| Michael Paul   | 2460 SE Willard<br>Milwaukie 97222       | mpa-12001@gmail.com     |
| Ray Bryan      | 11416 SE 27th Ave<br>Milwaukie, OR 97222 | raylbryan2@gmail.com    |
| Siri Bernad    | 2437 SE Lake                             | sribernad@comcast.net   |
|                |  |                         |
|                |  |                         |
|                |  |                         |
|                |  |                         |
|                |  |                         |



April 18, 2017

Steven Nicholas  
Heery International  
Two Centerpointe Drive, Suite 250  
Lake Oswego, OR 97035

**RE: Milwaukie High School – Arborist Tree Evaluation**

Mr. Nicholas:

As requested, attached is the Detailed Tree Inventory information for the trees shown on the Topographic Survey. Please let me know if you have any questions.

Sincerely,

**AKS ENGINEERING & FORESTRY, LLC**



Bruce R. Baldwin  
ISA Certified Arborist #PN-6666A  
ISA Qualified Tree Risk Assessor  
Member, International Society of Arboriculture



## Detailed Tree Inventory for Milwaukie High School

AKS Job No. 5859 - Tree Evaluation: April 6 & 11, 2017

| Tree # | DBH (in.)           | Tree Species<br>Common Name<br>(Scientific name)  | Comments  | Health Rating* | Structure Rating** |
|--------|---------------------|---|---|----------------|--------------------|
| 10422  | 17                  | Cherry ( <i>Prunus sp.</i> )                      | Pruning scars with good woundwood closure; Large exposed roots; Many scaffold branches weakly attached at one point | 1              | 2                  |
| 10423  | 15                  | Maple ( <i>Acer sp.</i> )                         | Many large exposed roots; Multiple scaffold branches weakly attached at one point                                   | 1              | 2                  |
| 10973  | 24,12               | Maple ( <i>Acer sp.</i> )                         | <b>OFFSITE</b>  | 1              | 1                  |
| 10986  | 49                  | Cedar ( <i>Cedrus sp.</i> )                       |   | 1              | 1                  |
| 10987  | 24                  | European White Birch<br>( <i>Betula pendula</i> ) | Broken branches; Some bulges on bole  | 1              | 2                  |
| 10988  | 55                  | Cedar ( <i>Cedrus sp.</i> )                       | Codominant 20' from ground with included bark   | 1              | 2                  |
| 10989  | 17                  | Western Redcedar<br>( <i>Thuja plicata</i> )      | Codominant 10' from ground with included bark   | 1              | 2                  |
| 11081  | 8                   | Maple ( <i>Acer sp.</i> )                         |   | 1              | 1                  |
| 11082  | 13                  | Maple ( <i>Acer sp.</i> )                         | Small bulge on bole at base   | 1              | 1                  |
| 11124  | 13                  | Maple ( <i>Acer sp.</i> )                         | Roots are girdling  | 1              | 2                  |
| 11130  | 25                  | European White Birch<br>( <i>Betula pendula</i> ) | Lean (S); Cavities with decay; Bulges on bole; Dead branches; Crooked   | 2              | 3                  |
| 11131  | 32                  | European White Birch<br>( <i>Betula pendula</i> ) | Broken branches; Pruning scars with decay; Epicormic sprouts; Bulges on bole; Scars; Cavities with decay; Crooked   | 2              | 3                  |
| 11132  | 38                  | Bigleaf Maple<br>( <i>Acer macrophyllum</i> )     | Large exposed roots; Large bulge on bole  | 1              | 2                  |
| 11133  | 7,8,11,<br>11,12,13 | Western Redcedar<br>( <i>Thuja plicata</i> )      | 11" Stems - Crooked and sweep away from main stem; 11" Stem - bark peeling; Poor structure                          | 2              | 2                  |
| 11150  | 7                   | Cherry ( <i>Prunus sp.</i> )                      | Roots exposed and damage to top; Crooked  | 1              | 1                  |
| 11151  | 7                   | Cherry ( <i>Prunus sp.</i> )                      | Roots exposed and damage to top; Crooked  | 1              | 1                  |
| 11175  | 7                   | Cherry ( <i>Prunus sp.</i> )                      | Large roots appear to be girdling; Many roots above surface; Pruned   | 1              | 2                  |
| 11176  | 6                   | Cherry ( <i>Prunus sp.</i> )                      | Bark peeling  | 2              | 1                  |
| 11219  | 62                  | Cedar ( <i>Cedrus sp.</i> )                       |   | 1              | 1                  |
| 11291  | 10                  | Maple ( <i>Acer sp.</i> )                         |   | 1              | 1                  |
| 11407  | 6                   | Maple ( <i>Acer sp.</i> )                         |   | 1              | 1                  |
| 11409  | 6                   | Maple ( <i>Acer sp.</i> )                         |   | 1              | 1                  |
| 11460  | 6                   | Maple ( <i>Acer sp.</i> )                         |   | 1              | 1                  |
| 11461  | 9                   | Maple ( <i>Acer sp.</i> )                         |   | 1              | 1                  |



## Detailed Tree Inventory for Milwaukie High School

AKS Job No. 5859 - Tree Evaluation: April 6 & 11, 2017

| Tree # | DBH (in.) | Tree Species<br>Common Name<br>(Scientific name)       | Comments   | Health Rating* | Structure Rating** |
|--------|-----------|--|--|----------------|--------------------|
| 11480  | 7         | Maple ( <i>Acer sp.</i> )                              |  | 1              | 1                  |
| 11482  | 6         | Maple ( <i>Acer sp.</i> )                              |  | 1              | 1                  |
| 11882  | 20        | Western Redcedar<br>( <i>Thuja plicata</i> )           | Codominant 10' from the ground with included bark                | 1              | 2                  |
| 11885  | 8         | Cherry ( <i>Prunus sp.</i> )                           |  | 1              | 1                  |
| 11935  | 13        | Western Redcedar<br>( <i>Thuja plicata</i> )           |  | 1              | 1                  |
| 11947  | 9         | Cherry ( <i>Prunus sp.</i> )                           |  | 1              | 1                  |
| 11960  | 7         | Cherry ( <i>Prunus sp.</i> )                           |  | 1              | 1                  |
| 11973  | 8         | Cherry ( <i>Prunus sp.</i> )                           |  | 1              | 1                  |
| 12148  | 25        | Bigleaf Maple<br>( <i>Acer macrophyllum</i> )          | <b>OFFSITE</b> ; Evaluation from the property line; Appears dead | 3              | 3                  |
| 12149  | 9         | Japanese Maple<br>( <i>Acer palmatum</i> )             |  | 1              | 1                  |
| 12150  | 9         | Japanese Maple<br>( <i>Acer palmatum</i> )             |  | 1              | 1                  |
| 12151  | 6         | Japanese Maple<br>( <i>Acer palmatum</i> )             |  | 1              | 1                  |
| 12172  | 8         | Japanese Maple<br>( <i>Acer palmatum</i> )             |  | 1              | 1                  |
| 12212  | 16        | "Cleveland Select" Pear<br>( <i>Pyrus calleryana</i> ) |  | 1              | 1                  |
| 12226  | 10        | "Cleveland Select" Pear<br>( <i>Pyrus calleryana</i> ) |  | 1              | 1                  |
| 12227  | 11        | "Cleveland Select" Pear<br>( <i>Pyrus calleryana</i> ) |  | 1              | 1                  |
| 12356  | 61        | Cedar ( <i>Cedrus sp.</i> )                            |  | 1              | 1                  |
| 12799  | 12        | "Cleveland Select" Pear<br>( <i>Pyrus calleryana</i> ) |  | 1              | 1                  |
| 12867  | 9         | Engelmann Spruce<br>( <i>Picea engelmannii</i> )       |  | 1              | 1                  |
| 12908  | 15        | "Cleveland Select" Pear<br>( <i>Pyrus calleryana</i> ) |  | 1              | 1                  |



## Detailed Tree Inventory for Milwaukie High School

AKS Job No. 5859 - Tree Evaluation: April 6 & 11, 2017

| Tree # | DBH (in.) | Tree Species<br>Common Name<br>(Scientific name)       | Comments   | Health Rating* | Structure Rating** |
|--------|-----------|--|--|----------------|--------------------|
| 12909  | 8         | "Cleveland Select" Pear<br>( <i>Pyrus calleryana</i> ) |  | 1              | 1                  |
| 12911  | 12        | "Cleveland Select" Pear<br>( <i>Pyrus calleryana</i> ) |  | 1              | 1                  |
| 13413  | 6         | Cherry ( <i>Prunus sp.</i> )                           |  | 1              | 1                  |
| 14080  | 9         | European White Birch<br>( <i>Betula pendula</i> )      | <b>OFFSITE</b> ; Some bore holes   | 1              | 1                  |
| 14081  | 10,13     | Douglas-fir<br>( <i>Pseudotsuga menziesii</i> )        | Codominant with included bark  | 1              | 2                  |
| 14082  | 7         | Douglas-fir<br>( <i>Pseudotsuga menziesii</i> )        |  | 1              | 1                  |
| 14083  | 9,11      | Douglas-fir<br>( <i>Pseudotsuga menziesii</i> )        | Codominant with included bark; Large roots lifting asphalt to east                 | 1              | 2                  |
| 14084  | 14        | Douglas-fir<br>( <i>Pseudotsuga menziesii</i> )        |  | 1              | 1                  |
| 14085  | 14        | Douglas-fir<br>( <i>Pseudotsuga menziesii</i> )        | Some broken branches; Sap flow   | 1              | 1                  |
| 14086  | 17        | Douglas-fir<br>( <i>Pseudotsuga menziesii</i> )        | Bulges on bole   | 2              | 2                  |
| 14087  | 12        | Douglas-fir<br>( <i>Pseudotsuga menziesii</i> )        | Codominant 10' from the ground with included bark                                  | 1              | 2                  |
| 14088  | 15        | Douglas-fir<br>( <i>Pseudotsuga menziesii</i> )        | Sap flow   | 1              | 1                  |
| 14089  | 9         | Oregon Oak<br>( <i>Quercus garryana</i> )              | Sparse foliage; Some dead branches; Multiple branches weakly attached at one point | 2              | 2                  |
| 20187  | 6         | Cherry ( <i>Prunus sp.</i> )                           | Pruning scars; Decay at pruning scars; Bark peeling at base of tree; Cracked bark  | 2              | 2                  |
| 20188  | 6,6,9,10  | Cherry ( <i>Prunus sp.</i> )                           | Large bulges at base; Pruned; Multiple weak attachments at one point               | 1              | 1                  |
| 20192  | 27        | California Black Oak<br>( <i>Quercus kelloggii</i> )   | Some broken branches   | 1              | 1                  |
| 20701  | 10        | "Cleveland Select" Pear<br>( <i>Pyrus calleryana</i> ) |  | 1              | 1                  |
| 30000  | 8         | Maple ( <i>Acer sp.</i> )                              |  | 1              | 1                  |
| 200001 | 84        | Giant Sequoia<br>( <i>Sequoiadendron giganteum</i> )   | <b>OFFSITE</b>   | 1              | 1                  |



## Detailed Tree Inventory for Milwaukie High School

AKS Job No. 5859 - Tree Evaluation: April 6 & 11, 2017

| Tree # | DBH (in.) | Tree Species<br>Common Name<br>(Scientific name) | Comments                                 | Health Rating* | Structure Rating** |
|--------|-----------|--|--|----------------|--------------------|
| 200002 | 15        | Cherry ( <i>Prunus sp.</i> )                     | OFFSITE                                  | 1              | 1                  |
| 200003 | 6,6       | Cherry ( <i>Prunus sp.</i> )                     | OFFSITE; Lean (S); Short stature         | 1              | 2                  |
| 200004 | 8         | Cedar ( <i>Cedrus sp.</i> )                      | OFFSITE                                  | 1              | 1                  |
| 200005 | 12        | Maple ( <i>Acer sp.</i> )                        | OFFSITE                                  | 1              | 1                  |
| 200006 | 8         | Cherry ( <i>Prunus sp.</i> )                     | OFFSITE; Sparse foliage; Broken branches | 2              | 1                  |

**Total # of Existing Trees Inventoried = 68**

**\*Health Rating:**

1 = Good Health - A tree that exhibits typical foliage, bark, and root characteristics, for its respective species, shows no signs of infection or infestation, and has a high level of vigor and vitality.

2 = Fair Health - A tree that exhibits some abnormal health characteristics and/or shows some signs of infection or infestation, but may be reversed or abated with supplemental treatment.

3 = Poor Health - A tree that is in significant decline, to the extent that supplemental treatment would not likely result in reversing or abating its decline.

**\*\*Structure Rating:**

1 = Good Structure - A tree that exhibits typical physical form characteristics, for its respective species, shows no signs of structural defects of the canopy, trunk, and/or root system.

2 = Fair Structure - A tree that exhibits some abnormal physical form characteristics and/or some signs of structural defects, which reduce the structural integrity of the tree, but are not indicative of imminent physical failure, and may be corrected using arboricultural abatement methods.

3 = Poor Structure - A tree that exhibits extensively abnormal physical form characteristics and/or significant structural defects that substantially reduces the structural viability of the tree, cannot feasibly be abated, and are indicative of imminent physical failure.

**Arborist Disclosure Statement:**

Arborists are tree specialists who use their education, knowledge, training, and experience to examine trees, recommend measures to enhance the health of trees, and attempt to reduce the risk of living near trees. The Client and Jurisdiction may choose to accept or disregard the recommendations of the arborist, or seek additional advice. Arborists cannot detect every condition that could possibly lead to the structural failure of a tree. Trees are living organisms that fail in ways we do not fully understand. Conditions are often hidden within trees and below ground. Arborists cannot guarantee that a tree will be healthy or safe under all circumstances, or for a specified period of time. Likewise, remedial treatments, like medicine, cannot be guaranteed. Trees can be managed, but they cannot be controlled. To live near trees is to accept some degree of risk. The only way to eliminate all risk associated with trees is to eliminate all trees. Neither this author nor AKS Engineering & Forestry, LLC have assumed any responsibility for liability associated with the trees on or adjacent to this site.

At the completion of construction, all trees should once again be reviewed. Land clearing and removal of adjacent trees can expose previously unseen defects and otherwise healthy trees can be damaged during construction.

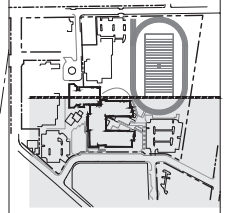


111 SW Fifth Ave., Suite 2400  
 Portland, OR 97204  
 O: 503.273.3234  
 F: 503.274-4661  
 www.kpff.com



**MILWAUKIE HIGH SCHOOL**

NORTH CLACKAMAS SCHOOL DISTRICT  
 2301 SE Willard Street, Milwaukie, OR 97222  
 t: (503) 353-5830  
 f: (503) 353-5846



|           |                     |
|-----------|---------------------|
| revisions |                     |
| phase     | LAND USE SET        |
| date      | 03/11/2019          |
| project   | 17010               |
|           | EXISTING CONDITIONS |

**C100**

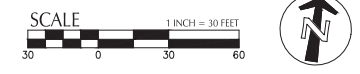
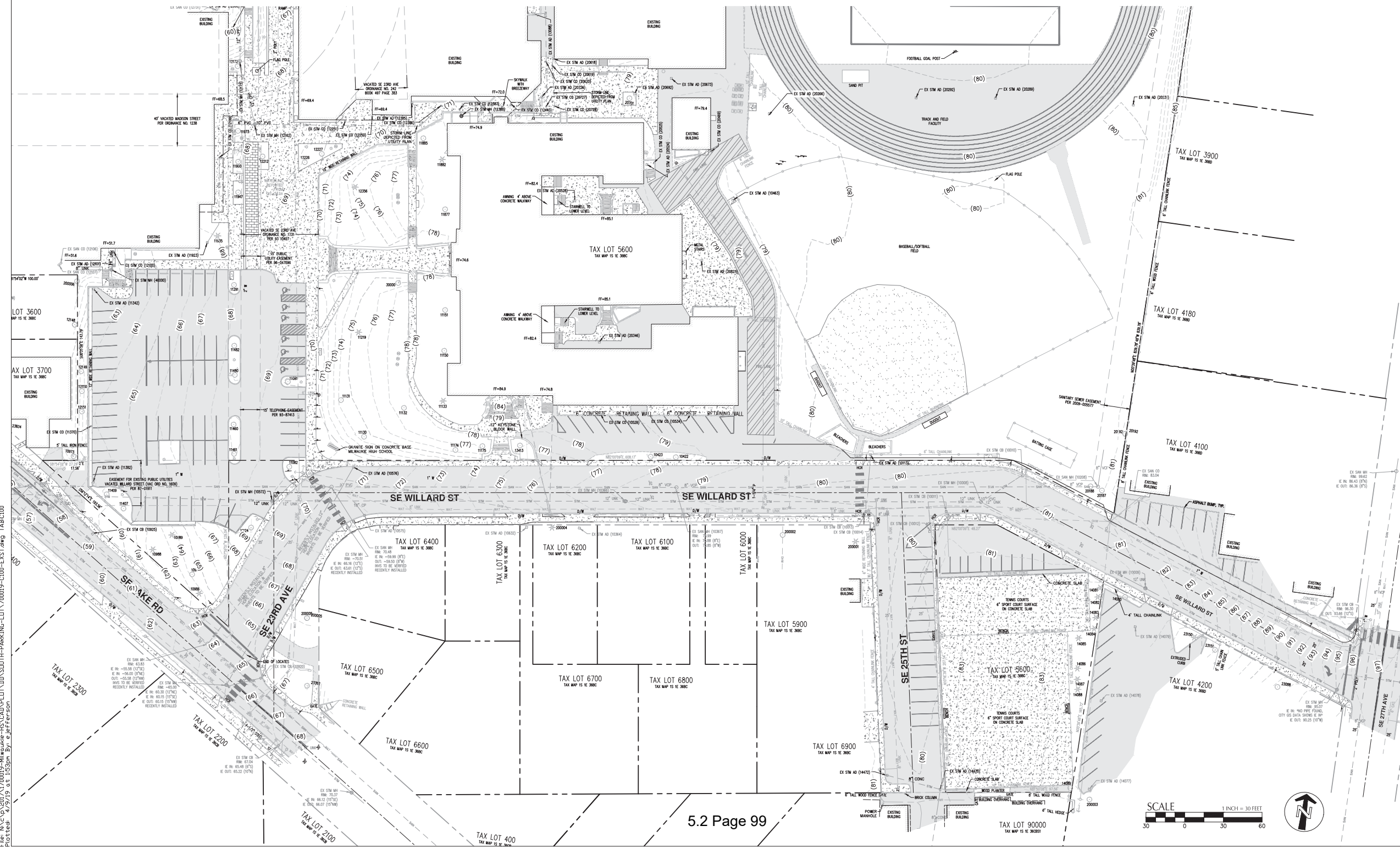
**NOTES:**

- UTILITIES SHOWN ARE BASED ON UNDERGROUND UTILITY LOCATE MARKINGS AS PROVIDED BY OTHERS, PROVIDED PER UTILITY LOCATE TICKET NUMBER 17005061, 1806844, 1806850, 18072916, AND 1807294. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND LOCATES REPRESENT THE ONLY UTILITIES IN THE AREA. CONTRACTORS ARE RESPONSIBLE FOR VERIFYING ALL EXISTING CONDITIONS PRIOR TO BEGINNING CONSTRUCTION.
- FIELD WORK WAS CONDUCTED MARCH 8-22, APRIL 10 & 14, AUGUST 14-18 2017, AND APRIL 9-12, APRIL 30-31 & MARCH 1, 2018.
- VERTICAL DATUM ELEVATIONS ARE NAVD88 DERIVED FROM THE TRIMBLE VRS NOW NETWORK. NETWORK ELEVATIONS WERE CHECKED AGAINST PUBLISHED DATASHEET ELEVATIONS AT NSG BENCHMARK NO. R01487, R01488, AND R01491.
- THIS MAP DOES NOT CONSTITUTE A PROPERTY BOUNDARY SURVEY.
- SURVEY IS ONLY VALID WITH SURVEYOR'S STAMP AND SIGNATURE.
- BUILDING FOOTPRINTS ARE MEASURED TO SIDING UNLESS NOTED OTHERWISE. CONTACT SURVEYOR WITH QUESTIONS REGARDING BUILDING TIES.
- CONTOUR INTERVAL IS 1 FOOT.
- TREES WITH DIAMETER OF 6" AND GREATER ARE SHOWN. TREE DIAMETERS WERE MEASURED UTILIZING A DIAMETER TAPE AT BREAST HEIGHT. TREE INFORMATION IS SUBJECT TO CHANGE UPON ARBORIST INSPECTION.
- HORIZONTAL DATUM: LOCAL DATUM PLANE SCALED FROM OREGON STATE PLANE NORTH 3601, NAD83(2011) EPOCH 2010.0000, BY HOLDING A PROJECT MEAN GROUND COMBINED SCALE FACTOR OF 1.0000945532 AT A CALCULATED CENTRAL PROJECT POINT WITH GRID VALUES OF (NORTH 654950.01, EAST 7654523.35). THE MERIDIAN CONVERGENCE ANGLE AT THE CALCULATED CENTRAL POINT IS -130'48". THE STATE PLANE COORDINATES WERE DERIVED FROM THE TRIMBLE VRS NOW NETWORK WITH HORIZONTAL POSITION CHECKED AGAINST PUBLISHED DATASHEET COORDINATES AT NSG CONTROL POINT PID R01491.

- CITY OF MILWAUKIE ORDINANCE NUMBER 242, BOOK 497 PAGE 383 IS BLANKET IN NATURE AND COVERS 23RD STREET 500 FEET FROM WASHINGTON ST. AND WILLARD ST. FROM LAKE ROAD EASTERLY TO HENNINGMAN ACRES.
- EASEMENTS FOR EXISTING PUBLIC UTILITIES IN VACATED STREET AREAS AND THE CONDITIONS IMPOSED THEREBY, BOOK 181 PAGE 94 DOES NOT DESCRIBE ANY UTILITIES OR EASEMENTS.

**LEGEND**

|                          |                         |                             |                     |
|--------------------------|-------------------------|-----------------------------|---------------------|
| <b>EXISTING</b>          | <b>EXISTING</b>         | <b>EXISTING</b>             | <b>EXISTING</b>     |
| DECIDUOUS TREE           | STORM SEWER CLEAN OUT   | GUY WIRE ANCHOR             | RIGHT-OF-WAY LINE   |
| CONIFEROUS TREE          | STORM SEWER CATCH BASIN | POWER POLE                  | BOUNDARY LINE       |
| FIRE HYDRANT             | STORM SEWER AREA DRAIN  | POWER JUNCTION BOX          | PROPERTY LINE       |
| WATER METER              | STORM SEWER MANHOLE     | POWER PEDESTAL              | CENTERLINE          |
| WATER VALVE              | GAS METER               | COMMUNICATIONS JUNCTION BOX | DITCH               |
| SANITARY SEWER CLEAN OUT | GAS VALVE               | COMMUNICATIONS RISER        | CURB                |
| SANITARY SEWER MANHOLE   | SIGN                    | MAILBOX                     | EDGE OF PAVEMENT    |
|                          | STREET LIGHT            |                             | EASEMENT            |
|                          |                         |                             | FENCE LINE          |
|                          |                         |                             | GRAVEL EDGE         |
|                          |                         |                             | POWER LINE          |
|                          |                         |                             | OVERHEAD WIRE       |
|                          |                         |                             | COMMUNICATIONS LINE |
|                          |                         |                             | FIBER OPTIC LINE    |
|                          |                         |                             | GAS LINE            |
|                          |                         |                             | STORM SEWER LINE    |
|                          |                         |                             | SANITARY SEWER LINE |
|                          |                         |                             | WATER LINE          |



File: N:\C:\p\2017\170019-Hiwaikie-HS-CAD\PL\LOT\_100-EXIST.dwg TAB:C100  
 Pictorial: 4/9/19 at 10:59am By: eJefferson

File: N:\CADD\2017\1700019-Milwaukie-HS-CADD\PLT\DD\SDUTH-PARKING-LDT\700019-C150-DEM.DWG TAB-C154  
 Plotted: 4/9/19 at 10:39pm By: eJFferson



**SHEET NOTES**

1. CONTRACTOR MAY STAGE WITHIN LIMITS OF DEMOLITION.
2. REMOVE ALL SITE COMPONENTS AND RECYCLE COMPONENTS AS REQUIRED IN THE SPECIFICATIONS.
3. GENERAL DEMOLITION PERMIT SHALL BE SECURED BY THE CONTRACTOR.
4. ALL TRADE LICENSES AND PERMITS NECESSARY FOR THE PROCUREMENT AND COMPLETION OF THE WORK SHALL BE SECURED BY THE CONTRACTOR PRIOR TO COMMENCING DEMOLITION.
5. THE CONTRACTOR SHALL PRESERVE AND PROTECT FROM DAMAGE ALL EXISTING RIGHT-OF-WAY SURVEY MONUMENTATION DURING DEMOLITION. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING AND PAYING FOR THE REPLACEMENT BY A LICENSED SURVEYOR OF ANY DAMAGED OR REMOVED MONUMENTS.
6. PROTECT ALL ITEMS ON ADJACENT PROPERTIES AND IN THE RIGHT OF WAY INCLUDING BUT NOT LIMITED TO SIGNAL EQUIPMENT, PARKING METERS, SIDEWALKS, STREET TREES, STREET LIGHTS, CURBS, PAVEMENT AND SIGNS. CONTRACTOR SHALL BE RESPONSIBLE FOR RESTORING ANY DAMAGED ITEMS TO ORIGINAL CONDITION.
7. PROTECT STRUCTURES, UTILITIES, SIDEWALKS, AND OTHER FACILITIES IMMEDIATELY ADJACENT TO EXCAVATIONS FROM DAMAGES CAUSED BY SETTLEMENT, LATERAL MOVEMENT, UNDERMINING, WASHOUT AND OTHER HAZARDS.
8. SAWCUT STRAIGHT LINES IN SIDEWALK, AS NECESSARY.
9. CONTRACTOR IS RESPONSIBLE TO CONTROL DUST AND MUD DURING THE DEMOLITION PERIOD, AND DURING TRANSPORTATION OF DEMOLITION DEBRIS. ALL STREET SURFACES OUTSIDE THE CONSTRUCTION ZONE MUST BE KEPT CLEAN.
10. ALL EXPOSED PORTIONS OF UNDERGROUND UTILITIES TO BE ABANDONED SHALL BE PLUGGED.
11. DEMOLITION REQUIREMENTS ARE SHOWN DIAGRAMMATICALLY ON THESE DRAWINGS. THE SHEETS MAY NOT SHOW EVERY SITE ELEMENT THAT MAY NEED TO BE REMOVED. CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING THE SCOPE OF DEMOLITION WORK AND FOR EXAMINING EXISTING SITE CONDITIONS.
12. SEE C710-C713 OF MILWAUKIE HIGH SCHOOL PERMIT SET FOR DEMOLITION WITHIN THE PUBLIC R.O.W.

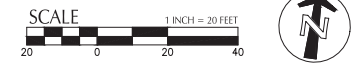
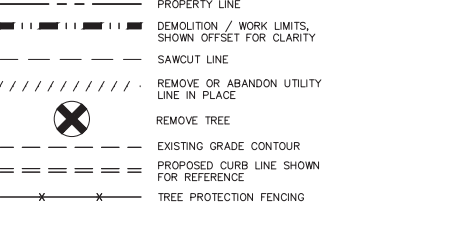
**X DEMOLITION KEY NOTES**

- 20 NOT USED
- 21 NOT USED
- 22 NOT USED
- 23 REMOVE EXISTING SIGNS. COORDINATE WITH PROPERTY OWNER.
- 24 NOT USED
- 25 NOT USED
- 26 REMOVE FENCE
- 27 REMOVE AREA DRAIN/CATCH BASIN AND CONNECTED INFRASTRUCTURE
- 28 SAWCUT EXISTING ASPHALT (REFERENCE SITE PLAN FOR EXACT LOCATION).
- 29 REMOVE EXISTING CONCRETE
- 30 REMOVE EXISTING WOODEN PLANTER
- 31 REMOVE EXISTING CURB
- 32 REMOVE EXISTING WALL
- 33 COORDINATE WITH POWER COMPANY TO REMOVE POWER POLE
- 34 REMOVE EXISTING BENCH
- 35 REMOVE EXISTING ASPHALT PAVEMENT
- 36 REMOVE MOVE GATE AND FENCE
- 37 REMOVE EXISTING STAIRS

**x PROTECTION KEY NOTES**

- 40 PROTECT TREE
- 41 PROTECT EXISTING BUILDING/STRUCTURE
- 42 PROTECT EXISTING FENCE/GATE
- 43 PROTECT EXISTING UTILITY

**SHEET LEGEND**



**BRIC**  
ARCHITECTURE, INC.

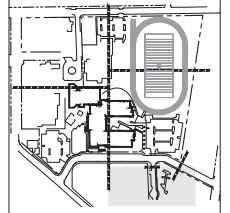
1233 NW Northrup Street  
Suite 100  
Portland, Oregon 97209  
tel. (503) 595 4900



**kpff**  
111 SW Fifth Ave., Suite 2400  
Portland, OR 97204  
O: 503.223.3251  
F: 503.274.4661  
www.kpff.com

**NORTH**  
Clackamas Schools

**MILWAUKIE HIGH SCHOOL**  
NORTH CLACKAMAS SCHOOL DISTRICT  
2301 SE Willard Street, Milwaukie, OR 97222  
t: (503) 353-5830  
f: (503) 353-5846



|                 |              |
|-----------------|--------------|
| key plan        |              |
| revisions       |              |
| phase           | LAND USE SET |
| date            | 03/11/2019   |
| project         | 17010        |
| DEMOLITION PLAN |              |
| <b>C154</b>     |              |



**SHEET NOTES**

1. ALL DIMENSIONS ARE TO FACE OF CURB OR FACE OF WALL (UNLESS OTHERWISE NOTED).
2. SEE LANDSCAPE PLANS FOR PLANTING AND ADDITIONAL SITE FEATURES.
3. SEE LANDSCAPE PLANS FOR ALL PEDESTRIAN HARDSCAPE ON THE LANDSCAPE ARCHITECT SIDE OF THE REFERENCE LINE INCLUDING CONCRETE SCORING AND JOINT LOCATIONS.
4. SEE ARCHITECTURAL PLANS FOR ADDITIONAL DIMENSIONS AND LAYOUT.
5. SEE LANDSCAPE PLANS FOR ALL FENCING HANDRAILS AND GATES.
6. SEE MILWAUKIE HIGH SCHOOL SHEETS C700-C802 FOR PROPOSED FRONTAGE IMPROVEMENTS IN RIGHT-OF-WAY, TO BE PERMITTED UNDER SEPARATE PERMIT.

**SHEET LEGEND**

- PROPERTY LINE
- [Pattern] LIGHT ASPHALT PAVEMENT SECTION (FULL IN PARKING)
- [Pattern] HEAVY ASPHALT PAVEMENT SECTION (DRIVE AISLE)
- [Pattern] CONCRETE SIDEWALK - SEE LANDSCAPE PLANS FOR ADDITIONAL DETAIL
- [Pattern] PERVIOUS CONCRETE - SWT10
- [Pattern] LANDSCAPE AREA - SEE LANDSCAPE PLANS FOR DETAILS
- SAWCUT LINE
- LIMITS OF WORK - OFFSET FOR CLARITY
- ⊙ SITE LIGHTING (SEE SHEET EPH1 FOR PHOTOMETRICS)
- ⊕ PROPOSED TREE
- MATCHLINE
- RED PAINTED CURB

**KEY NOTES**

- | #  | DESCRIPTION                                     |
|----|---|
| 1  | SETBACK DRIVEWAY APPROACH                       |
| 2  | STANDARD CURB                                   |
| 3  | CURB ENDING TAPER TO FLUSH                      |
| 4  | WHEEL STOP                                      |
| 5  | CONCRETE SIDEWALK                               |
| 5a | PERVIOUS CONCRETE SIDEWALK                      |
| 6b | "NO PARKING" ZONE STRIPING                      |
| 6c | 4" PARKING STRIPING, TYPICAL                    |
| 9  | CURB RAMP                                       |
| 11 | FLUSH CONCRETE CROSSING/DRIVEWAY APRON          |
| 15 | SITE LIGHTING - SEE SHEET EPH1 FOR PHOTOMETRICS |
| 19 | FLUSH CURB                                      |
| 20 | HEIGHTENED CURB (MAX 18")                       |
| 32 | PROVIDE STOP BAR                                |
| 33 | CARPOOL PARKING SIGN                            |
| 34 | PROVIDE DIRECTIONAL ARROWS                      |
| 35 | SEE LANDSCAPE PLANS FOR PLANTING INFORMATION    |
| 37 | STOP SIGN                                       |
| 38 | DO NOT ENTER SIGN                               |

**BRIC**  
ARCHITECTURE, INC.

1233 NW Northrup Street  
Suite 100  
Portland, Oregon 97209  
tel. (503) 595 4900



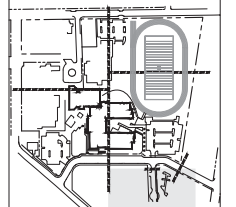
**kpff**

111 SW Fifth Ave., Suite 2400  
Portland, OR 97204  
t: 503.223.3251  
f: 503.274.4661  
www.kpff.com

**NORTH**  
Clackamas Schools

**MILWAUKIE HIGH SCHOOL**

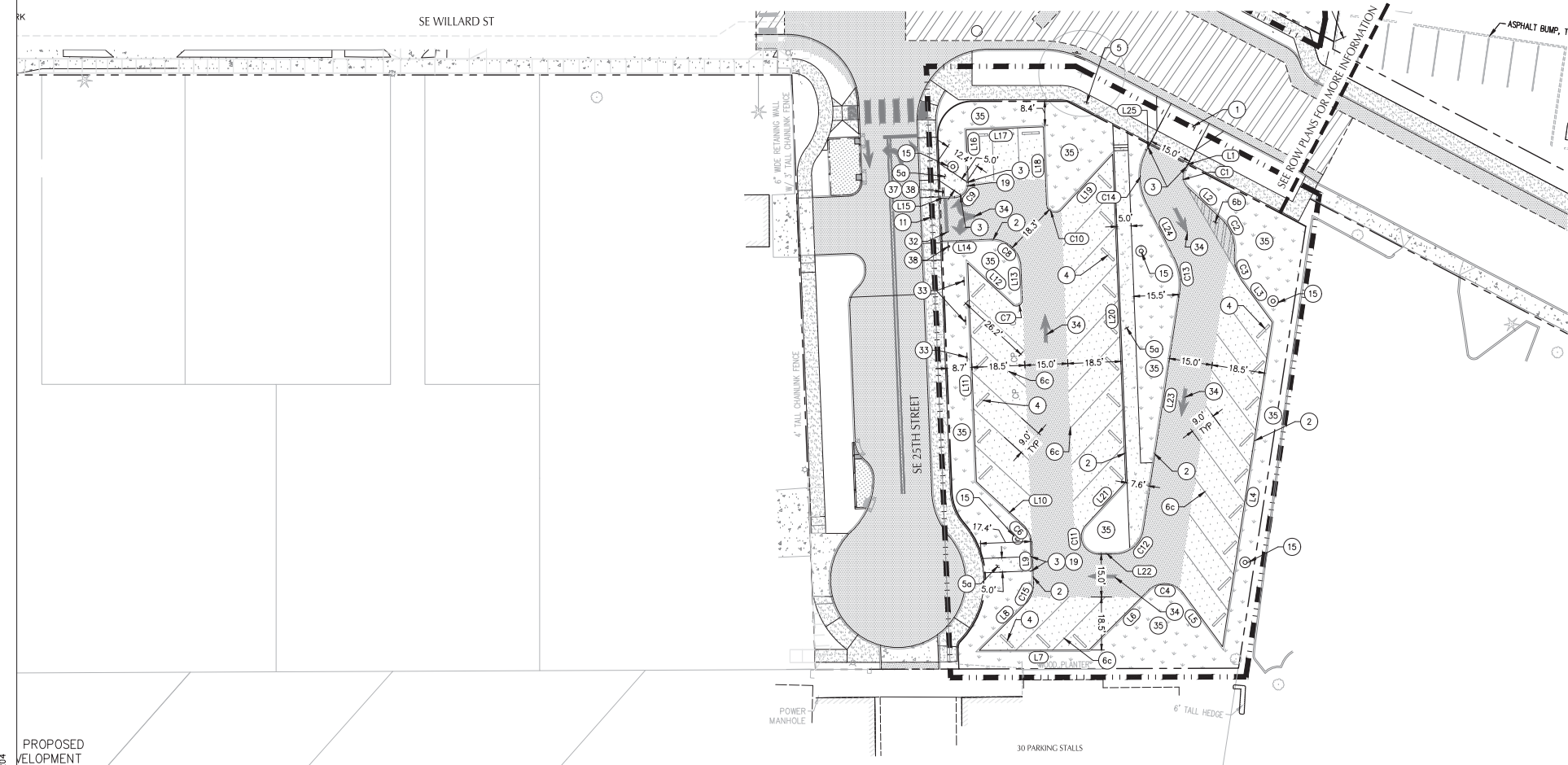
NORTH CLACKAMAS SCHOOL DISTRICT  
2301 SE Willard Street, Milwaukie, OR 97222  
t: (503) 353-5830  
f: (503) 353-5846



key plan

| revisions | phase                             | date       | project |
|-----------|-----------------------------------|------------|---------|
|           | LAND USE SET                      | 03/11/2019 | 17010   |
|           | SITE AND HORIZONTAL CONTROL PLANS |            |         |

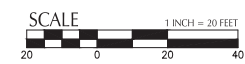
**C204**



| LINE TABLE |         |                  |
|------------|---------|------------------|
| LINE NO.   | LENGTH  | DIRECTION        |
| L1         | 2.74'   | S19° 40' 20.00"W |
| L2         | 15.97'  | S54° 41' 48.24"E |
| L3         | 17.28'  | S44° 17' 29.89"E |
| L4         | 114.55' | S0° 42' 30.11"W  |
| L5         | 23.25'  | N44° 17' 29.89"W |
| L6         | 29.42'  | S36° 54' 36.97"W |
| L7         | 38.23'  | S81° 50' 06.00"W |
| L8         | 22.68'  | N36° 54' 36.97"E |
| L9         | 13.47'  | N10° 16' 38.73"W |
| L10        | 22.02'  | N55° 16' 38.73"W |
| L11        | 76.37'  | N10° 16' 38.73"W |
| L12        | 21.33'  | S55° 16' 38.73"E |
| L13        | 11.65'  | N10° 16' 38.73"W |
| L14        | 17.73'  | S79° 43' 21.00"W |
| L15        | 5.50'   | S79° 43' 21.00"W |
| L16        | 18.51'  | S10° 16' 38.73"E |
| L17        | 27.00'  | S79° 43' 21.27"W |
| L18        | 27.02'  | N10° 16' 38.73"W |
| L19        | 26.32'  | S34° 43' 21.27"W |
| L20        | 114.55' | N10° 16' 38.73"W |

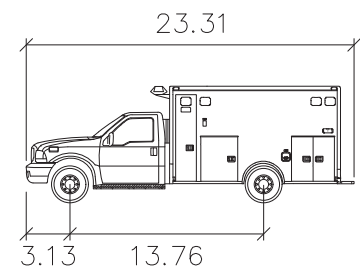
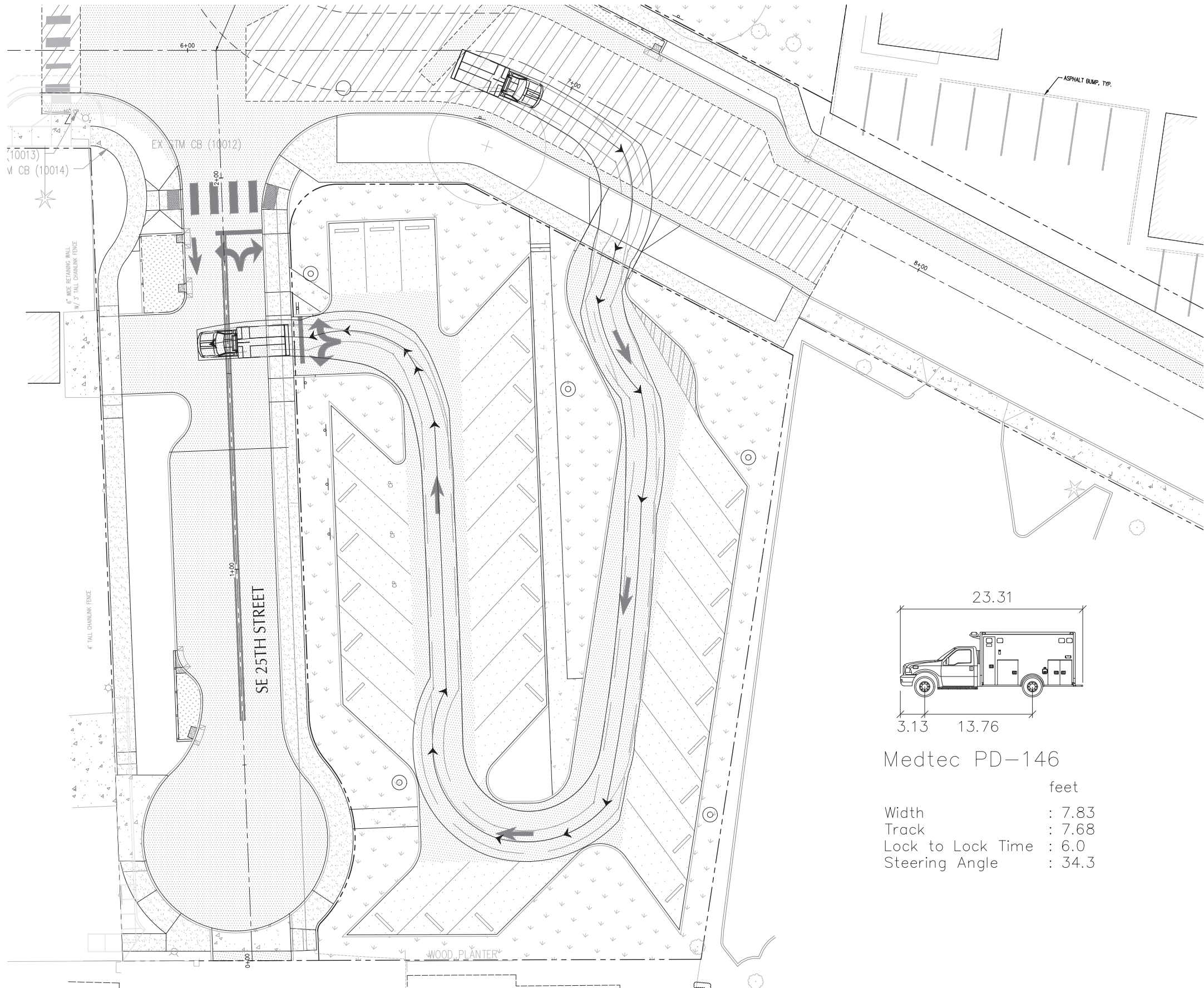
| LINE TABLE |        |                  |
|------------|--------|------------------|
| LINE NO.   | LENGTH | DIRECTION        |
| L21        | 20.44' | N34° 43' 21.27"E |
| L22        | 5.78'  | S81° 50' 08.00"W |
| L23        | 83.58' | S0° 42' 30.11"W  |
| L24        | 21.53' | S34° 43' 56.24"E |
| L25        | 2.74'  | S19° 40' 20.00"W |

| CURVE DATA TABLE |        |        |            |
|------------------|--------|--------|------------|
| CURVE NO.        | LENGTH | RADIUS | DELTA      |
| C1               | 6.89'  | 5.00'  | 78°56'39"  |
| C2               | 12.83' | 14.50' | 50°40'46"  |
| C3               | 11.03' | 15.50' | 40°45'53"  |
| C4               | 13.79' | 8.00'  | 98°47'53"  |
| C6               | 7.85'  | 10.00' | 45°00'00"  |
| C7               | 4.71'  | 2.00'  | 135°00'00" |
| C8               | 15.71' | 10.00' | 90°00'00"  |
| C9               | 7.85'  | 5.00'  | 90°00'00"  |
| C10              | 7.07'  | 3.00'  | 135°00'00" |
| C11              | 13.92' | 6.00'  | 132°5'13"  |
| C12              | 14.16' | 10.00' | 81°07'38"  |
| C13              | 12.68' | 20.50' | 35°26'26"  |
| C14              | 18.99' | 20.00' | 54°24'16"  |
| C15              | 8.24'  | 10.00' | 47°11'16"  |



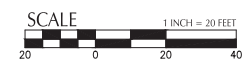
File: N:\C:\p\2017\1700019-Milwaukie-HS-CAD\PL\DT\DD\SD\TH-PARKING-LDT\700019-C200-SITE.dwg TAB: C204  
Plotted: 4/9/19 at 1:54pm By: cjefferson

File: N:\CADD\2017\1700019-Milwaukie-HS-CADD\PLT\DD\SDUTH-PARKING-LDT\700019-C215-AUTO-TURN.dwg TAB: C215  
 Plotted: 4/9/19 at 1:54pm By: eJefferson



Medtec PD-146

|                   |        |
|-------------------|--------|
|                   | feet   |
| Width             | : 7.83 |
| Track             | : 7.68 |
| Lock to Lock Time | : 6.0  |
| Steering Angle    | : 34.3 |

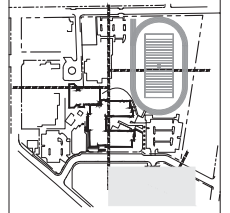


111 SW Fifth Ave., Suite 2400  
Portland, OR 97204  
O: 503.273.3231  
F: 503.274.4661  
www.kpff.com



**MILWAUKIE HIGH SCHOOL**

NORTH CLACKAMAS SCHOOL DISTRICT  
2301 SE Willard Street, Milwaukie, OR 97222  
t: (503) 353-5830  
f: (503) 353-5846



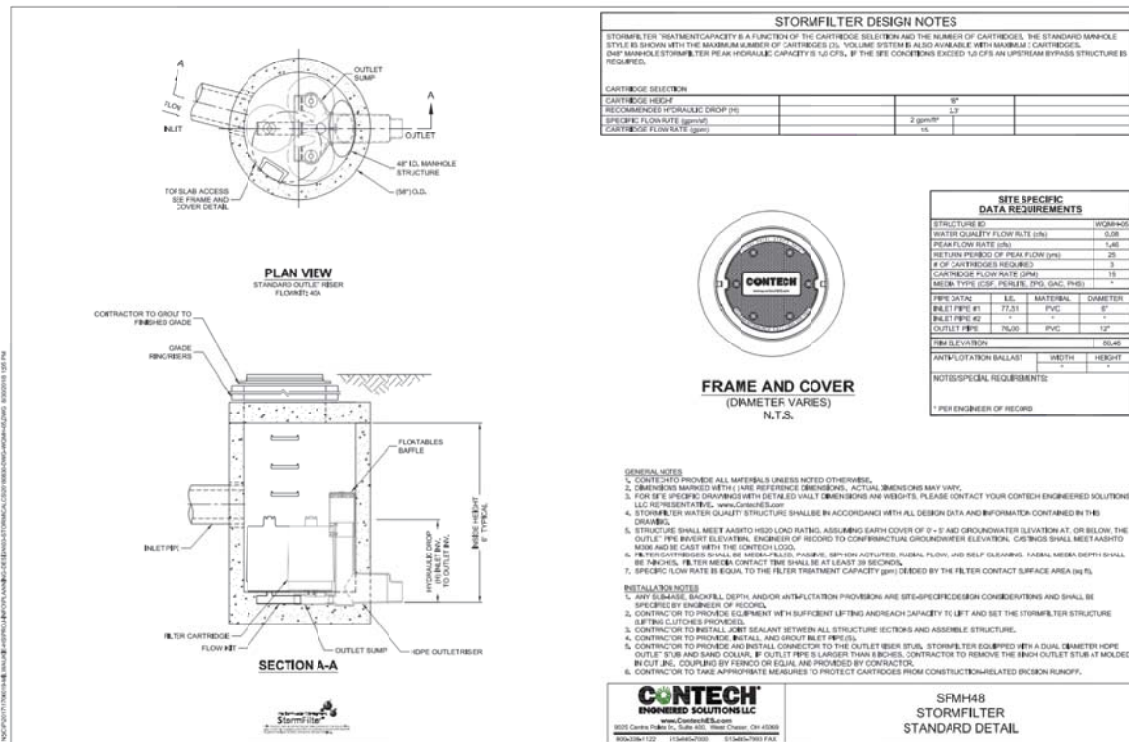
key plan

|           |              |
|-----------|--------------|
| revisions |              |
| phase     | LAND USE SET |
| date      | 03/11/2019   |
| project   | 17010        |

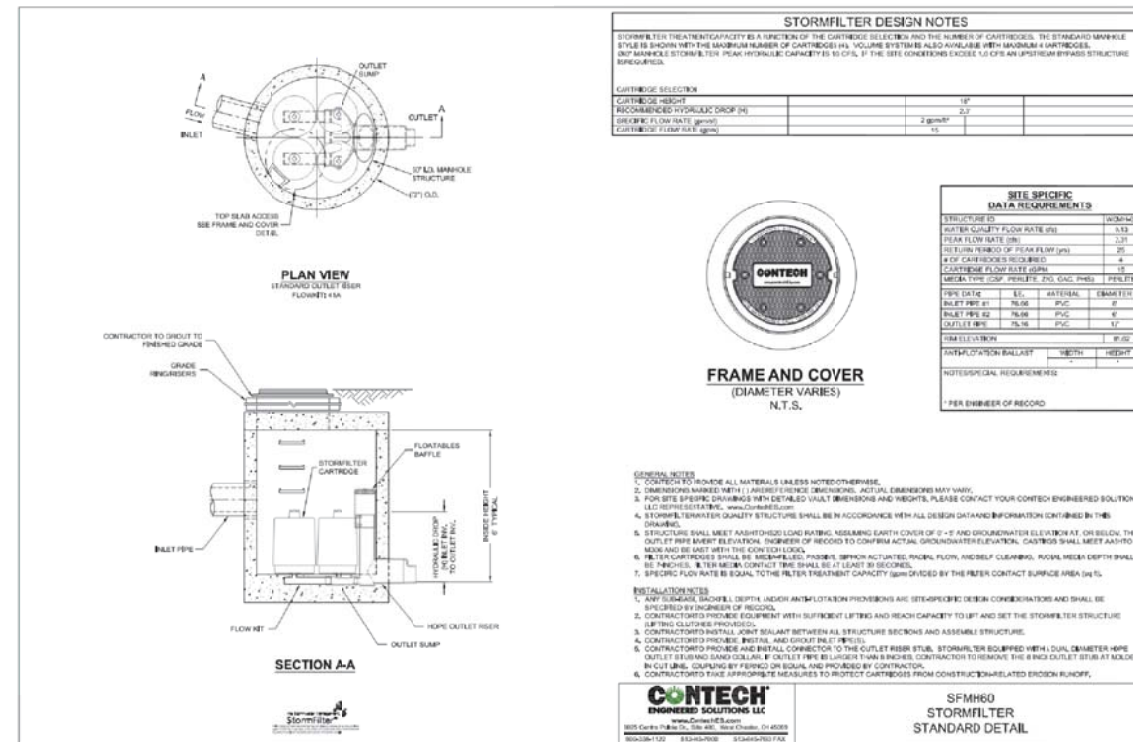
CIRCULATION PLAN



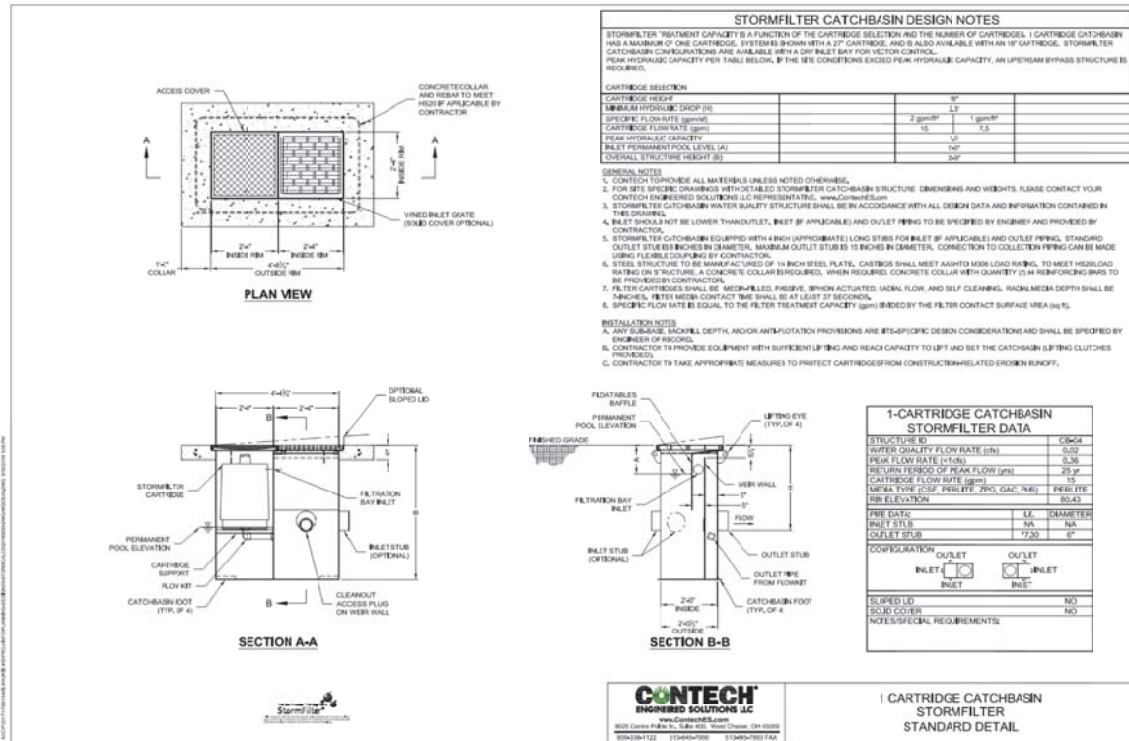




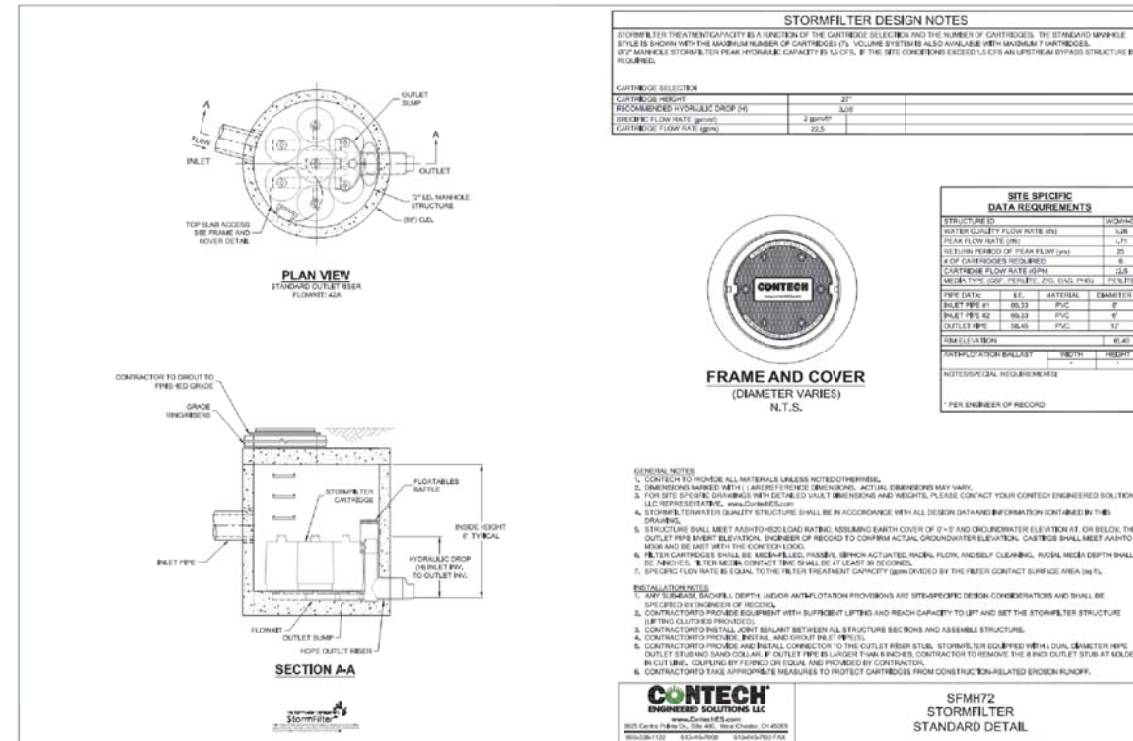
3 WATER QUALITY FILTER MANHOLE WQMH-05  
SCALE: NTS



1 WATER QUALITY STORM FILTER MANHOLE WQMH-01  
SCALE: NTS



4 WATER QUALITY FILTER CATCH BASIN CB-04  
SCALE: NTS



2 WATER QUALITY FILTER MANHOLE WQMH-03  
SCALE: NTS

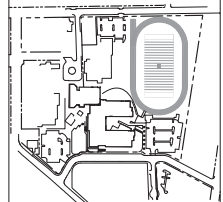


111 SW Fifth Ave., Suite 2400  
Portland, OR 97204  
P: 503.321.3331  
F: 503.774.4681  
[www.kpff.com](http://www.kpff.com)



**MILWAUKIE HIGH SCHOOL**

NORTH CLACKAMAS SCHOOL DISTRICT  
2301 SE Willard Street, Milwaukie, OR 97222  
t: (503) 353-5830  
f: (503) 353-5846

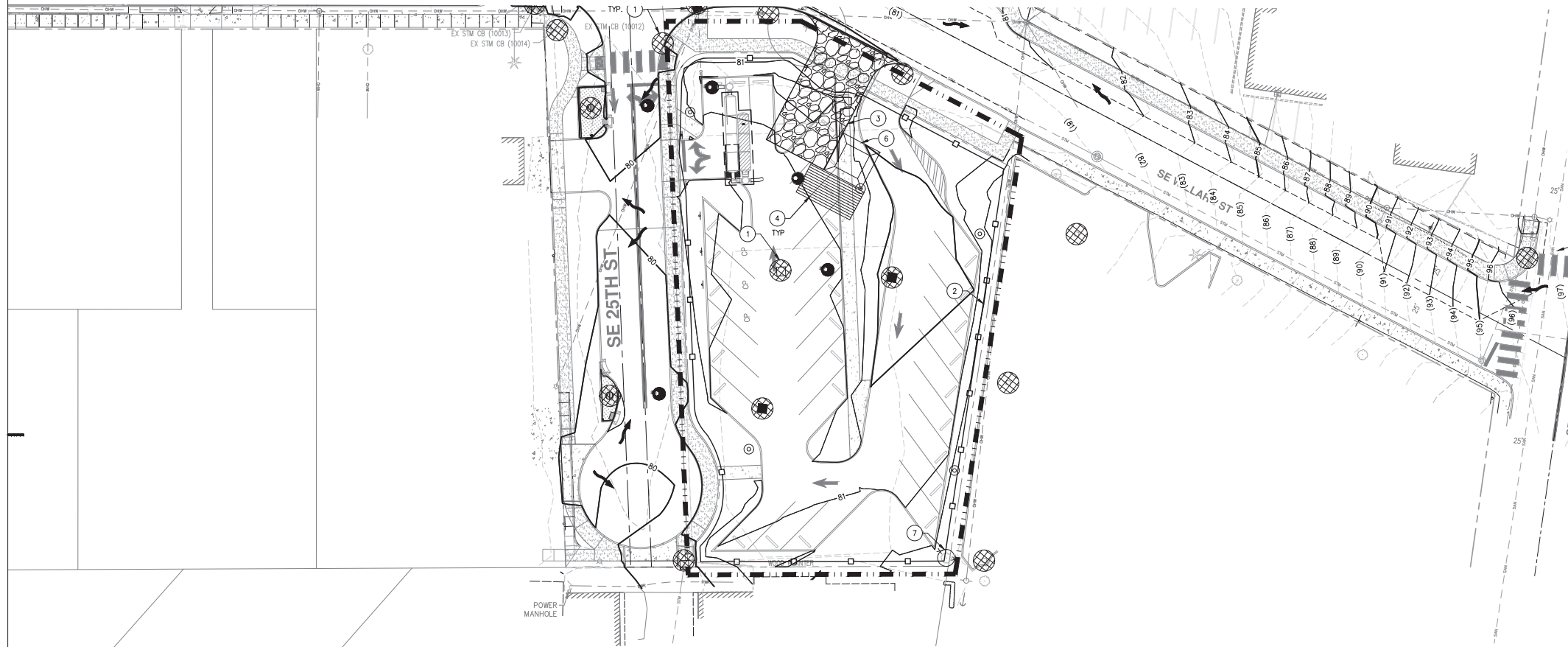


|           |                            |
|-----------|----------------------------|
| revisions |                            |
| phase     | 100% CD - IFC - PERMIT SET |
| date      | 11/26/2018                 |
| project   | 171010                     |

ONSITE DETAILS

**C605**

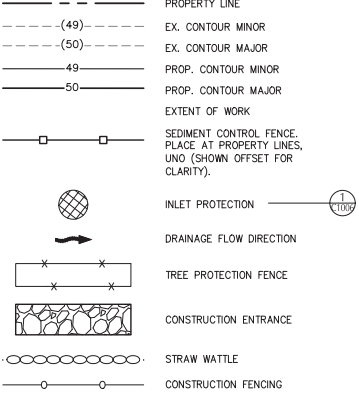
11/20/2017 9:08 AM  
 P:\2017\17-00019-Milwaukie-HS-CAD\PL\DT\DD\SDUTH-PARKING-LDT\700019-C1001-ESC.dwg TABC1005  
 Plotted: 4/9/19 at 1:56pm By: eJefferson



**SHEET NOTES**

- 1 SEE C1000 FOR NOTES.
  - 2 ALL STOCKPILED TOPSOIL MUST BE COVERED DETAIL PER 7/C1006.
  - 3 CONTRACTOR TO INSTALL AND MOVE CONSTRUCTION FENCING AS PROJECT PHASING REQUIRES.
- (X) **KEY NOTES**
- 1 INLET PROTECTION BARRIER OVER EXISTING CATCH BASIN OR AREA DRAIN
  - 2 SEDIMENT FENCE
  - 3 CONSTRUCTION ENTRANCE
  - 4 TIRE WASH
  - 5 STRAW WATTLE
  - 6 CONCRETE WASHOUT
  - 7 TREE PROTECTION FENCE
  - 8 INSTALL GEOTEXTILE FABRIC UNDER COMMERCIAL CONSTRUCTION ENTRANCE PER DETAIL 4/C1006.
  - 9 CONSTRUCTION FENCING

**SHEET LEGEND**



**BRIC**  
 ARCHITECTURE, INC.

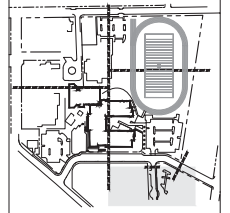
1233 NW Northrup Street  
 Suite 100  
 Portland, Oregon 97209  
 tel. (503) 595 4900



**kpff**  
 111 SW Fifth Ave., Suite 2400  
 Portland, OR 97204  
 O: 503.223.3231  
 F: 503.274.4661  
 www.kpff.com

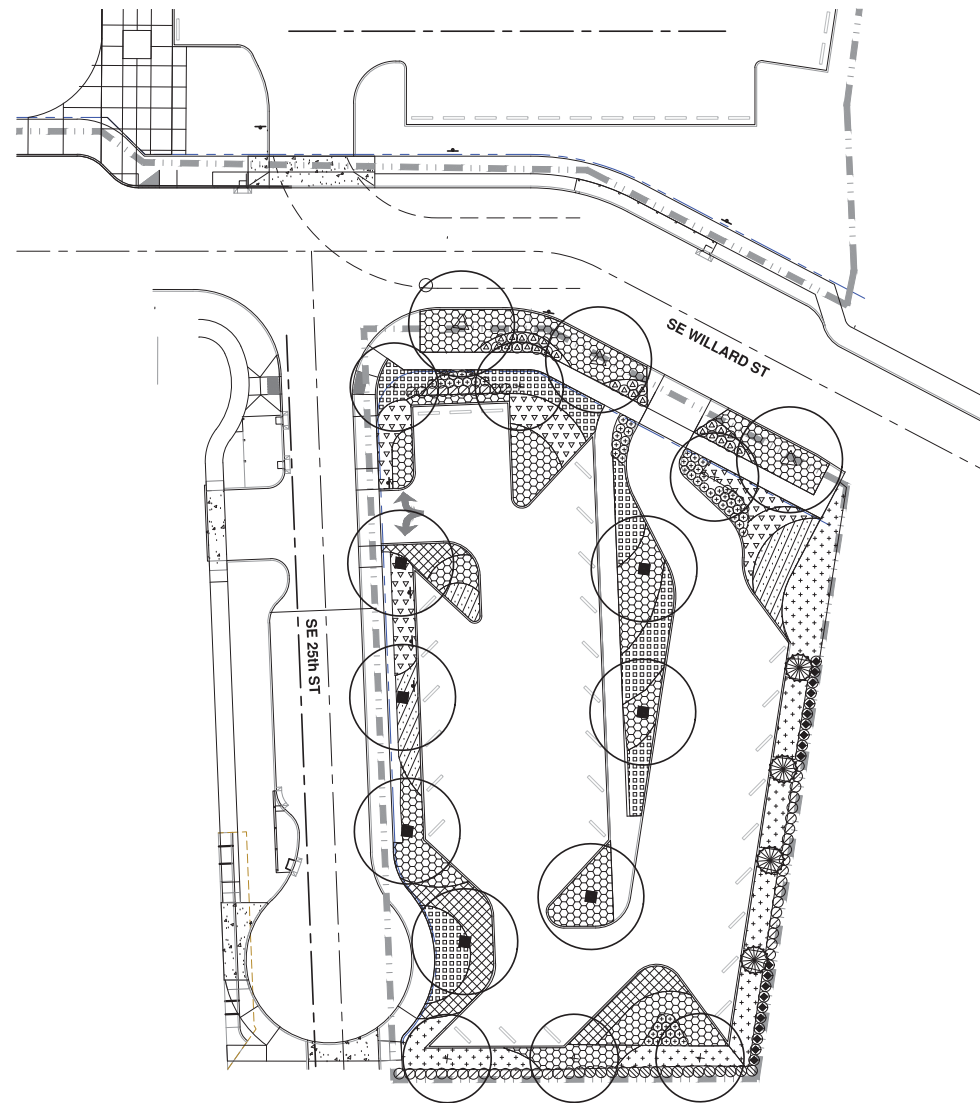


**MILWAUKIE HIGH SCHOOL**  
 NORTH CLACKAMAS SCHOOL DISTRICT  
 2301 SE Willard Street, Milwaukie, OR 97222  
 t: (503) 353-5830  
 f: (503) 353-5846



|                                 |              |
|---------------------------------|--------------|
| revisions                       | ---          |
| phase                           | LAND USE SET |
| date                            | 03/11/2019   |
| project                         | 17010        |
| EROSION & SEDIMENT CONTROL PLAN |              |

**C1005**



**PLANT SCHEDULE**

**TREES**

- 6 Prunus x yedoensis 'Akebono' - Akebono Cherry  
2" CAL/BASE, WELL BRANCHED, LIMBED TO 6'
- 7 Crataegus bitorquata 'Saratoga' - Saratoga Quince  
2" CAL/BASE, WELL BRANCHED, LIMBED TO 6'
- 3 Quercus kelloggii - American Yellowwood  
2" CAL/BASE, WELL BRANCHED, LIMBED TO 6'
- 4 Chamaecyparis obtusa 'Gracilis' - Hinoki Cypress  
6" HT/BASE, WELL BRANCHED

**SHRUBS**

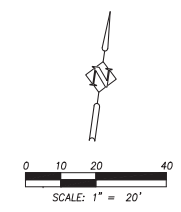
- 62 Cornus sericea 'Kelsey' - Kelsey Dogwood  
1 GAL/CONT., FULL PLANTS, SPACING AS SHOWN
- 17 Daphne x barkwoodii 'Carol Mackie' - Carol Mackie Daphne  
2 GAL/CONT., FULL PLANTS, SPACING AS SHOWN
- 27 Escallonia x 'Newport Dwarf' - Compact Red Escallonia  
1 GAL/CONT., FULL PLANTS, SPACING AS SHOWN
- 46 Viburnum davidii - David Viburnum  
1 GAL/CONT., FULL PLANTS, SPACING AS SHOWN
- 28 Choisya ternata - Mexican Orange  
1 GAL/CONT., FULL PLANTS, SPACING AS SHOWN
- 20 Nandina domestica 'Tyrone' - Heavenly Bamboo  
5 GAL/CONT., FULL PLANTS, SPACING AS SHOWN

**GROUNDCOVER**

- 1,447 Deschampsia cespitosa - Tufted Hair Grass  
1 GAL/CONT., FULL PLANTS, 18" O.C.
- 340 Schizanthus scaberrimus 'Standing Ovation' - Little Bluestem  
1 GAL/CONT., FULL PLANTS, 24" O.C.
- 626 Festuca glauca 'Elijah Blue' - Elijah Blue Fescue  
1 GAL/CONT., FULL PLANTS, 12" O.C.
- 209 Pennisetum alopecuroides 'Hameln' - Fountain Grass  
1 GAL/CONT., FULL PLANTS, 24" O.C.
- 157 Helictotrichon sempervirens - Blue Oat Grass  
1 GAL/CONT., FULL PLANTS, 30" O.C.
- 442 Mahonia repens - Creeping Oregon Grape  
1 GAL/CONT., FULL PLANTS, 24" O.C.

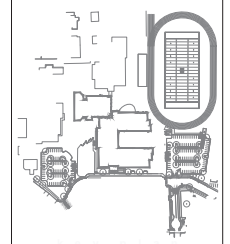
**GENERAL PLANTING NOTES**

1. ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH CURRENT CITY OF MILWAUKIE STANDARDS AND THE OREGON UNIFORM BUILDING CODES.
2. INSTALL EROSION CONTROL SYSTEMS IN ACCORDANCE WITH CITY OF MILWAUKIE STANDARDS PRIOR TO SITE WORK AND LANDSCAPE INSTALLATION.
3. MARK AND PROTECT ALL UTILITIES, SITE FEATURES, AND VEGETATION TO REMAIN IN PLACE.
4. REMOVE EXISTING TOP SOILS AND STOCKPILE PER SPECIFICATIONS. AMEND TOP SOILS IN ACCORDANCE WITH LANDSCAPE PLANS AND SPECIFICATIONS PRIOR TO PLACEMENT IN PLANTING AREAS.
5. LANDSCAPE INSTALLATION SHALL INCLUDE PROVISION OF AN AUTOMATIC IRRIGATION SYSTEM DESIGNED TO COMPLIMENT PLANTING MEETING LOCAL AND STATE BUILDING CODES.
6. PLANT MATERIAL INSTALLED SHALL CONFORM IN SIZE AND GRADE TO THE "AMERICAN STANDARD FOR NURSERY STOCK" CURRENT EDITION.
7. THE QUANTITIES OF PLANT MATERIALS SHALL BE AS DETERMINED BY THE CONTRACTOR IN ACCORDANCE WITH THE SPECIFIED SPACING OR LOCATION ON THE PLAN. MATERIAL QUANTITIES SHOWN ON PLAN ARE FOR CONTRACTOR CONVENIENCE ONLY AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO INSTALLATION. SURPLUS OR SHORTAGES OF PLANT QUANTITIES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
8. LANDSCAPE CONTRACTOR SHALL MAINTAIN PLANTINGS FOR THE DURATION OF THE 1 YEAR WARRANTY PERIOD AFTER SUBSTANTIAL COMPLETION AND GUARANTEE ALL PLANTINGS TO BE IN SATISFACTORY AND VIGOROUS HEALTH. PROVIDE MAINTENANCE SCHEDULE PER SPECIFICATIONS.



**MILWAUKIE HIGH SCHOOL**

NORTH CLACKAMAS SCHOOL DISTRICT  
 2301 SE Willard Street, Milwaukie, OR 97222  
 t: (503) 353-5830  
 f: (503) 353-5846



|           |                   |
|-----------|-------------------|
| revisions |                   |
| phase     | LAND USE          |
| date      | SUBMITTAL         |
| project   | 03/15/2019        |
|           | 17010             |
|           | PLANTING PLAN -   |
|           | SOUTH PARKING LOT |

**L1.0**



**MILWAUKIE HIGH SCHOOL**

NORTH CLACKAMAS SCHOOL DISTRICT  
2301 SE Willard Street, Milwaukie, OR 97222  
t: (503) 353-5830  
f: (503) 353-5846

| LAND USE - SOUTH PARKING LOT LUMINAIRE SCHEDULE |  |   |                                     |   |             |        |              |                                    |             |  |
|---|--|---|-------------------------------------|---|-------------|--------|--------------|------------------------------------|-------------|--|
| TYPE  | DESCRIPTION  | HOUSING   | SHIELDING                           | MOUNTING  | FINISH      | UL/IPC | DRIVER/POWER | LAMP(S)                            | INPUT WATTS | MFG/CATALOG #                                      |
|   |  |   |                                     |   |             | RATING | SUPPLY       |                                    |             |  |
| SA-2  | POST TOP LED AREA LUMINAIRES TO MATCH EXISTING. TYPE II DISTRIBUTION | NOMINAL 18-INCH DIAMETER BY OVERALL HEIGHT OF 32-INCH DIE-CAST ALUMINUM | PRECISION ACRYLIC REFRACTIVE OPTICS | 18-FOOT HIGH, STRAIGHT STEEL ROUND POLE. POLE TO WITHSTAND 100 MILE PER HOUR WINDS WITH A GUST FACTOR OF 1.3. ABOVE ROUND CONCRETE PEDESTAL | DARK BRONZE |        |              | NOMINAL 6685 LUMENS, 4000K, 80 CRI | 75 WATTS    | LITHONIA LIGHTING MRP LED SERIES TO MATCH EXISTING |
| SA-5  | POST TOP LED AREA LUMINAIRES TO MATCH EXISTING. TYPE V DISTRIBUTION  | NOMINAL 18-INCH DIAMETER BY OVERALL HEIGHT OF 32-INCH DIE-CAST ALUMINUM | PRECISION ACRYLIC REFRACTIVE OPTICS | 18-FOOT HIGH, STRAIGHT STEEL ROUND POLE. POLE TO WITHSTAND 100 MILE PER HOUR WINDS WITH A GUST FACTOR OF 1.3. ABOVE ROUND CONCRETE PEDESTAL | DARK BRONZE |        |              | NOMINAL 6685 LUMENS, 4000K, 80 CRI | 75 WATTS    | LITHONIA LIGHTING MRP LED SERIES TO MATCH EXISTING |

TYPE SA & EXISTING

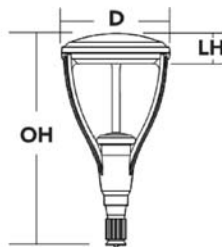


**MRP LED**  
LED Area Luminaire

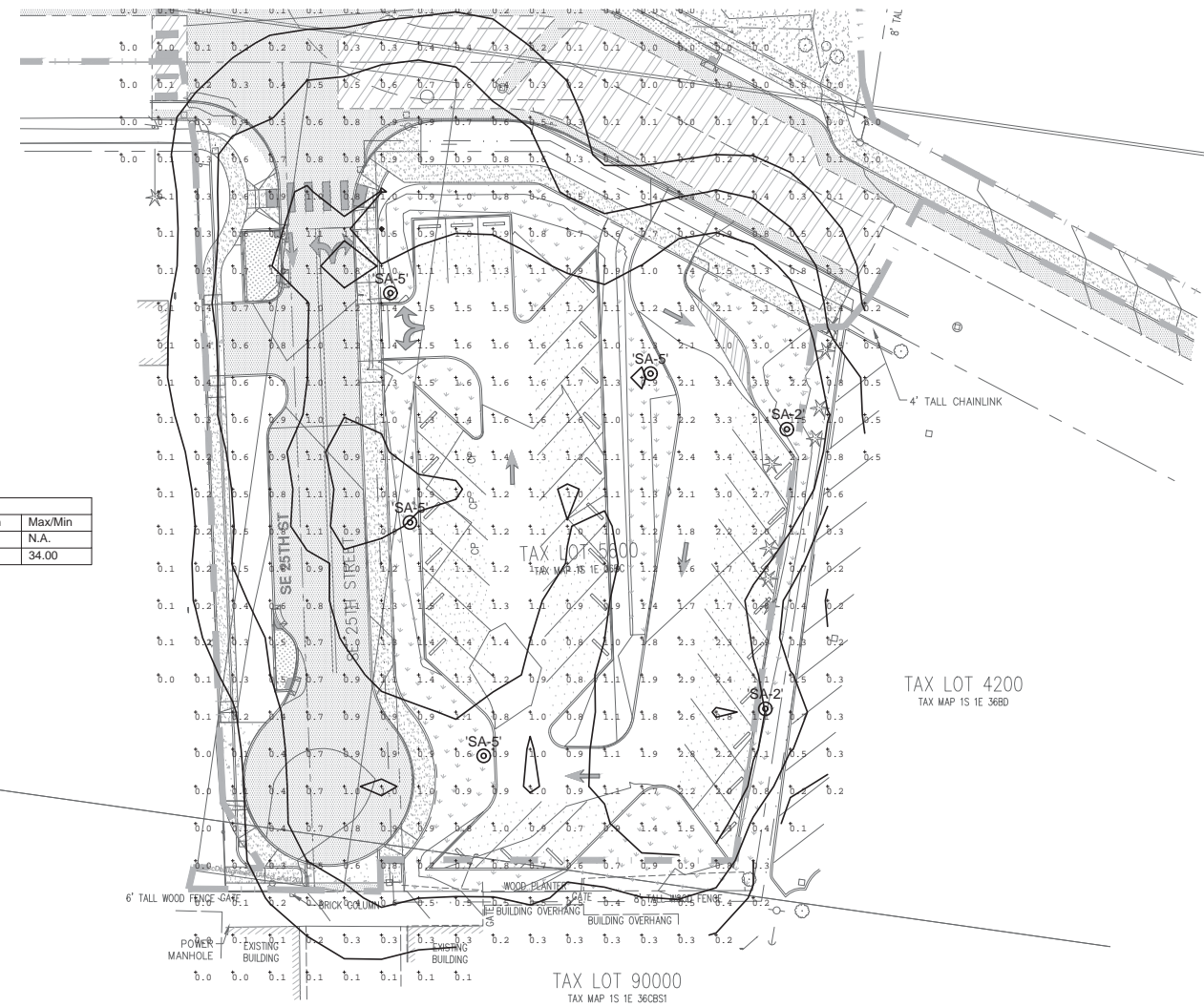


**Specifications**

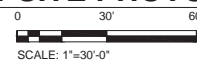
- EPA:** 1.125 ft<sup>2</sup> (0.105 m<sup>2</sup>)
- Luminaire Height:** 6-3/8" (16.2 cm)
- Overall Height:** 32" (81.3 cm)
- Diameter:** 18" (45.7 cm)
- Weight (max):** 37.5 lbs (17 kg)



| Calculation Summary |             |       |      |     |     |         |         |
|---------------------|-------------|-------|------|-----|-----|---------|---------|
| Label               | CalcType    | Units | Avg  | Max | Min | Avg/Min | Max/Min |
| Overall Site        | Illuminance | Fc    | 0.79 | 3.4 | 0.0 | N.A.    | N.A.    |
| South East Parking  | Illuminance | Fc    | 1.06 | 3.4 | 0.1 | 10.60   | 34.00   |



**1 SITE PHOTOMETRICS - SOUTH PARKING LOT LIGHTING**



SCALE: 1"=30'-0"

key plan

| revisions |  |
|-----------|--|
|           |  |

| phase   | LAND USE SET |
|---------|--------------|
| date    | 03/15/2019   |
| project | 17010        |

SITE PHOTOMETRICS - LIGHTING

**EPH1**

PROJECT 2017-0517  
CONTACT Deborah Raines  
**INTERFACE**  
ENGINEERING  
100 SW Main St.  
Suite 1600  
Portland, OR 97204  
TEL 503.382.2266  
FAX 503.382.2262  
www.interfaceengineering.com



**Brett Kelter**

---

**From:** Wright, Jodi <Jodi.Wright@nwnatural.com>  
**Sent:** Friday, March 29, 2019 9:22 AM  
**To:** Brett Kelter  
**Subject:** FW: CSU-2019-002 Application Referral  
**Attachments:** Planning Referral\_CSU-2019-002.pdf

NW Natural has a 1" steel main 13 feet south of the North Property Line of SE Willard. There is/was a service addressed as 11300 that is or was feeding the primary building on site. These need to be protected during construction activities. If there are on site concerns or questions please contact Field Tech Jim Sweany at 971-280-0256.

Kind Regards,

**Jodi Wright**

NW Natural – East Metro Resource Engineer  
(Parkrose, Mt Scott and Central)  
w: 503.226.4211 x6745 m: 503.367-4984  
[Jodi.wright@nwnatural.com](mailto:Jodi.wright@nwnatural.com)

---

**From:** Patty Stewart [mailto:StewartP@milwaukieoregon.gov]  
**Sent:** Thursday, March 28, 2019 11:31 AM  
**To:** Alma Flores; Alex Roller; Samantha Vandagriff; Ann Ober; Boumann; izak.hamilton@clackamasfire.com; Ray Bryan; Heavytech77@gmail.com; kenken@co.clackamas.or.us; Paulette.Copperstone@oregonmetro.gov; developmentreview@trimet.org; Wright, Jodi; Dennis Egner; Steve Bartol; Peter Passarelli; Justin Gericke  
**Subject:** [External]CSU-2019-002 Application Referral

Greetings,

Please see the attached Application Referral for land use application **CSU-2019-002** for 2301 SE Willard St (Milwaukie High School)  
The link to the application webpage with the application materials is below.

Comments for this application are due by **April 11, 2019**. Please respond in writing.

If you have any questions, please feel free to contact Brett Kelter at 503-786-7657 or [kerverb@milwaukieoregon.gov](mailto:kerverb@milwaukieoregon.gov)

[www.milwaukieoregon.gov/planning/csu-2019-002](http://www.milwaukieoregon.gov/planning/csu-2019-002)

Thank you,

**Patty Stewart**

Administrative Specialist II, Community Development  
City of Milwaukie  
o: 503.786.7600 d: 503.786.7634  
6101 SE Johnson Creek Blvd • Milwaukie, OR 97206

 Please consider our environment before printing this email.

## MEMORANDUM

**TO:** Community Development Department  
**THROUGH:** Kelly Brooks, Acting Director of Engineering  
**FROM:** Alex Roller, Engineering Technician II  
**RE:** Construction of Parking lot – 2301 SE Willard St  
CSU-2019-002  
**DATE:** May 6, 2019

Change use of part of High School property from tennis courts to parking lot.

1. MMC Chapter 19.700 – Public Facility Improvements

The Planning Commission finds that the following complies with applicable criteria of MMC Chapter 19.700.

- A. MMC Chapter 19.700 applies to partitions, subdivisions, new construction, and modification or expansion of an existing structure or a change or intensification in use that result in any projected increase in vehicle trips or any increase in gross floor area on the site.

CSU-2017-007 established that there will be an increase in traffic resulting from the larger high school. The conditions written for CSU-2017-007 that apply to the SE 25<sup>th</sup> Ave and SE Willard St frontages apply to this land use application.

MMC 19.700 applies to the proposed development, however the requirements from CSU-2017-007 cover the modifications from this CSU.

- B. MMC Section 19.708.1 requires all development shall comply with access management, clear vision, street design, connectivity, and intersection design and spacing standards.

19.708.1.A – Access Management

Access requirements shall comply with access management standards contained in Chapter 12.16.

19.708.1.B – Clear Vision

2. Clear vision requirements shall comply with clear vision requirements contained in Chapter 12.24.

3. MMC Chapter 12.08 – Street & Sidewalk Excavations, Construction, and Repair

- A. This will apply to all construction that is completed in the right-of-way. The public improvement process will follow MMC 12.08.020.

4. MMC Chapter 12.16 – Access Management

The Planning Commission finds that the following complies with applicable criteria of MMC Chapter 12.16.

- A. MMC Chapter 12.16.040 establishes standards for access (driveway) requirements.

12.16.040A: requires that all properties be provided street access with the use of an accessway.

The proposed development is consistent with MMC 12.16.040A.

12.16.040B: Access spacing onto arterial and collector streets.

Neither SE 25<sup>th</sup> nor SE Willard are arterial or collector streets. Does not apply.

12.16.040C: Accessway Locations

1: Double Frontage

Lot will be allowed two accessways, as both SE Willard St and SE 25<sup>th</sup> Avenue are the same classification. The proposed development is consistent with MMC 12.16.040.C.1

2: Limiting driveway access from arterials and collectors.

Proposed development is consistent with MMC 12.16.040.C.2, and neither 25<sup>th</sup> nor Willard are collector or arterial classification.

3: Distance from property line

Plans currently show accessway as approximately 60-feet from the side property line. Proposed development is consistent with MMC 12.16.040.C.3.

4: Distance from Intersection

a: Plans currently show the driveway as approximately 105-feet from the intersection.

The proposed development is consistent with 12.16.040.C.4.b.

12.16.040D: Number of Accessway Locations

1: Safe access

Applicant has proposed the minimum number of accessway locations.

The proposed development is consistent with MMC 12.16.040.D.1.

2-3: Does not apply to this development, as no accessways are on arterials or collectors are proposed.

12.16.040E & 12.16.040F: Accessway Design - ADA standards & Width

Proposed driveways will conform to 12.16.040.E & 12.16.040.F through conditions from CSU-2017-007.

5. MMC Chapter 12.24 – Clear Vision at Intersections

The Planning Commission finds that the following complies with applicable criteria of MMC Chapter 12.24

A. 12.24.030: clear vision requirements

Proposed driveways, accessways and intersections will conform to 12.24.030 through conditions from CSU-2017-007.

**Recommended Conditions of Approval:**

1. Existing engineering conditions from CSU-2017-007 covering frontage improvements, bonding, stormwater, driveway widths and clear vision will apply to this CSU.